

# STATE ENVIRONMENTAL IMPACT REPORT

Florida Department of Transportation

Florida's Turnpike Enterprise

Florida's Turnpike (SR 91)

Project Development and Environment (PD&E) Study

From North of Jupiter (Indiantown Road / SR 706) (MP 117.0)

To North of Fort Pierce (Okeechobee Road / SR 70) (MP 153.7)

Palm Beach, Martin, and St. Lucie Counties, Florida

ETDM Project No.: 14295

FDOT Financial ID No.:423374-1-22-01

Federal Aid Project No.: N/A

Date: May 2022

# STATE ENVIRONMENTAL IMPACT REPORT FORM

## 1. PROJECT DESCRIPTION AND PURPOSE AND NEED:

### a. Project Information:

Project Name: Florida's Turnpike (SR 91)

Project Limits: From North of Jupiter (Indiantown Road / SR 706) (MP 117.0) to North of Fort Pierce (Okeechobee Road / SR 70) (MP 153.7)

County: Palm Beach, Martin, and St. Lucie

ETDM Number (If applicable): 14295

Financial Management Number: 423374-1-22-01

Federal-Aid Project Number: N/A

Project Manager: Brian P. Ribaric, PE

### b. Proposed Improvements:

A description of the proposed improvements is provided in Section 1.1, Project Description, in **Appendix A**

### c. Purpose and Need:

The Purpose and Need is provided in Section 1.2, Purpose and Need, in **Appendix A**.

### d. Project Planning Consistency:

Disregard providing historical details, instead focus on future phases of segments being advanced. If more than one segment is being advanced additional tables should be added.

Currently Adopted CFP-LRTP	COMMENTS
N	No project segments or phases are presently included in the Palm Beach TPA, Martin MPO, or St. Lucie TPO Cost Feasible Plan – Long Range Transportation Plans.



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PHASE	Currently Approved TIP	Currently Approved STIP	TIP/STIP \$	TIP/STIP FY	COMMENTS
PE (Final Design)	N	N	\$		No design phases are in the 5-year Work Program for the entire corridor.
R/W	N	N	\$		No right-of-way phases are in the 5-year Work Program for the entire corridor.
Construction	N	N	\$		No construction phases are in the 5-year Work Program for the entire corridor.

\*Include pages from current TIP/STIP/LRTP

### 2. ENVIRONMENTAL ANALYSIS

Issues/Resources

\*Substantial Impacts?

\*\*Supporting Information

Yes No Enhance No Inv.

#### A. SOCIAL & ECONOMIC

1. Social	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.1.1</u>
2. Economic	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.1.2</u>
3. Land Use Changes	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.1.3</u>
4. Mobility	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.1.4</u>
5. Aesthetic Effects	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.1.5</u>
6. Relocation Potential	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.1.6</u>

#### B. CULTURAL

1. Historic Sites/Districts	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.2.1</u>
2. Archaeological Sites	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.2.2</u>
3. Recreational Areas and Protected Lands	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.2.3</u>

#### C. NATURAL

1. Wetlands and Other					
Surface Waters	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.3.1</u>
2. Aquatic Preserves and					
Outstanding FL Waters	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.3.2</u>
3. Water Resources	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.3.3</u>
4. Wild and Scenic Rivers	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.3.4</u>

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5. Floodplains	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.3.5</u>
6. Coastal Barrier Resources	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<u>Appendix A, Section 2.3.6</u>
7. Protected Species and Habitat	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.3.7</u>
8. Essential Fish Habitat	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.3.8</u>

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### D. PHYSICAL

1. Highway Traffic Noise	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.4.1</u>
2. Air Quality	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.4.2</u>
3. Contamination	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.4.3</u>
4. Utilities and Railroads	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.4.4</u>
5. Construction	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.4.5</u>
6. Bicycles and Pedestrians	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.4.6</u>
7. Navigation	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>Appendix A, Section 2.4.7</u>

\* Substantial Impacts?: Yes = Substantial Impact; No = No Substantial Impact; Enhance = Enhancement; NoInv = Issue absent, no involvement.

\*\*Supporting information is documented in the referenced attachment(s).

### 3. ANTICIPATED PERMITS

Section 404 Dredge and Fill Permit (State 404 Permit) – USACE/FDEP

Section 10 Permit – USACE

Section 408 Alteration of a USACE Civil Works Project – USACE/SFWMD

U.S. Coast Guard Bridge Permit - USCG

Section 7 (a) Wild and Scenic Rivers Act Determination – NPS

Environmental Resource Permit - SFWMD

Right-of-Way Occupancy Permit – SFWMD

Sovereign Submerged Lands Easements – FDEP

National Pollutant Discharge Elimination System – FDEP

Gopher Tortoise Relocation Permit – FWC

Incidental Take Permit (as necessary) – USFWS/FWC

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## 4. ENGINEERING ANALYSIS

The engineering analysis is contained in the *Preliminary Engineering Report (PER)*. Engineering analysis was prepared in accordance with Part 1 Chapter 4, Project Development Process and Part 2, Chapter 3, Engineering Analysis of the FDOT Project Development and Environment (PD&E) Manual.

## 5. COMMITMENTS

Florida's Turnpike Enterprise (FTE) is committed to the following measures to minimize impacts to the social, natural and/or physical environment:

- If protected plants are observed during future surveys conducted prior to construction, the FTE will coordinate with FDACS and public parks (e.g., Jonathan Dickinson State Park) prior to construction for possible relocation of protected plants.
- If Florida Sandhill crane nests are observed during future surveys conducted prior to construction, then a 400-foot buffer will be implemented if construction occurs during the nesting season (January through July). The FTE will coordinate with the FWC during the project construction phase, if necessary.
- During the design phase of the Florida's Turnpike interchange at Crosstown Parkway, the FTE will coordinate with the City of Port St. Lucie (City) to ensure access is provided to the City's proposed adventure park, located north of Crosstown Parkway and west of the Turnpike. FTE will also coordinate with St. Lucie County to ensure access is provided to the St. Lucie West Middle School and St. Lucie West Centennial High School, located north of Crosstown Parkway and west of SW Cameo Boulevard.
- Florida's Turnpike Enterprise will continue to coordinate and partner with St Lucie County on:
  - the design and construction of the new W Midway Road interchange, including the replacement of the existing W Midway Road bridge over Florida's Turnpike and proposed on- and off-ramps associated with this interchange.
  - the design and construction of the new proposed Florida's Turnpike bridge over Glades Cut Off Road, including ensuring the new proposed structure will adequately span St. Lucie County's proposed improvements to Glades Cut Off Road.
- During the design phase, a Level II Impact to Construction Assessment will be conducted for sites with a potential contamination risk rating of medium or high, based on proposed right-of-way acquisition and design construction plans.
- Asbestos testing results will be obtained for building structures to be demolished or renovated in conjunction with the project.

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- The FTE is committed to the construction of feasible and reasonable noise abatement measures at locations identified in Noise Study Report, dated March 2022, contingent upon the following conditions during future Design phases:
  - Final recommendations on the construction of abatement measures is determined during the project's final design and through the public involvement process;
  - Detailed noise analyses during the final design process support the need, feasibility, and reasonableness of providing abatement;
  - Cost analysis indicates that the cost of the noise barrier(s) will not exceed the cost reasonable criterion;
  - Community input supporting types, heights, and locations of the noise barrier(s) is provided to the FTE; and
  - Safety and engineering aspects as related to the roadway user and the adjacent property owner have been reviewed and any conflicts or issues resolved.
- FTE will continue to coordinate with St. Lucie County for the Midway Road bridge over Florida's Turnpike and Midway Road/Florida's Turnpike interchange design and construction.
- FTE will continue to coordinate with St. Lucie County for the design and construction of the Florida's Turnpike bridge over Glades Cut Off Road.

### 6. FDOT SELECTED ALTERNATIVE

The Preferred Alternative includes widening Florida's Turnpike (SR 91) mainline from four 12-foot lanes to eight 12-foot lanes by adding two general toll lanes in each direction and widening both the inside and outside shoulders from 10 feet to 12 feet and providing a median barrier. The proposed typical section holds the outside edge of shoulder of the existing northbound lanes and reconstructs to the west to avoid impacting the specified width for Florida Gas Transmission (FGT).

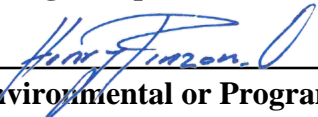
Operational improvement alternatives were developed for the four existing interchanges located at SW Martin Highway, SE Becker Road, SW Port St. Lucie Boulevard, and Okeechobee Road (SR 70). Two new interchange locations were developed as well and include Crosstown Parkway and W Midway Road. The following list presents the Preferred Alternative selected for each respective interchange location.

- SW Martin Highway – Alternative 7B
- SE Becker Road – Alternative 1
- SW Port St. Lucie Boulevard – Alternative 2
- Crosstown Parkway – Alternative 3
- W Midway Road – Alternative 1
- Okeechobee Road (SR 70) – Alternative 4

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For a detailed description of the Preferred Alternative, refer to the *Recommendation of Preferred Alternative Memorandum*, prepared under separate cover.

**7.  APPROVED FOR PUBLIC AVAILABILITY (Before public hearing when a public hearing is required)**

  
\_\_\_\_\_  
**Environmental or Program Development  
Manager or Administrator**

6/29/2021  
**Date**

**8. PUBLIC INVOLVEMENT**

1.  A public hearing is not required.
2.  A public hearing will be held on (insert date). This draft document is publicly available, and comments can be submitted to FDOT until August 11, 2021.  
District Contact Information:
3.  A public hearing was held on July 22, 2021 and the transcripts are available. (see Appendix G)
4.  An opportunity for a public hearing was afforded and was documented (insert date).

**9. APPROVAL OF FINAL DOCUMENT**

This project has been developed without regard to race, color, national origin, age, sex, religion, disability, or family status.

The final SEIR reflects consideration of the PD&E Study and the public hearing.

\_\_\_\_\_  
**District Secretary or Designee**  
(See Appendix H for Delegation Letter)

/ /  
**Date**

**10. SUPPORTING INFORMATION**

SEE ATTACHMENTS

# **APPENDICES**

**Appendix A – Supporting Information**

**Appendix B – ETDM Programming Summary Report and  
Agency Comments**

**Appendix C – Preferred Alternative Conceptual Plans**

**Appendix D – SHPO Coordination Letters**

**Appendix E – Agency Coordination**

**Appendix F – Recreational Areas Map**

**Appendix G – Public Hearing Transcripts**

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*Supporting Information*

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## 1.0 PROJECT SUMMARY

### 1.1 Project Description

Florida's Turnpike Enterprise (FTE) conducted a Project Development and Environment (PD&E) study to evaluate capacity improvements to the existing Florida's Turnpike (SR 91) corridor in Palm Beach, Martin and St. Lucie Counties, Florida. The project limits extend from north of Jupiter/Indiantown Road at Mile Post (MP) 117 to north of Okeechobee Road (SR 70) at MP 153.7, a distance of approximately 36.7 miles. Refer to **Figure 1-1** for the Project Location Map. The project consists of the widening of Florida's Turnpike (SR 91) from four to eight lanes by adding two general toll lanes in each direction.

Currently, Florida's Turnpike (SR 91) is a four-lane limited access toll facility. The interchange at Jupiter/Indiantown Road at MP 116 is not included in this study. The interchange of Turnpike and SR 714/SW Martin Highway (MP 134.6) is the only exit to Martin County. The Turnpike has two interchanges in Port St. Lucie in St. Lucie County, one at SE Becker Road (MP 138.5) and the other at SR 716/SW Port St. Lucie Boulevard (MP 143.1). The Port St. Lucie - Fort Pierce Service Plaza is at MP 145. The northernmost interchange is at SR 70/Okeechobee Road (MP 153.2) near Fort Pierce in St. Lucie County.

Numerous bridge structures will be widened or reconstructed along with the roadway. The project corridor includes crossings of the Loxahatchee River and St. Lucie Canal. Potential reconfiguration of existing interchanges and potential new interchange access locations were also evaluated as part of this PD&E study. The proposed new interchange access locations selected were Crosstown Parkway (MP 144.7) and W Midway Road (MP 150.4). The evaluation of a new I-95 direct connection interchange with the Turnpike near SE Bridge Road (MP 125.5) in Martin County is not part of this PD&E Study but will be part of a separate PD&E Study (FPID No. 446975-1-22-01).

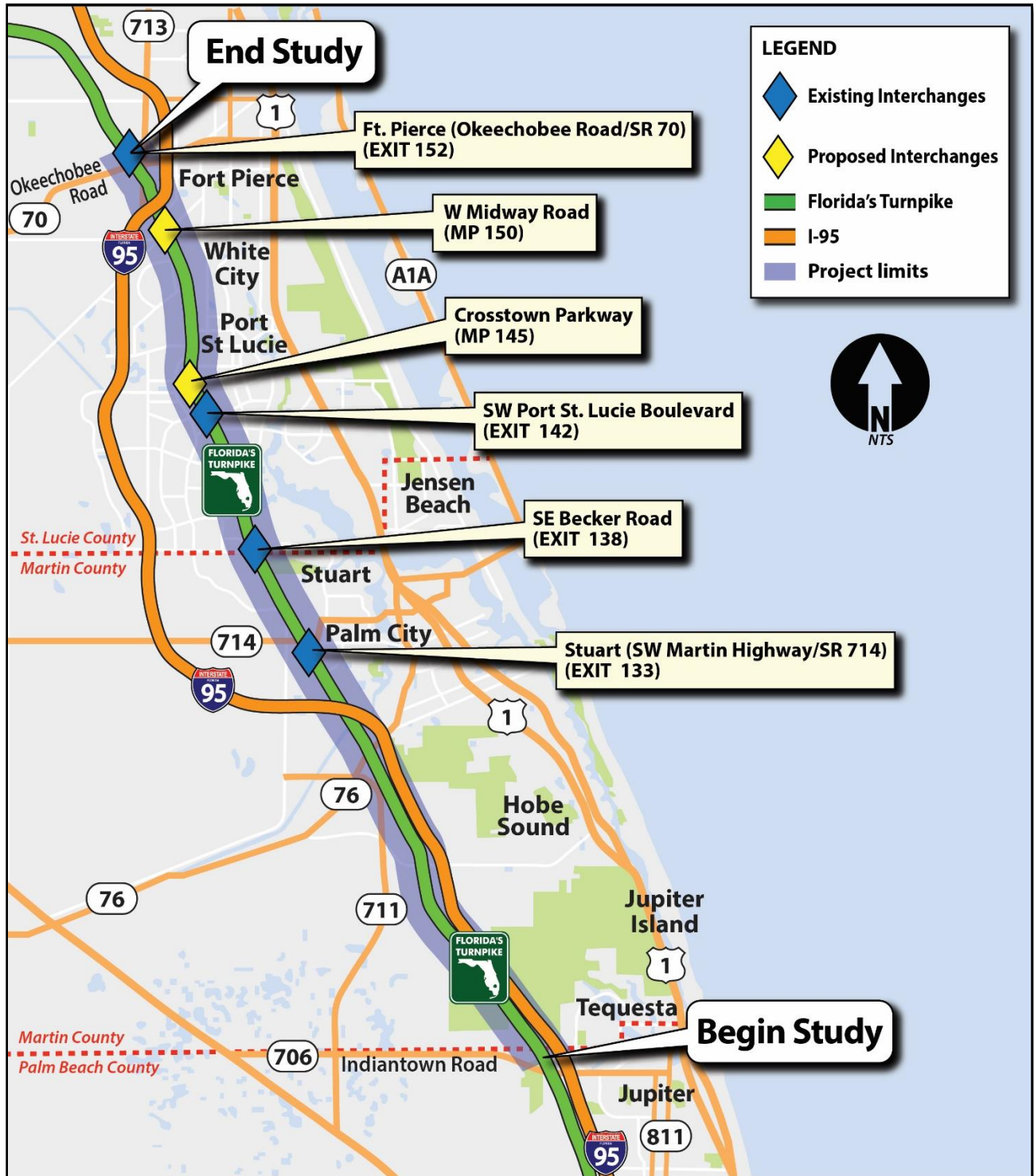


Figure 1-1: Project Location Map

## 1.2 Purpose & Need

The purpose of the project is to enhance the integrity of the highway while accommodating future traffic demands, improving overall safety, and meeting current design standards. New interchange access locations were evaluated as part of this study, as well as operational improvements to the existing interchanges.

The need for the project is based on the following criteria:

The primary purpose of the widening of Florida's Turnpike (SR 91) from north of Indiantown Road to north of Okeechobee Road is to add capacity that will accommodate future traffic volumes of freight and passenger vehicles linked to the projected growth in population and employment. The SR 91 corridor is located within Palm Beach, Martin, and St. Lucie Counties. As of April 1, 2018, the population in Palm Beach County is estimated to reach over 1.8 million by 2045, which represents a 26.3% increase; the population in St. Lucie County is expected to increase by 35.6% by 2045 to nearly 410,000; and the population in Martin County is expected to increase by 22.7% by 2045 to nearly 190,000. As the city and county populations increase, traffic will increase on area roadways as well. Based on data compiled for the Treasure Coast Regional Planning Model, by 2040, the Treasure Coast (including Martin, St. Lucie, and Indian River Counties) is expected to add an additional 104,000 workers, for an increase of 42%. St. Lucie County is projected to experience the largest gross gains in the workforce from 2010 to 2040. Key industries in the region set to experience the most growth include professional, health, retail, and construction.

Although freeway segments are all currently operating at an acceptable Level of Service (LOS) D or better and ramp roadways are currently operating under capacity with Volume-to-Capacity ratios less than 1.0, the Turnpike mainline will require three lanes of travel in each direction by year 2035 north of Port St. Lucie Boulevard, by year 2042 between SW Port St. Lucie Boulevard and SE Becker Road, and by year 2025 south of SE Becker Road. Four lanes will be required between SE Becker Road and SW Martin Highway by year 2033.

Establishment of two Freight Logistics Zones in St. Lucie County and around the Treasure Coast International Airport (TCI) and the Port of Ft. Pierce and a 1,200-acre Intermodal Logistics Center located just north of TCI have the potential to significantly increase freight traffic to and from these areas in northern St. Lucie County.

A total of 516 crashes occurred along the Turnpike within the project area during the 2012-2016 study period. Among the total 516 crashes, 325 were property damage only crashes, 182 were injury related crashes, 135 crashes involved trucks and 16 crashes involved at least one fatality. Of the total crashes, 103 (20.0%) were due to front to rear, 79 (15.3%) involved hitting a concrete traffic barrier, 58 (11.2%) involved hitting a guardrail face, and 56 (10.9%) were due to sideswipe in the same direction. Adding lanes to increase capacity and other operational enhancements at interchanges are anticipated to greatly improve the safety of the corridor. However, all portions of the Turnpike within the project limits contain crash rates lower than the statewide average for similar facility types.

During the 2012-2016 study period, one intersection in the project area that will be enhanced by proposed improvements at the SW Martin Highway interchange is the SW Martin Highway and SW High Meadow Avenue intersection. From 2012 to 2016, this intersection had 46 crashes, resulting in a crash rate higher

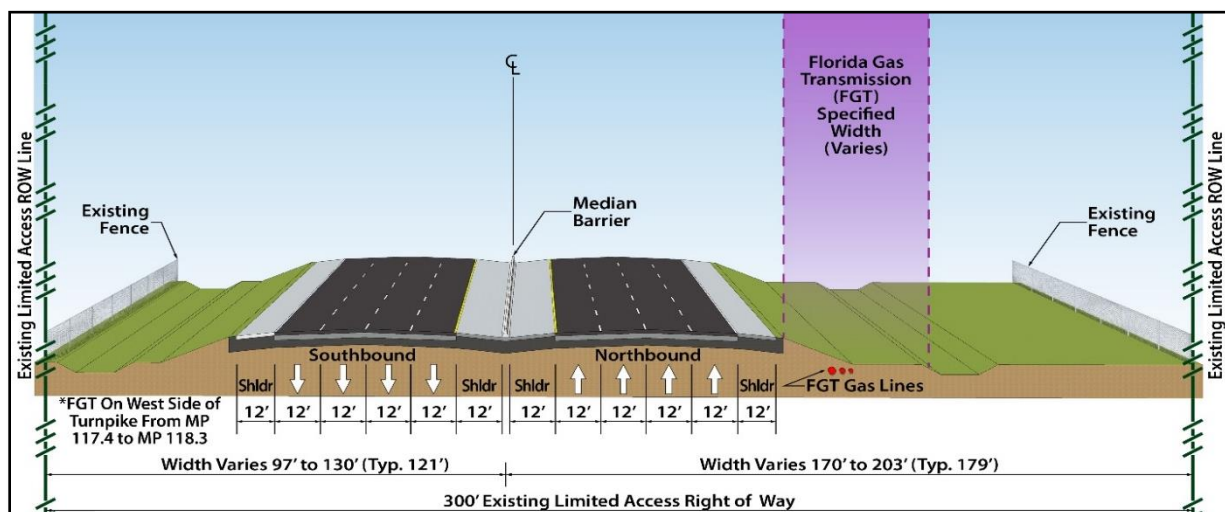
than the statewide average for similar facilities (1.572 crashes per million vehicle travel miles). Of the 46 total crashes, the most common crash type was front to rear with 24 crashes (52.2%), followed by six angle crashes (13.0%), and six unknown crashes (13.0%) during those years. While there were no fatalities or severe injuries, there were 27 crashes which resulted in property damage only (58.7%), 15 crashes which resulted in minor injuries (32.6%), and four crashes which resulted in moderate injuries (8.7%).

Additionally, the Turnpike is identified as a “critical transportation facility” in the Treasure Coast Regional Planning Council’s (TCRPC) Evacuation Transportation Analysis as part of the Statewide Regional Evacuation Study Program. Critical transportation facilities play an important role for all evacuation scenarios. For the Evacuation Level A Operational Scenario, the most minor storm event evaluated, portions of this study corridor are identified as “critical segments with highest vehicles queues.” For Evacuation Levels B through E Operational Scenarios, with E being the highest level of evacuation, the entirety of the project area segments is identified as “critical segments with highest vehicle queues.”

### 1.3 Alternatives Analysis Summary

#### 1.3.1 Preferred Mainline Alternative

This PD&E study involves the widening of Florida’s Turnpike (SR 91) mainline from four 12-foot-wide travel lanes to eight 12-foot-wide travel lanes by adding two general toll lanes in each direction and widening both the inside and outside shoulders from 10 feet to 12 feet. Due to the presence of Florida Gas Transmission (FGT) buried utilities on the east side of the roadway, all proposed widening will occur to the west. The proposed eight lane typical section is consistent throughout the entirety of the project limits. The proposed typical section includes four 12-foot-wide travel lanes in each direction, 12-foot-wide paved outside shoulders, 12-foot-wide paved inside shoulders, and a two-foot-wide concrete median barrier. The required typical section width for Florida’s Turnpike (SR 91) is 146 feet. The proposed mainline improvements fall within the available right-of-way and do not require right-of-way acquisition. A detailed graphic of Florida’s Turnpike (SR 91) mainline preferred typical section is provided below in **Figure 1-2**.



**Figure 1-2: Mainline – Preferred Typical Section**



**Table 1-1: Preferred Interchange Alternatives**

Interchange	Mile Post (MP)	Type	Preferred Alternative
SW Martin Highway (SR 714)	MP 135	Full Interchange	Alternative 7B
SE Becker Road	MP 138	Full Interchange	Alternative 1
SW Port St. Lucie Boulevard	MP 142	Full Interchange	Alternative 2
Crosstown Parkway	MP 145	Partial Interchange (to/from south)	Alternative 3
W Midway Road	MP 150	Full Interchange	Alternative 1
Okeechobee Road (SR 70)	MP 152	Full Interchange	Alternative 4

**1.3.2 No-build Alternative**

The No-build Alternative was also considered for this corridor. As part of this alternative, it is assumed that no capacity or safety improvements will be made to the mainline roadway or any existing interchanges. It is also assumed that no new interchanges will be constructed. This alternative remained a viable alternative throughout the PD&E study.

**1.3.3 Summary of Preferred Alternative**

The Preferred Alternative was analyzed to determine the potential impacts to the social, cultural, natural, and physical environment compared to the No-build Alternative. **Table 1-2** summarizes the impacts associated with the Preferred Alternative. The project specific alternative evaluation between the Preferred Alternative and the No-build Alternative is shown in **Table 1-3**.

**Table 1-2: Environmental Impact Summary of Preferred and No-build Alternatives**

Item		Preferred Alternative	No-build Alternative
Social	Right-of-Way Impacts (acres)	61.2	0
	Number of Parcels Impacted	85	0
	Number of Business or Resident Relocations	5	0
	Number of Community Facilities Impacted	0	0
	Park and Recreational Facilities Impacted	1	0
Cultural	Native American Lands Impacted (acres)	0	0
	NRHP-Eligible Historical and Archaeological Sites Impacted (number)	0	0
Natural	Wetland Impacts (acres)	62.7	0
	Other Surface Water Impacts (acres)	367.1	0
	Essential Fish Habitat Impacts (acres)	6.1	0
	Floodplain Impacts (acres)	73.6	0
	Protected Species (potential)	Low	None
	Critical Habitat (acres)	0	0
Physical	Number of Contamination/Hazardous Waste Sites Impacted**	27	0
	Number of Residences Impacted by Noise	1,518	0
	Number of Potential Utilities Relocated	65	0

\* NRHP = National Register of Historic Places

\*\* total medium or high ranked sites within 500 feet of project area

**Table 1-3: Alternative Evaluation Matrix**

<b>Comparison Metric</b>	<b>Preferred Alternative</b>	<b>No-build Alternative</b>
Conforms with Transportation Plan	No	No
Maintains Florida's Turnpike LOS	Yes	No
Accommodates Future Travel Demands	Yes	No
Improves Evacuation Time	Yes	No
Improves Emergency Response Time	Yes	No
Additional Right-of-Way Required (acres)	61.2	0.0
Project Cost (in 2020 dollars)	\$1.45 billion*	\$0.00

\* Project Costs will be reassessed during the design phase of each project segment.



## 2.0 ENVIRONMENTAL IMPACT ANALYSIS

### 2.1 Social and Economic Impacts

The documentation of the existing and proposed conditions and the evaluation of potential social impacts are provided in the following support documents completed as part of this study.

- Sociocultural Effects Evaluation (SCE) Memorandum (February 2021)
- Conceptual Stage Relocation Plan (CSR) (January 2022)
- Aesthetic Plan Report (November 2020)

#### 2.1.1 Social

As part of the Efficient Transportation Decision Making (ETDM) screening process, the United States Environmental Protection Agency (USEPA) commented that there are significant low-income, minority, linguistically isolated, and other special populations in the project corridor. To ensure compliance with Executive Order (EO) 12898, *Environmental Justice*, accommodations for limited English proficiency were provided during public involvement outreach and engagement efforts. The Preferred Alternative for the mainline envisions constructing the improvements in such a way as to “hold” the east side of the current typical section in order to accommodate the existing FGT pipeline. All lane additions are expected to be constructed within the existing right-of-way for the mainline. Therefore, no physical impacts or relocations are expected to residents or businesses adjacent to the existing mainline. Additionally, there are four existing interchanges within the project limits that are recommended to be improved, and two new interchanges proposed within the project area. A total of 61.2 acres of right-of-way acquisition is required for the six interchange locations. A total of two business relocations and three residential relocations will occur as a result of the proposed interchange improvements. Moreover, the improved access to the existing four interchanges, and the additional access at the two new proposed interchanges is expected to provide residents with better access to the Turnpike, which may improve economic choices for residents and businesses within the corridor. The project is not expected to create new barriers to social interaction for the communities surrounding the project, nor detract from community goals or special designations. The addition of the new interchanges and the improvement of the existing interchanges along the corridor should provide equal or better economic opportunities for residents and businesses in the community, as enhanced access to such a significant transportation resource is provided. The improvements will also assist in evacuation of residents and visitors during a hurricane emergency.

Additionally, a noise study report and air quality screening report have been developed as part of the PD&E Study, which complies with Executive Order 13045, *Protection of Children from Environmental Health Risks and Safety Risks*. These reports are discussed in detail in Sections 2.4.1 Highway Traffic Noise, and 2.4.2 Air Quality. The proposed project is located in the counties of Palm Beach, Martin, and St. Lucie, an area currently designated as being in attainment for particulate matter (2.5 and 10 microns in size) and carbon monoxide (CO). The project is not forecasted to change the volume of motor vehicles or the vehicle-miles-traveled (VMT) within any of the evaluated segments by more than 11 percent and, when considering the total VMT, would result in an approximate two percent reduction in the VMT within the project corridor which would correspondingly decrease Mobile Source Air Toxics (MSATs). Moreover, USEPA regulations for vehicle engines and fuels will result in a decline in MSAT

emissions over the next several decades. The reduction in MSATs with the project, along with the reduction in emissions due to the regulations, will result in lower future background levels of the pollutants.

For the year 2045 Build condition, noise levels were modelled using the Federal Highway Administration's (FHWA) Traffic Noise Model (TNM) at 3,134 noise sensitive sites. Noise levels at 1,518 residences and 108 non-residential "special land use" sites, are predicted to approach or exceed the Noise Abatement Criteria (NAC) for the year 2045 Build Alternative and therefore considered "impacted". No noise sensitive sites are expected to experience a substantial increase (15 decibels, using an "A" scale weighting = dB(A)) in traffic noise compared to existing conditions.

Analyses were performed of the impacted locations to determine if noise abatement was potentially feasible and reasonable under FDOT policy. The noise barrier analysis performed to date indicates that noise barriers could potentially provide reasonable and feasible noise abatement for 1,366 of the 1,518 impacted residences, as well as provide a 5 dB(A) noise reduction benefit to 1,493 non-impacted residences. Noise abatement was not determined feasible and reasonable for any of the 108 impacted special use sites; however, some of the special use locations will receive incidental benefits from noise barriers for the residential areas. Noise barriers at 14 locations were determined to be potentially feasible and reasonable and will be given further consideration during the Design phase of this project. Noise abatement will be further considered during the design phase of the project.

### **2.1.2 Economic**

The proposed improvements would support economic vitality through freight and goods movement by reducing congestion and improving access at the existing interchanges and enhancing access through the implementation of the two new interchanges. By improving the connectivity between major trucking and freight routes, rail, ports, and distribution centers, the project will increase the ability to provide goods and products inside and outside the region. The improved connectivity is expected to translate into job opportunities within the region to support freight and logistics centers.

The proposed improvements also create a better overall transportation system linkage, as well as relieving congestion on the local system thus improving access to these areas for residents living in the surrounding neighborhoods. The project is not anticipated to impact any of the transit services that cross over the Turnpike mainline and will not negatively affect current transportation modes that serve special needs population, nor create any disproportionate effects on these populations.

### **2.1.3 Land Use Changes**

The project has minimal potential for negative effects on the land use along of the corridor. Improvements to the mainline can be built within the existing right-of-way. It is expected that right-of-way will need to be acquired for future stormwater management pond sites and proposed interchanges; however, relative to the overall project size, additional right-of-way needs will be minimal and will have little effect on the overall land use along the project corridor.

Because of the limited access nature of the mainline, the project should have minimal effects on adjacent future land use. At the existing interchanges, the improved access should help existing and future commercial and residential land use elements. The proposed new interchanges will provide opportunity for land use changes in the interchange area. FTE is coordinating these new interchange access points with the appropriate local governments

so that local comprehensive plans and Capital Improvement Plan can be modified, as necessary. For a detailed discussion of the existing and proposed land uses along the project corridor see the *Sociocultural Effects Evaluation Memorandum* (February 2021) completed for this project.

#### **2.1.4 Mobility**

Mobility is the ability of residents to move freely about their community through a variety of transportation modes. Extra emphasis is on providing improved transportation for non-driving and transit dependent populations (i.e. low-income, elderly, disabled, and children) so that normal daily activities can be carried out in their neighborhoods more easily.

The project will improve mobility in the project and adjoining area by reducing congestion and improving access. The project provides an opportunity to enhance mobility for all modes, including pedestrians, bicyclist, and transit users, for the businesses and residents in the area. Local transportation providers have an opportunity to revise and enhance their services in concert with the proposed improvements.

#### **2.1.5 Aesthetic Effects**

Proposed construction of improvements to Florida's Turnpike (SR 91) mainline, including modifications to four existing interchanges and addition of two new interchanges, will not involve changes to the aesthetic character compatibility, community values, sensitive areas, or visual features within the project area. In general, the project will retain the current aesthetic aspects of the corridor.

As part of this PD&E Study, an *Aesthetic Plan Report* (November 2020) has been developed that will provide guidance for aesthetics through the design, construction, and maintenance stages of the project. FTE has invested heavily in creating a unique aesthetic brand that greatly enhances the traveler's experience using the Mainline Toll System. The importance of aesthetics was emphasized when the FTE created the "*Landscape Program Master Plan*" for all Turnpike facilities in Florida. Much of this philosophy to achieve a successful, predictable and efficient Landscape Program developed in this *Landscape Program Master Plan* is being incorporated into this study.

#### **2.1.6 Relocation Potential**

A *Conceptual Stage Relocation Plan* (January 2022) has been developed, which identifies the impacts to businesses and resident that may require relocation due to the proposed project. The purpose of the *Conceptual Stage Relocation Plan* is to identify community characteristics, analyze the impact of the project on the community, and identify residences and businesses that would be affected by the project and any special relocation needs.

Based on interchange designs and stormwater pond requirements for the proposed widening, the corridor may require business and residential relocations.

Three residential relocations are anticipated to accommodate the Preferred Alternative. Two of the residential relocations are located at the Florida's Turnpike (SR 91)/SW Port St. Lucie Boulevard interchange. The third residential relocation is located at the Florida's Turnpike (SR 91)/Okeechobee Road (SR 70) interchange.

There are two potential business relocations anticipated to accommodate the Preferred Alternative. One business relocation is located at the Florida's Turnpike (SR 91)/SW Martin Highway interchange, while the second is located

at the Florida's Turnpike (SR 91)/SW Port St. Lucie Boulevard interchange. Information about the anticipated residential and business relocations can be found in the project's *Conceptual Stage Relocation Plan*.

## 2.2 Cultural Resources

The documentation of the existing and proposed conditions and the evaluation of the potential effects of the project on Cultural Resources are provided in the following support document completed as part of this study.

- Cultural Resources Assessment Survey (CRAS) (September 2020)

A *Cultural Resource Assessment Survey* (CRAS) was prepared to identify cultural resources within the project area of potential effect (APE) and assess the resources in terms of their eligibility for listing in the National Register of Historic Places (NRHP) according to the criteria set forth in 36 Code of Federal Regulations (CFR) Section 60.4. The State Historic Preservation Office (SHPO) concurred with the CRAS on October 26, 2020. The archaeological APE for this project is defined as the geographic limits of the proposed project improvements, while the historic APE is defined as 150 feet outward from the proposed project improvements.

The CRAS was forwarded to the SHPO for consultation and review. Copies of the SHPO concurrence with the findings and recommendations of the CRAS (letter dated September 16, 2020, concurrence dated October 26, 2020) is included as **Appendix D**.

### 2.2.1 Historic Sites/Districts

The historic resources survey resulted in the identification of 31 previously recorded historic resources, consisting of 15 canal segments, eight road segments, one railroad segment, six buildings, and one bridge, within the project limits. The survey also resulted in the identification of 38 unrecorded historic resources, including nine newly identified buildings, 28 newly identified bridges, and one newly identified segment of Florida's Turnpike (SR 91) mainline within the project area.

All but two of the historic linear resources have either been previously determined ineligible for National Register or are considered ineligible for the National Register based on the results of this project's survey.

Portions of the St. Lucie Canal and FEC Railway – Lake Harbor Branch have been determined eligible in segments of the current APE. Field survey reveals that these two resources maintain their historic associations and integrity within the current project APE and are therefore considered eligible for the National Register within the project APE.

All six of the previously recorded historic buildings have been previously determined by the SHPO to be National Register-ineligible. Field survey and historical research did not reveal any additional information to re-evaluate these resources, and therefore, they remain ineligible for the National Register.

Field survey resulted in the identification of nine newly identified historic buildings within the current project APE. All of the nine buildings are of a common style and type in South Florida and lack historical significance. Therefore, they are ineligible for individual listing in the National Register under Criteria A, B, C, or D.

The 29 bridges associated with Florida's Turnpike (SR 91) that are located within the current project APE are common types that were popular in the mid-twentieth century and were built throughout the US. Therefore, they are considered ineligible for the National Register under Criteria A, B, C, or D, individually and as a resource group.

During the review of the project APE, the surrounding area was also reviewed to identify any potential National Register-eligible historic districts. The buildings surrounding the APE exhibit common construction techniques and designs from their respective area of construction. Further, most buildings exhibit some form of exterior alteration that comprises historic integrity. Historic research also did not reveal any significant historic associations. Therefore, there are no potential historic districts within, or partially within, the current project APE.

### **2.2.2 Archaeological Sites**

No archaeological sites were identified within the archaeological APE. Background research, a pedestrian survey, and extensive subsurface testing conducted during the current survey determined that large portions of the archaeological APE have been subjected to land modification associated with the construction of Florida's Turnpike (SR 91) mainline and its numerous interchanges, the surrounding development, and the installation of underground utilities. While subsurface testing was not feasible within areas of existing hardscape or underground utility corridors, 156 shovel tests were excavated throughout the archaeological APE, and no cultural material was identified within any of the tests. The results of the current survey confirmed a low potential for encountering intact archaeological resources within the archaeological APE.

### **2.2.3 Recreational Areas**

Between Indiantown Road (SR 706) and north of Okeechobee Road (SR 70), there are thirteen (13) parks, preserves, and natural areas within the area surrounding the project (see **Appendix F**). The project is adjacent to substantial public lands that serve as important wildlife habitat and are used for recreation. Below is a brief description of the recreational areas that are adjacent to and/or cross Florida's Turnpike (SR 91) mainline, as well as their potential to be impacted by the proposed improvements.

The southern end of the project is surrounded by Loxahatchee River Natural Area, Cypress Creek Natural Area, and Riverbend Park to the west and Jonathan Dickinson State Park to the east. This area includes various recreational trails and navigable waterways. A paddling trail, along the Loxahatchee River, crosses underneath Florida's Turnpike (SR 91) mainline, just north of MP 117.5. The Loxahatchee River Management Area Multi-Use Trail runs adjacent to the mainline for approximately 0.25 miles, south of the Loxahatchee River. The Cypress Creek Management Area "Ocean to Lake Trail," crosses underneath Florida's Turnpike (SR 91) mainline, just north of MP 119.5.

Halpatokee Regional Park and Atlantic Ridge Preserve State Park are located in the southeast quadrant of the interchange between I-95 and SW Kanner Highway. They include biking, hiking, and paddling trails, with one paddling trail crossing underneath the Thomas B. Manuel Bridge, along the St. Lucie River. Phipps Park and St. Lucie North Campgrounds are located on either side of the St. Lucie River, on the west side of Florida's Turnpike (SR 91) mainline. There is an existing boat ramp, east of the southern end of the Thomas B. Manuel Bridge, which can be accessed from Phipps Park.

The City of Port St. Lucie is planning to construct an adventure park in the northeast quadrant of the intersection of SW Cameo Boulevard and Crosstown Parkway (see **Appendix C** – Preferred Alternative Concept Plans, sheet 55). The Preferred Alternative interchange for Crosstown Parkway includes a fishhook ramp, which ties into the northern terminus of SW Cameo Boulevard. The adventure park is planned to be constructed on the land which has SW Cameo Boulevard to its west, Florida's Turnpike (SR 91) mainline to its east, and Crosstown Parkway to its south.



The proposed fishhook ramp will bisect the adventure park parcel. Florida's Turnpike Enterprise has coordinated with the City of Port St. Lucie, throughout the duration of the PD&E study, to develop a Preferred Alternative that works in conjunction with the proposed adventure park.

The City of Port St. Lucie's Winterlakes Neighborhood Park is located within the southwest quadrant of the Florida's Turnpike (SR 91) mainline and Midway Road crossing (see **Appendix C** – Preferred Alternative Concept Plans, sheet 65). This park abuts the existing mainline right-of-way and construction of a new interchange with Midway Road will result in impacts to this park. Construction of a new southbound on-ramp to Florida's Turnpike (SR 91) from Midway Road will require approximately 0.60 acres of additional right-of-way from this park; however, these impacts are within a drainage buffer zone located between Florida's Turnpike (SR 91) and proposed park facilities and will not result in impacts to proposed park facilities. Florida's Turnpike Enterprise will continue to coordinate with the City of Port St. Lucie to minimize these impacts to the greatest extent possible.

The northern end of the project is surrounded by the Gordy Road Recreation Area and Ten Mile Creek Preserve to the west and George LeStrange Preserve to the east. There are walking trails within all three areas, although none of these recreational facilities cross Florida's Turnpike (SR 91) mainline. Paddling along Ten Mile Creek is allowed, west of Florida's Turnpike (SR 91) mainline.

With the exception of the Winterlakes Neighborhood Park, the proposed improvements are not anticipated to impact the recreational areas mentioned above. Temporary impacts may occur during construction, at the locations where facilities cross underneath Florida's Turnpike (SR 91) mainline. Refer to *Section 2.4.5* of this document, for information on the procedures to be followed during construction to minimize impacts to the recreational areas.

There are five schools with sports complexes and dedicated sports parks adjacent to Florida's Turnpike (SR 91) mainline. The majority of the sports facilities are located adjacent to the existing limited access right-of-way; however, no right-of-way will be needed from these facilities, therefore they are not expected to be directly impacted by the proposed improvements. The facilities include Jupiter Community Park, Southfork High School, Citrus Grove Park, Jessica Clinton Park, and Turtle Run Park. St. Lucie West Centennial High School is located in the northwest quadrant of the proposed Crosstown Parkway partial interchange concept. There are no proposed impacts to the existing facilities; however, two new ramps (the southbound on-ramp and northbound off-ramp) are proposed to tie into SW Cameo Boulevard, along the east side of the high school.

### **2.3 Natural Environment**

The documentation of the existing and proposed conditions and the evaluation of the project's potential effects on the natural environment are provided in the following support documents completed as part of this study.

- Natural Resources Evaluation (NRE) Report (May 2021)
- Pond Siting Report (PSR) (June 2021)
- Location Hydraulics Report (LHR) (June 2021)
- Water Quality Impact Evaluation (WQIE) (December 2020)

### **2.3.1 Wetlands and Other Surface Waters**

Potential direct impacts to wetlands and surface waters were assessed for the Preferred Alternative. Wetland and surface water habitat types to be impacted by the proposed improvements include natural and man-made streams and waterways, reservoirs, mixed wetland hardwoods, exotic wetland hardwoods, cypress, hydric pine, wetland forested mixed, wetland scrub, and freshwater marshes. Impacts associated with the Preferred Alternative total 429.81 acres and include, 14.11 acres of forested wetlands, 48.44 acres of scrub shrub wetlands, and 0.12 acres of herbaceous wetlands (total 62.67 acres of wetlands), and 9.30 acres of natural streams and waterways (natural surface waters) and 357.84 acres of man-made surface waters (total 367.14 acres of surface waters). Functional loss was calculated by wetland and natural surface water habitat types for the Preferred Alternative using the Uniform Mitigation Assessment Method (UMAM). Construction of the Preferred Alternative results in a loss of 39.25 functional units. These scores are subject to agency review and may change during the permitting process.

Indirect impacts resulting from construction of the Preferred Alternative include secondary wetland and surface water impacts in the proposed project area. These impacts are anticipated to be minor since the wetlands and surface waters are already associated with the existing roadway and interchanges. Habitats along the edge of the existing roadway and interchanges were disturbed when these areas were constructed and have since experienced constant disturbance from right-of-way maintenance and exposure to nuisance/exotic species. This “edge effect” will remain with the construction of the proposed improvements. Therefore, these disturbed edges are not expected to increase in areas where the roadway and interchanges already exist.

The ETAT evaluated the project’s effects on various natural resources, including wetlands. Environmental Technical Advisory Team (ETAT) comments are summarized in ETDM Summary Report in **Appendix B**. A pre-application meeting was conducted with the South Florida Water Management District (SFWMD), US Army Corps of Engineers (USACE), National Marine Fisheries Service (NMFS), and FTE on November 16, 2017, providing an overview of the project, discussing existing permits, and necessary permit requirements and approvals. Follow-up correspondence with the SFWMD took place in July 2017 and July 2020 to clarify permit requirements and approvals at specific waterway crossing locations. Coordination with the Florida Department of Environmental Protection (FDEP) took place on January 21, 2020 for a sovereign submerged lands determination for potential state-owned lands within the project area.

Wetland impacts, which will result from the construction of this project, will be mitigated pursuant to Section 373.4137, F.S. to satisfy all mitigation requirements of Part IV, Chapter 373, F.S. and 33 U.S.C. 1344. Compensatory mitigation for this project will be completed through the use of mitigation banks and any other mitigation options that satisfy state and federal requirements.

While the project will result in wetland impacts, the implementation of a mitigation plan that satisfies all requirements of Part IV, Chapter 373, F.S. and 33 U.S.C. 1344 will offset these impacts and construction of the project should not result in substantial impacts to wetlands.

### **2.3.2 Aquatic Preserves and Outstanding Florida Waters**

The Loxahatchee River-Lake Worth Creek Aquatic Preserve is located downstream (eastward) of the project area. The Loxahatchee River and Cypress Creek are both a part of this preserve; however, both cross the project area upstream of, and outside of, the preserve boundary. The proposed project has potential to affect the preserve, as it is located downstream of the Turnpike.

The waters of the Loxahatchee River at the crossing of Florida's Turnpike (SR 91) mainline are listed as an Outstanding Florida Water (OFW) pursuant to Rule 62-302.700(2)(d), F.A.C., through its designation as a National Wild and Scenic River. The Loxahatchee River is specifically identified as an OFW in Rule 62-302.700(9)(j), F.A.C.

If the proposed project directly discharges stormwater into the OFW, it must meet the more stringent water quality criteria implemented to protect OFWs. These criteria are identified in the Environmental Resource Permit Applicant's Handbook Vol. I, Section 10, and will be addressed during project design and permitting. The proposed project will include a stormwater management system, which will be designed in compliance with applicable water quality criteria to prevent degradation of water resources and habitat quality in and around the Loxahatchee River crossing.

### **2.3.3 Water Resources**

#### Water Quality

This project will require water quality treatment in accordance with the following SFWMD regulations:

Per SFWMD ERP Applicant's Handbook, Volume II, Section 4.2.1: Retention, detention, or both shall be provided for one of the three following criteria or equivalent combination thereof:

- 1) Wet detention volume shall be provided for the first inch of runoff from the developed project or the total runoff of 2.5 inches times the percentage of imperviousness, whichever is greater.\*
- 2) Dry detention volume shall be provided equal to 75 percent of the above amounts computed for wet detention.
- 3) Retention volume shall be provided equal to 50 percent of the above amounts computed for wet detention.

\*In addition to the criteria provided above, SFWMD confirmed that the required water quality volume is 2.5 inches over the new impervious area in areas of reconstruction and widening but clarified that full treatment of new and existing impervious should be provided, if feasible.

Per SFWMD Environmental Resource Permit (ERP) Applicant's Handbook, Volume II, Section 4.1.3: Systems that have direct discharge to an OFW must provide an additional fifty percent of the required treatment.

Per SFWMD ERP Applicant's Handbook, Volume II, Section 4.1.4: Systems discharging to a waterbody that has been identified as impaired by the FDEP shall be designated to provide a net improvement. Water body identification number (WBID) 31948 Tenmile Creek is impaired for total phosphorus (TP). With the implementation of a stormwater management plan that meets the SFWMD requirements, the construction of the project should not have a substantial impact on water quality.

The Turnpike mainline is permitted from MP 137.676 to 152.610 (Permit No. 56-00912-S, Application 910523-6). This permit provides treatment for 2.5 inches times the impervious area of the median shoulder in the roadside ditch adjacent to the southbound lanes via ditch blocks. However, this treatment area will be filled in with the proposed widening. The existing treatment will be replaced in proposed pond areas. The interchanges have permitted stormwater ponds within the infield areas. Proposed stormwater management facilities are recommended to accommodate the proposed widening and interchange



improvements. The *Pond Siting Report* (May 2020) estimates the volume required to mitigate FDOT and SFWMD stormwater requirements and identifies right-of-way for any necessary off-site stormwater management facilities. Three pond site alternatives were identified for each basin between milepost (MP) 133 and MP 142. Pond sizing calculations were provided for the remainder of the basins within the project limits. Given the seasonal high groundwater table (SHGWT) throughout the project corridor, wet detention ponds are recommended. The Loxahatchee River, as well as Cypress Creek and an adjacent wetland are classified as OFW, and any direct discharge into these water bodies requires an additional 50% treatment. The offsite discharges for this project are expected to occur within FDOT right-of-way (ROW) and the additional 50% treatment criteria does not apply, since the discharges will be indirect discharges to the OFW's.

Thirty pond sites were evaluated for the project limits between MP 133 and MP 142. The pond sites were evaluated on the basis of several factors including, total cost of each alternative, Federal Emergency Management Agency (FEMA) flood zone, wetland impacts, habitat and environmental impacts, as well as ease of hydraulic connectivity to the pond site. The preferred pond alternatives were selected based on the sites that could best be described based on these parameters.

From SE Becker Road to the C-24 Canal, Florida's Turnpike (SR 91) mainline is bordered by FGT on the east and a drainage canal on the west. This condition also exists from MP 121.9 to MP 125.4 and from MP 146.4 to MP 147.7, which are outside the limits of the pond siting but within the limits of the PD&E study. Given the design constraints, it is recommended to convert the existing ditch over the FGT line to a treatment swale. It is recommended that this approach be coordinated with FGT early in the design phase to ensure it is a feasible option.

#### Water Quantity

The proposed widening of the existing Turnpike mainline and associated interchange improvements will result in impacts to the adjacent FEMA floodplains. The anticipated impacts to the 100-year floodplain due to the proposed roadway widening were estimated to be 73.6 acres; however, the impact volume from the proposed widening and necessary compensation will need to be assessed during the design phase, when survey of the existing ground, geotechnical data for the SHGWT, and proposed cross sections are available. Off-site floodplain compensation sites, on-site swales, and infield storage areas should be evaluated to provide compensation for floodplain impacts.

The necessary culvert and bridge culvert extensions will have transverse impacts on the existing floodplains that will need to be further analyzed during the design phase. The proposed bridge widenings over the regulatory floodways at Loxahatchee River, Roebuck Creek, and Ten Mile Creek will require a No-Rise Certification from FEMA. The proposed improvements will have both transverse and longitudinal encroachments on the Loxahatchee River and Roebuck Creek, but only transverse encroachments area anticipated at Ten Mile Creek.

Replacement drainage structures for this project are limited to hydraulically equivalent structures which are not expected to increase the backwater surface elevations. Since flooding conditions in the project area are inherent in the topography or are a result of other outside contributing sources, and there is no practical alternative to eradicate flooding problems in any significant amount, existing flooding will continue, but will not increase as the result of the construction of this project.

The project will not affect existing flood heights or floodplain limits. There will be no significant change in the potential for interruption or termination of emergency service or emergency evacuation routes as the result of construction of this project. Therefore, it has been determined that this encroachment is not substantial.

### **2.3.4 Wild and Scenic Rivers**

Projects involving Wild and Scenic Rivers require consultation with the National Park Service (NPS) in accordance with Section 7 of the Wild and Scenic Rivers Act (WSRA). The NPS is the administrating agency responsible for making a Section 7(a) determination for the Loxahatchee River crossing. The NPS must conclude, based on information provided by the project proponent that the project will not have a "direct and adverse" effect on the free-flowing condition, water quality, or the outstandingly remarkable values for which the Loxahatchee River was designated.

Florida's Turnpike (SR 91) mainline crosses the Loxahatchee River within a Wild and Scenic River segment categorized as "Scenic." This segment extends from north of Indiantown Road (SR 706) to north of the Florida Turnpike/I-95 alignment. This segment is described in the NPS, Final Wild and Scenic River Study/Final Environmental Impact Statement (July 1984), for the Loxahatchee River, as largely undeveloped with many values that support its inclusion in the National Wild and Scenic River System. The Loxahatchee River has special designations affording it extra protections to maintain water quality and habitat viability. These designations include its status as an Outstanding Florida Water, including in the Loxahatchee River Lake-Worth Aquatic Preserve, and Florida's first component of the National Wild and Scenic River System.

Initial coordination meetings with the NPS occurred on February 15, 2018 and September 25, 2020, at which the proposed project was presented and discussed. As the project moves forward, further evaluation in accordance with Section 7(a) of the WSRA and additional coordination with the NPS will be required.

### **2.3.5 Floodplains**

FEMA floodplains are prevalent throughout the project corridor. There are three regulatory floodways within this study limits: Loxahatchee River, Roebuck Creek, and Tenmile Creek. The proposed widening of the Florida's Turnpike (SR 91) mainline, from four to eight lanes and associated interchange improvements, will result in impacts to the adjacent FEMA floodplains. The approximate location of floodplains along the project corridor are provided in the Location Hydraulics Report (April 2021) developed for this project. The anticipated area of floodplain impact due to the proposed roadway widening was estimated to be approximately 78.4 acres; however, the impact volume from the proposed widening and necessary compensation will need to be assessed during the design phase, when survey of the existing ground, geotechnical data for the SHGWT, and proposed cross sections are available. Off-site floodplain compensation sites, on site swales, and infield storage areas should be evaluated to provide compensation for the floodplain impacts.

Floodplain impacts will be mitigated as deemed appropriate by, and in conformance with, South Florida Water Management District regulations. As a result, the Project will not affect existing flood heights or floodplain limits. There will be no significant change in the potential for interruption or termination of emergency service or emergency evacuation routes as a result of construction of this project. Therefore, it has been determined that this encroachment is not significant.

### 2.3.6 Coastal Barrier Resources

No Coastal Barrier Resources are present within the project area. The proposed project will have no effect on these resources.

### 2.3.7 Protected Species and Habitat

Based on the evaluation of collected data, field reviews, Florida Natural Areas Inventory (FNAI) data, and database searches, multiple federal and state protected species were considered as having the potential to occur within or adjacent to the project area. Protected species documented occurrence locations were obtained from project specific requests and publicly available data sources from the United States Fish and Wildlife Service (USFWS), FNAI and Florida Fish and Wildlife Conservation Commission (FWC). For a species to be considered to have a potential to occur, the project area must be within the species' distribution range and potentially suitable habitat must occur. An effect determination was made for each federal and state protected species based on an analysis of the potential impacts of the Preferred Alternative on each species and technical assistance meetings held with the USFWS, NMFS and the FWC.

The project corridor and adjacent areas were evaluated for the presence of federal and/or state protected species and their suitable habitat in accordance with Part 2, Chapter 16 of the PD&E Manual. **Tables 2-1** and **2-2** summarize the effect determinations that have been made for each federal and state protected species based upon their probability ranking and the implementation measures and/or commitments (see **Section 2.3.7.1**) to offset any potential impacts to each species.

**Table 2-1: Federal Protected Species Effect Determination**

Project Effect Determination	Federal Listed Species
"No Effect"	American Alligator ( <i>Alligator mississippiensis</i> )
	Okeechobee Gourd ( <i>Cucurbita okeechobeensis</i> )
"May affect, but is not likely to adversely affect"	Florida Grasshopper Sparrow ( <i>Ammodramus savannarum floridanus</i> )
	Eastern Indigo Snake ( <i>Drymarchon couperi</i> )
	Wood Stork ( <i>Mycteria americana</i> )
	Smalltooth sawfish ( <i>Pristis pectinata</i> )
	West Indian Manatee ( <i>Trichechus manatus</i> )
"May affect"	Florida Scrub-jay ( <i>Aphelocoma coerulescens</i> )
	Crested Caracara ( <i>Caracara cheriway</i> )
	Florida Bonneted Bat ( <i>Eumops floridanus</i> )
	Red-cockaded Woodpecker ( <i>Leuconotopicus borealis</i> )
	Everglade Snail Kite ( <i>Rostrhamus sociabilis plumbeus</i> )

**Table 2-2: State Protected Species Effect Determination**

Project Effect Determination	State Listed Species
"No effect anticipated"	Golden Leather Fern ( <i>Acrostichum aureum</i> )
	Meadow Jointvetch ( <i>Aeschynomene pratensis</i> var. <i>pratensis</i> )
	Many-flowered Grass-Pink ( <i>Calopogon multiflorus</i> )
	Piedmont Joint Grass ( <i>Coelorachis tuberculosa</i> )

	Cutthroat Grass ( <i>Coleataenia abscissa</i> )
	Florida Tree Fern ( <i>Ctenitis abscissa</i> )
	Cublet Fern ( <i>Dennstaedtia bipinnata</i> )
	Night-scented Orchid ( <i>Epidendrum nocturnum</i> )
	Redberry Eugenia ( <i>Eugenia confusa</i> )
	Coastal Vervain ( <i>Glandularia maritima</i> )
	Spreading Pinweed ( <i>Lechea divaricata</i> )
	Celestial Lily ( <i>Nemastylis floridana</i> )
	Giant Sword Fern ( <i>Nephrolepis biserrata</i> )
	Scrub Bluestem ( <i>Schizachyrium niveum</i> )
	Ray Fern ( <i>Schizaea pennula</i> )
	Southern Ladies'-tresses ( <i>Spiranthes torta</i> )
	Banded Wild-pine ( <i>Tillandsia flexuosa</i> )
	Scentless Vanilla ( <i>Vanilla mexicana</i> )
	Redmargin Zephyrlily ( <i>Zephyranthes simpsonii</i> )
<i>"No adverse effect anticipated"</i>	Hand Fern ( <i>Ophioglossum palmatum</i> )
	Toothed Maiden Fern ( <i>Thelypteris serrata</i> )
	Gopher Tortoise ( <i>Gopherus polyphemus</i> )
	Florida Pine Snake ( <i>Pituophis melanoleucus mugitus</i> )
	Florida Sandhill Crane ( <i>Antigone canadensis pratensis</i> )
	Florida Burrowing Owl ( <i>Athene cunicularia floridana</i> )
	Little Blue Heron ( <i>Egretta caerulea</i> )
	Tricolored Heron ( <i>Egretta tricolor</i> )
	Southeastern American Kestrel ( <i>Falco sparverius paulus</i> )
	Roseate Spoonbill ( <i>Platalea ajaja</i> )
	Least Tern ( <i>Sternula antillarum</i> )

### 2.3.7.1 Implementation Measures and Commitments

Based on the field and literature reviews outlined in this report, federal- and state-protected species have the potential to occur within and adjacent to the project area. In order to assure that the proposed project will not adversely impact these species, the FTE will adhere to the following:

#### Implementation Measures

- As determined necessary through agency technical assistance, the FTE will perform surveys for the species discussed in this report and other wildlife species during the project design phase to ascertain the involvement, if any, of protected species. Species specific survey requirements will be considered for, but not limited to, the Florida scrub-jay, crested caracara, red-cockaded woodpecker, Everglade snail kite, and southeastern American kestrel.
- During the design and permitting phases of this project, a Wood Stork Foraging Analysis per USFWS methodology will be conducted to determine the amount of biomass lost from wetland and surface water impacts. Impacts to suitable foraging habitat for the federally protected wood stork will be mitigated

through the purchase of credits from a U.S. Fish and Wildlife Service-approved mitigation bank pursuant to Section 373.4137, F.S. or as otherwise agreed to by the FTE and the appropriate regulatory agencies.

- A full acoustic and roost survey for bats in accordance with current federal regulatory guidance, will be completed prior to permitting to verify activity and occupancy status of the Florida bonneted bat. Currently, the USFWS consultation area for the Florida bonneted bat is located south of the St. Lucie Canal (C-44 Canal).
- As needed, during the design and permitting phases of this project, a general plant survey will be conducted and if any federally or state protected plant species are found within 25 feet of construction limits, coordination will occur with the USFWS/Florida Department of Agriculture and Consumer Services (FDACS) to secure any necessary permits.
- During the design and permitting phase of this project, gopher tortoise surveys will be conducted and if any burrows are found within 25 feet of construction limits, coordination will occur with FWC to secure any necessary permits for gopher tortoises and associated commensal species before construction.
- If a bald eagle nest is identified within 660 feet of the proposed project area, the FTE will reinitiate technical assistance with the USFWS to secure all necessary approvals prior to the start of construction.
- During the design and permitting phases of this project, the FTE will conduct surveys to identify any osprey nests within the project area. If nest removal is deemed necessary, the FTE will remove nest(s) when they are inactive (i.e., without eggs or flightless young).
- The USFWS Standard Protection Measures for the Eastern Indigo Snake will be implemented to assure that the Eastern indigo snake will not be adversely impacted by the project.
- The Standard Manatee Construction Conditions for In-water Work (2011) will be implemented during construction at waterway crossings to prevent adverse impacts to the West Indian manatee.

#### Commitments

- If protected plants are observed during future surveys conducted prior to construction, the FTE will coordinate with FDACS and public parks (e.g., Jonathan Dickinson State Park) prior to construction for possible relocation of protected plants.
- If Florida sandhill crane nests are observed during future surveys conducted prior to construction, then a 400-foot buffer will be implemented if construction occurs during the nesting season (January through July). The FTE will coordinate with the FWC during the project construction phase, if necessary.

#### **2.3.8 Critical Habitat**

The project area was evaluated for the occurrence of Critical Habitat as defined by the Endangered Species Act of 1973, as amended, and 50 CFR Part 424. The USFWS is the authority, as a federal agency, to protect critical habitat from destruction or adverse modification of the biological or physical constituent elements essential to the conservation of listed species. Critical Habitat is defined as the specific areas within the geographical area occupied by a species on which are found those physical or biological features essential to the conservation of the species and which may require special management considerations or protections.

Based on a review of the project area, there is no Critical Habitat for any listed species within the project area. As a result, the project will not result in the destruction or adverse modification of Critical Habitat.

### 2.3.9 *Essential Fish Habitat*

Construction of the proposed improvements has the potential to impact a total of 6.10 acres of designated Essential Fish Habitat (EFH), including 3.49 acres of forested wetlands and 2.61 acres of surface waters. Potential impacts include direct impacts such as fill placement for roadway widening and bridge approaches, pile placement, bridge pier construction and indirect impacts such as shading of vegetated habitats, principally forested wetlands. For a detailed discussion of potential project effects on Essential Fish Habitat, please see the Natural Resources Evaluation (NRE)(May 2021) developed for this project. Potential impacts to EFH, as well as differentiation between direct and indirect impacts, can be further assessed at each waterway crossing as bridge concepts are advanced, and the project proceeds through the design process.

The potential impacts to EFH in the project area have been avoided and minimized to all extent practicable. This has been accomplished by locating the widening and/or replacement of bridge structures as close as possible to existing structures and utilizing existing filled causeways for bridge approaches and roadway to the extent feasible. Stormwater treatment is an essential feature of the proposed project. Surface water runoff from additional impervious areas will be treated to prevent increased water quality degradation as a result of the proposed improvements. Due to the incorporation of stormwater treatment facilities, the proposed project will not result in the degradation of water quality in the identified EFH. Additionally, sedimentation and erosion control measures (i.e., silt fences, turbidity barriers) will be utilized during construction to minimize soil exposure and siltation into the water column, further reducing adverse impacts to EFH.

The proposed impacts to areas of EFH within the Preferred Alternative do not contain any submerged aquatic vegetation and will occur in areas that have been previously disturbed by construction of the existing roadway, culverts, and bridge crossings. Additionally, the EFH within the project area is comprised of the furthest landward extent of the designated EFH and the salinity regimes do not meet the requirements for the majority of the South Atlantic Fish Management Council (SAFMC) managed species. Further, due to the topography of the forested wetland systems, portions of these habitats exist at an elevation that may preclude or substantially limit access and recruitment of life history stages of managed species. As a result, it is anticipated that the proposed project will not have significant direct or indirect adverse impacts on EFH. Based on this information, no populations of SAFMC or Atlantic States Marine Fisheries Commission (ASMFC) managed species with potential to occur within the project area are expected to be adversely affected by the proposed project. The project is anticipated to have a **“more than minimal but less than substantial”** potential for adverse effects on EFH.

## 2.4 **Physical Environment**

The documentation of the existing and proposed conditions and the evaluation of the potential effects to the physical environment are provided in the following support documents completed as part of this study.

- Noise Study Report (NSR) (March 2022)
- Air Quality Technical Memorandum (October 2020)
- Contamination Screening Evaluation Report (CSER) (February 2021)
- Utilities Assessment Package (UAP) (February 2021)



### 2.4.1 Highway Traffic Noise

A traffic noise analysis documented in a Draft PD&E Noise Study Report (NSR) (dated March 2022) was performed in accordance with Title 23, Code of Federal Regulations, Part 772 (23 CFR 772), *Procedures for Abatement of Highway Traffic Noise and Construction Noise* following methodology and procedures established by FDOT in the PD&E Manual, Part 2, Chapter 18. The purpose of the NSR is to identify noise sensitive sites that would be impacted by the Preferred Alternative, evaluate potential abatement measures at impacted noise sensitive sites, and determine where potential noise abatement (i.e., noise barriers) is recommended for further evaluation during the design phase. Predicted noise levels were produced using the FHWA, Traffic Noise Model (TNM), version 2.5.

Noise levels developed for this analysis are expressed in decibels using an “A”-scale (dB(A)) weighting. This scale most closely approximates the response characteristics of the human ear. All noise levels are reported as hourly equivalent noise levels ( $L_{Aeq1h}$ ). The  $L_{Aeq1h}$  is defined as the equivalent steady-state sound level that, in a given hourly period, contains the same acoustic energy as the time-varying sound level for the same hourly period. Use of the dB(A) and  $L_{Aeq1h}$  metrics to evaluate traffic noise is consistent with 23 CFR 772. Noise abatement measures are considered when in conjunction with “impacts”, meaning predicted future year traffic noise levels approach, meet, or exceed the FHWA’s Noise Abatement Criteria (NAC) or when there is a substantial increase (15 dB(A)) in traffic noise levels.

For the year 2045 Build condition, noise levels were predicted at 3,134 receptor points, representing 5,091 residences and 203 special use receptor points. Noise levels at 1,518 residences and 108 non-residential “special land use” sites, are predicted to approach or exceed the NAC for the year 2045 Build Alternative and therefore considered “impacted”. No noise sensitive sites are expected to experience a substantial increase (15 dB(A)) in traffic noise compared to existing conditions.

Analyses were performed of the impacted locations to determine if noise abatement was potentially feasible and reasonable under FDOT policy. The noise barrier analysis performed to date and summarized in **Table 2-3** indicates that noise barriers could potentially provide reasonable and feasible noise abatement for 1,366 of the 1,518 impacted residences, as well as provide a 5 dB(A) noise reduction benefit to 1,493 non-impacted residences. Noise abatement was not determined feasible and reasonable for any of the 108 impacted special use sites; however, some of the special use locations will receive incidental benefits from noise barriers for the residential areas. The results of the noise barrier evaluations where noise abatement was determined to be potentially feasible and reasonable are summarized by noise sensitive area in **Table 2-3**.

The PD&E study phase analysis indicates that noise barriers are potentially feasible and reasonable at 14 noise sensitive areas. These noise barriers may benefit 1,366 residences with predicted noise levels that approach or exceed the NAC. **Table 2-3** shows the 14 noise sensitive areas where preliminary noise barriers were determined to be potentially feasible and reasonable. The potentially feasible and reasonable noise barriers meet the Florida Department of Transportation’s (FDOT) cost reasonableness criterion with a preliminary cost of under the \$42,000 per benefited receptor criterion. Noise barriers at these 14 locations will be given further consideration during the Design phase of this project. The dimensions of noise walls are subject to change during the Design phase of the project. Furthermore, it should be noted that as part of the conceptual PD&E assessment process, several noise wall locations appear to have engineering constraints that may render them non-constructible or which could result in them not being

cost-reasonable. While these constraints will be assessed with greater scrutiny in future design projects, an effort was made to identify those walls that may have such potential constraints in the NSR.

### **Statement of Likelihood**

FTE is committed to the construction of feasible and reasonable noise abatement measures. Fourteen potentially feasible and reasonable noise barrier systems have been identified for this project (see **Table 2-3** for more detail on the noise barriers) contingent upon the following conditions:

- Final recommendations on the construction of abatement measures are determined during the project's final design and through the public involvement process;
- Detailed noise analyses during the final design process support the need, feasibility and reasonableness of providing abatement;
- Cost analysis indicates that the cost of the noise barrier(s) will not exceed the cost reasonable criterion;
- Community input supporting types, heights, and locations of the noise barrier(s) is provided to FTE; and
- Safety and engineering aspects have been reviewed and any conflicts or issues resolved.

A land use review will be performed during the design phase to identify all noise sensitive sites that may have received a building permit subsequent to the noise study but prior to the project's Date of Public Knowledge. The date that the State Environmental Impact Report is approved by FTE will be the Date of Public Knowledge. If the review identifies noise sensitive sites that have been permitted prior to the Date of Public Knowledge, then those sensitive sites will be evaluated for traffic noise impacts and abatement considerations.

#### **2.4.2 Air Quality**

The proposed project is located in the counties of Palm Beach, Martin, and St. Lucie, an area currently designated as being in attainment for particulate matter (2.5 and 10 microns in size) and carbon monoxide (CO). As such, the State Implementation Plan conformity requirements of the Clean Air Act are not applicable to these two pollutants.

The project alternatives (No-build and Build) were subjected to the CO screening model entitled CO Florida 2012 that makes various worst-case assumptions related to site conditions, meteorology, and traffic. The FDOT's CO Florida 2012 model uses USEPA software to produce estimates of one-hour and eight-hour CO at default air quality receptor locations. The one- and eight-hour estimates can be directly compared to the National Ambient Air Quality Standards (NAAQS) for CO.

In the project's design year (2045) with the No-build Alternative, the intersection with a combination of the highest intersection leg approach volume and delay is the Okeechobee Road (SR 70) and S Kings Highway intersection (i.e., turnpike ramps). In the design year with the Build Alternative, the intersection with a combination of the highest intersection leg approach volume and delay is the SW Martin Highway and SW High Meadow Avenue intersection (an intersection east of the turnpike ramps). Both the No-build and Build Alternatives were evaluated for both intersections.



**Table 2-3: Potentially Feasible and Reasonable Noise Barrier Evaluation Summary**

Noise Sensitive Area	Number of Impacted Residences	Noise Barrier Approx. Begin Station	Noise Barrier Approx. End Station	Preliminary Noise Barrier Height (ft.)	Preliminary Noise Barrier Length (ft.) <sup>1</sup>	Preliminary Noise Barrier Location	Preliminary Noise Barrier Cost <sup>2</sup>	Number of Residences Potentially Benefited by a Noise Barrier <sup>3</sup>		Cost Per Benefited Residence
								Impacted	Total	
<b>NOISE BARRIERS NORTHBOUND SIDE OF TURNPIKE</b>										
Hammock Creek & Highlands Reserve (CNE NB05)	73	841+80	931+80	14	9,000	SH	\$3,780,000	57	144	\$26,250
Coquina Cove Apartments and Martin Downs Country Club Residences (CNE NB06)	67	994+20	1025+20	22	3,100	ROW	\$2,550,000	67	187	\$13,636
		1023+00	1035+00	14	1,200	SH				
Copperleaf (CNE NB07)	25	1109+80	1138+80	14	2,900	SH	\$1,218,000	25	50	\$24,360
Jessica Clinton Park-Port St. Lucie Section 39 (CNE NB08)	77	1285+00	1335+00	14	5,000	SH	\$2,100,000	77	133	\$15,789
Osprey Ridge & Port St. Lucie Section 18 (CNE NB09)	71	1412+40	1419+80	22	900	ROW	\$2,362,800	71	97	\$24,359
		1385+20	1413+40	14	2,840	SH				
		1370+00	1382+20	14	1,200	SH				
		1382+20	1385+20	8	300	SH				
River Park and Cove at St. Lucie (CNE NB12)	280	1603+70	1713+50	14	10,980	SH	\$4,611,600	280	509	\$9,060
St. James Golf Club and Monoco Court residences (CNE NB13, NB14, and NB15)	101	1719+20	1796+00	14	7,700	SH	\$3,234,000	101	331	\$9,770

**Table 2-3: Potentially Feasible and Reasonable Noise Barrier Evaluation Summary**

Noise Sensitive Area	Number of Impacted Residences	Noise Barrier Approx. Begin Station	Noise Barrier Approx. End Station	Preliminary Noise Barrier Height (ft.)	Preliminary Noise Barrier Length (ft.) <sup>1</sup>	Preliminary Noise Barrier Location	Preliminary Noise Barrier Cost <sup>2</sup>	Number of Residences Potentially Benefited by a Noise Barrier <sup>3</sup>		Cost Per Benefited Residence
								Impacted	Total	
<b>NOISE BARRIERS SOUTHBOUND SIDE OF TURNPIKE</b>										
Wildwood Estates & Sunshine Parkway Manor (SB05)	48	742+00	774+40	22	3,350	ROW	\$2,211,000	47	64	\$34,547
Port St. Lucie Section 34, Port St. Lucie Section 36, Port St. Lucie Section 37, Port St. Lucie Section 41 and Windmill Point (CNE SB10)	154	1290+60	1382+20	22	9,140	ROW	\$13,153,200	154	432	\$30,447
		1184+20	1249+40	22	6,540	ROW				
		1251+60	1275+60	22	2,400	ROW				
		1277+60	1287+40	22	980	ROW				
		1378+90	1387+90	8	900	SH				
		1286+10	1291+70	8	560	SH				
		1248+10	1252+90	8	480	SH				
1274+40	1278+90	8	450	SH						
Port St. Lucie – Section 5 (CNE SB 11)	48	1386+30	1422+30	22	3,600	ROW	\$2,376,000	45	66	\$36,000
Port St. Lucie – Section 9 (CNE SB12 & CNE SB13)	97	1447+00	1516+50	22	7,280	ROW	\$4,804,800	96	172	\$27,935
Lake Forest (CNE SB14)	93	1542+00	1595+20	22	5,390	ROW	\$3,557,400	93	207	\$17,186
Magnolia Lakes, Palms of St. Lucie West and Paradise Villas (CNE SB15)	104	1617+70	1704+90	22	8,720	ROW	\$5,755,200	88	178	\$32,333
Vizcaya Falls & Winterlakes (CNE SB16 & CNE SB17)	183	1726+60	1789+70	22	6,260	ROW	\$4,131,600	165	289	\$14,296

1 Full height is for length indicated. The length for any required taper in height at a shoulder noise barrier termination would be in addition to the length indicated.  
 2 Unit cost of \$30/ft2 for all non-shoulder noise barriers.  
 3 Total includes impacted/benefited residences and residences with a predicted noise level that does not approach or exceed 67 dBA but are incidentally benefited.  
 SH = Shoulder Noise Barrier  
 ROW = Right-of-way Line Noise Barrier

Estimates of CO were predicted for the default receptors which are located at distances from 10 to 150 feet from the edge of the roadway. Based on the results from CO Florida 2012, the highest project-related CO levels are not predicted to meet or exceed the NAAQS. As such, the project “passes” the screening model. Additionally, the project is expected to generally improve the level of service which would reduce overall delay and congestion within the project area.

This project has not been linked with any special mobile source air toxic (MSAT) concerns but would add substantial new capacity (from the existing four lane facility to an improved eight lane facility). However, the design year average daily traffic (AADT) is projected to be less than 140,000 on all roadway segments. Therefore, following the MSAT evaluation procedures described in Part 2, Chapter 19 of the FDOT PD&E Manual, the project has a low potential for MSAT effects and only a qualitative evaluation of MSATs is required.

The project is not forecasted to change the volume of motor vehicles or the vehicle-miles-traveled (VMT) within any of the evaluated segments by more than 11 percent and, when considering the total VMT, would result in an approximate two percent reduction in the VMT within the project corridor which would correspondingly decrease MSATs.

Moreover, USEPA regulations for vehicle engines and fuels will result in a decline in MSAT emissions over the next several decades. Based on regulations now in effect, an analysis of national trends with USEPA’s MOVES2014 model forecasts a combined reduction of over 90 percent in the total annual emissions rate for the priority MSATs from 2010 to 2050, in which vehicle-miles of travel are projected to increase by over 45 percent (Updated Interim Guidance on Mobile Source Air Toxics Analysis in NEPA [National Environmental Policy Act] Documents, FHWA, October 2016). The reduction in MSATs with the project, along with the reduction in emissions due to the regulations, will result in lower future background levels of the pollutants.

### 2.4.3 Contamination

A Level I contamination evaluation was conducted for areas within 500 feet of the project corridor and a *Contamination Screening Evaluation Report (CSER)* (February 2021) was prepared. Based on the Level I screening evaluation, a total of 108 mainline and 38 alternative pond site potential contamination sites were identified within the project limits. **Tables 2-4** and **2-5** present a summary of the risk ratings assigned for each potential contamination site/facility for the mainline and alternative pond sites, respectively. **Table 2-6** provides details of the medium and high potential contamination sites for the mainline roadway and alternative pond sites. Please see **Appendix C – Preferred Alternative Conceptual Plans** for the locations of Medium and High ranked contamination sites for the mainline roadway.

**Table 2-4: Summary of Risk Ratings - Mainline**

Number of Mainline Sites per Risk Rating			
High	Medium	Low	No
5	19	65	19

**Table 2-5: Summary of Risk Ratings –  
Alternative Pond Sites**

<b>Number of Pond Sites per Risk Rating</b>			
<b>High</b>	<b>Medium</b>	<b>Low</b>	<b>No</b>
0	3	10	25

Based on the conclusions of this study and risk ratings noted above, the following recommendations are made.

- During the design phase, a Level II Impact to Construction Assessment will be conducted for sites with a potential contamination risk rating of medium or high, based on proposed right-of-way acquisition and design construction plans.
- Asbestos testing results will be obtained for building structures to be demolished or renovated in conjunction with the project.

#### **2.4.4 Utilities and Railroads**

##### Utilities

Existing utility facilities along the project area include power, gas, electric, fiber optics, water, sewer, and communications. A preliminary plan set with aerial background was sent to all Utility Agency Owners (UAOs), identified by Sunshine 811 as having utility facilities in the project area, for their use in identifying their facilities. Refer to the *Utilities Assessment Package*, February 2021, for a description of the existing utilities within and adjacent to the project area. **Table 2-7** provides a summary of the existing utility agency owners and their respective utility types.

Conservative utility facility relocation estimates were requested as part of the utility coordination process for each UAO. The total combined estimated cost for relocations is \$46,300,000. **Table 2-8** provides a summary of the anticipated utility impacts and their associated costs. Only impacted facilities are listed in the table below.

It is anticipated the municipal water and sewer providers (City of Port St. Lucie, Martin County, and Ft. Pierce Utility Authority) may request a Utility Work by Highway Contractor Agreement (UWHCA), as demonstrated on past projects of similar scope.

**Table 2-6: High and Medium Rated Contamination Sites**

<b>Appendix C Sheet Number</b>	<b>Site Name &amp; Address</b>	<b>Databases/Facility ID</b>	<b>Approximate Distance from Turnpike ROW (unless otherwise noted)</b>	<b>Contaminants of Concern</b>	<b>Risk Rating</b>
C-81 through C-84	Groves/Crops (including former) No address	N/A	Within ROW, East and West of ROW	Pesticides, herbicides, arsenic	Medium
C-24	Worldwide Dedicated Services Florida Turnpike MM 128.2 Stuart, FL	VOLCLNUP 180179	Within ROW (east side of NB lane)	Petroleum	High
C-28	Former Marathon Plaza 95/ 7 Eleven #41164 8100 Jack James Dr. Stuart, FL	TANKS 9800857 TANKS 9800990 (use 9800857)	440 feet east; (tank farm 15 feet south of Kanner Highway)	Petroleum	Medium
C-28	Mobil/Sunshine #821 8062 Jack James Dr. Stuart, FL	TANKS 9812553	550 feet east (tank farm 30 feet north of Kanner Highway)	Petroleum	Medium
C-39	FDOT Turnpike MM 133 Martin County Yard/ Stuart Maintenance Yard Turnpike Interchange Stuart, FL	LUST/TANKS 8626159	Within ROW	Petroleum	Medium
C-37	Mobil Martin Downs 3551 SW Martin Highway Palm City, FL	LUST/TANKS 9201330	Adjoining north	Petroleum	Medium
C-37	Palm City Auto Lube/ Highway Enterprise Inc. 3584 Armellini Ave. Palm City, FL	LUST/TANKS/STCERC 8945557NONTSD FLR000078402	Adjoining north	Petroleum	Medium

C-37	Como Oil Co. of FL/ Martin County Petroleum 3586 SW Martin Hwy Stuart, FL	LUST/TANKS/STCER 8511487 NONTSD FLT010069151	Adjoining north of SW Martin Hwy/Adjoining east of Florida's Turnpike ROW MW-7 is 70 feet east of Turnpike ROW, MW-7 is 130 feet north of SW Martin Hwy ROW	Petroleum	Medium
C-38	Exxon/Midnight Farms 3590 SW Deggeller Ct. Palm City, FL	LUST/TANKS 8511426 NONTSD FLR000061002	Adjoining north of SW Martin Hwy 700 feet west of Florida's Turnpike	Petroleum	Medium
C-41	Martin Downs Country Club Inc. 4300 SW Mallard Creek Trail Palm City, FL	WASTE CLEANUP 157657 TANKS 8630063 NONTSD FLD981920986	Golf course and maintenance facility are 50 east; Tank farm 220 feet east of ROW	Petroleum, herbicides, pesticides, arsenic	Medium
C-52	SPILL Florida's Turnpike South of MM 140.5 STA 1285 East side of NB lane Port St. Lucie, FL	NA	Within ROW	Petroleum	Medium
C-58	BP/Coco Vista Macmillan 468 Port St. Lucie Blvd Port St. Lucie, FL	TANKS 9808703	Adjoining south	Petroleum	Medium
C-58	Sunoco (Former Mobil) Expert Auto (Former Kwons Service Center) 461/471 Port St. Lucie Blvd Port St. Lucie, FL	LUST/STCERC/TANKS 8516222 ERNS 173593	Adjoining north of Port St. Lucie Blvd. 900 feet west of Florida's Turnpike	Petroleum	Medium
C-57	Turnpike Substation 2300 Bayshore Blvd Port St. Lucie, FL	ERNS 933786	Within ROW	Petroleum, PCBs	Medium
C-59	Port St. Lucie Shell/ Former Chevron 299 St. Lucie Blvd Port St. Lucie, FL	LUST/TANKS 9063940 NONTSD FLD984210195	Adjoining north of St. Lucie Blvd 100 feet east of Florida's Turnpike	Petroleum	Medium
C-63	Fort Pierce Service Plaza MM 145 Port St. Lucie, FL	ERNS 1218682	Within ROW	Petroleum	Medium

C-77	St. Lucie Recycling 5950 Glades Cutoff Rd. Ft. Pierce, FL	SLDWST_LF 101649	Adjoining east	Solid waste	Medium
C-78 & C-79	St. Lucie County Glades Rd Landfill/St. Lucy Co Bailing and Recycling Facility 6120 Glades Cutoff Rd. Ft. Pierce, FL	SLDWST 70652	Adjoining west	Ammonia, chloride, sodium, iron, TDS, benzene	High
C-81	Toll Plaza at Ft. Pierce/ FDOT Turnpike MM 152 Ft. Pierce, FL	TANKS 9400360 TANKS 9602391	Within ROW	Petroleum	Medium
C-84	Love's Travel Stop/ Pilot Travel Center 7150 Okeechobee Road Ft. Pierce, FL	LUST/STCERC/TANKS 9701235 9701238 (use 9701235) VOLCLNUP 272775	Adjoining east of the Okeechobee Rd project limit, Tank farm 120 feet east 1,100 feet east of Florida's Turnpike	Petroleum	Medium
C-84	Boudrias Groves 2898 Kings Hwy Ft. Pierce, FL	LUST/STCERC/TANKS 8516061	Adjoining Okeechobee Rd ROW 800 feet east of Florida's Turnpike	Petroleum	High
C-84	Florida Department of Transportation Highway 70 & Kings Hwy (NW corner) Ft. Pierce, FL	LUST/STCERC/TANKS 9102669	Within Kings Hwy ROW, adjoining Okeechobee Road ROW, and 600 feet east of Florida's Turnpike ROW	Petroleum	High
C-82	Pilot Travel Center #090 7300 Okeechobee Road Ft. Pierce, FL	LUST/STCERC/TANKS 9802058	Adjoining north of the Okeechobee Rd ROW 270 feet east of Florida's Turnpike	Petroleum	High
C-75	Townstar #481 6600 W Midway Road Port St. Lucie, FL	LUST/STCERC/TANKS 8942900	Adjoining west of the Midway Rd project limit, Tank farm 120 feet west 0.45 miles west of Florida's Turnpike	Petroleum	Medium

SMF 38-3 (including access easement)	Site #109: Groves/Row Crops (including former)	No Facility ID	04. miles west of Florida's Turnpike, 0.1 mile east of SW Leighton Farm Avenue, and 1.8 miles south of SW Martin Highway	Petroleum, herbicides, pesticides, heavy metals	Medium
	Site #22: Hammock Creek Golf Club, 2400 SW Golden Bear Way, Palm City,	No Facility ID		Herbicides, pesticides, petroleum,	
SMF 39-1 (including access easement)	Open field/residential	No Facility ID	West of SW Leighton Farm Avenue, north of SW Honey Terrace, 0.15 miles west of Florida's Turnpike, 0.7 miles south of SW Martin Highway	Herbicides, pesticides, petroleum	Medium
SMF 39-2	Site #111: Groves/Row Crops (including former)	No Facility ID	West of Florida's Turnpike, east of SW Leighton Farm Avenue, 0.5 miles south of SW Martin Highway	Herbicides, pesticides, heavy metals, petroleum	Medium



**Table 2-7: Utility Agency Owners**

Utility Agency Owner	Contact	Utility Type
<p><b>AT&amp;T Corp.</b> 6000 Metro West Blvd, Suite 201 Orlando, FL 32835</p>	<p>c/o PEA, Inc. Stefan Eriksson 407-578-8000 seriksson@pea-inc.net</p>	<p>Communications</p>
<p><b>AT&amp;T Florida</b> 7747 Ellis Road West Melbourne, FL 32904</p> <p>120 North K Street Lake Worth, FL 33460</p>	<p>Luke Folkerts 407-496-6041 LF2490@att.com</p> <p>Garth Bedward 561-504-9263 GB7410@att.com</p>	<p>Communications</p>
<p><b>City of Port St. Lucie</b> 900 SE Ogden Lane Port St. Lucie, FL 34983</p>	<p>Laney Southerly 772-873-6400 lsoutherly@cityofpsl.com</p>	<p>Water, Sewer and Fiber</p>
<p><b>Comcast</b> 3960 RCA Blvd, Suite 6002 Palm Beach Gardens, FL 33410</p> <p>1495 NW Britt Road Stuart, FL 34994</p>	<p>Miya Fisher 561-818-6633 miya_fisher@comcast.com</p> <p>Rick Johnson 772-692-9010 rick_johnson@comcast.com</p>	<p>Communications</p>
<p><b>Crown Castle</b> 1601 NW 136 Ave, Suite A-200 Sunrise, FL 33323</p>	<p>Danny Haskett 786-610-7073 danny.hasket@crowncastle.com</p>	<p>Communications</p>
<p><b>Deltacom</b> 1530 Delta Com Drive Anniston, AL 36207</p>	<p>John McGuffey 256-241-6438 John.mcguffey@elink.com</p>	<p>Communications</p>
<p><b>Florida City Gas</b> 4045 NW 97 Ave Doral, FL 33178</p>	<p>Maria Paula Lopez 786-332-8913 maria.lopez@nexteraenergy.com</p>	<p>Gas</p>
<p><b>Florida Gas Transmission</b> 2405 Lucien Way, Suite 200 Maitland, FL 32751</p>	<p>Joseph E. Sanchez 407-838-7171 Joseph.E.Sanchez@energytransfer.com</p>	<p>Gas Pipeline</p>
<p><b>Ft. Pierce Utility Authority (FPUA) Water &amp; Sewer</b> 1701 South 37 Street Fort Pierce, FL 34947</p>	<p>James Carnes 772-466-1600 jcarnes@fpua.com</p>	<p>Water &amp; Sewer</p>
<p><b>Ft. Pierce Utility Authority Electric</b> 1701 South 37 Street Fort Pierce, FL 34947</p>	<p>Jason Mittler 772-466-1600 jmittler@fpua.com</p>	<p>Electric</p>
<p><b>Ft. Pierce Utility Authority Fiber</b> 1701 South 37 Street Fort Pierce, FL 34947</p>	<p>Jason Mittler 772-466-1600 jmittler@fpua.com</p>	<p>Communications</p>
<p><b>Ft. Pierce Utility Authority Gas</b> 1701 South 37 Street Fort Pierce, FL 34947</p>	<p>Jason Mittler 772-466-1600 jmittler@fpua.com</p>	<p>Gas</p>
<p><b>FPL Distribution</b> 4406 SW Cargo Way Palm City, FL 34990</p> <p>15430 Endeavor Drive Jupiter, FL 33478</p>	<p>Rob Morris 772-223-4215 rob.morris@fpl.com</p> <p>Reynoldo Calzadilla 321-214-3848 reynoldo.calzadilla@fpl.com</p>	<p>Electric</p>
<p><b>FPL Transmission</b> 15430 Endeavor Drive Jupiter, FL 33478</p>	<p>Tricia D'Annunzio 561-904-3560 tricia.dannunzio@fpl.com</p>	<p>Electric</p>

Utility Agency Owner	Contact	Utility Type
<b>Hometown Communications (Blue Stream Fiber)</b> 1982 SW Hayworth Ave Port St. Lucie, FL 34987	Steve Lencse 954-752-7244 slencse@mybluestream.com	Communications
<b>Hotwire Communications</b> 10360 USA Today Way Miramar, FL 33025	Walter Sancho-Davila 954-699-0900 walter.sancho-davila@hotwirecommunication.com	Communications
<b>ITS Fiber (Indiantown Telephone)</b> 15925 SW Warfield Blvd Indiantown, FL 34956	Eddie Richeson 229-507-1308 eddier@itsfiber.net	Communications
<b>Martin County Utilities</b> 2378 SE Ocean Blvd Stuart, FL 34995	Steve Vandersluis 772-221-1437 svaners@martin.fl.us	Water & Sewer
<b>St. Lucie County Utilities</b> 2300 Virginia Ave Ft Pierce, FL 34982	Raymond Murankus 772-462-5221 murankusr@stlucieco.org	Water & Sewer
<b>St. Lucie West Services District</b> 450 SW Utility Drive Port St. Lucie, FL 34986	Joshua Miller 772-340-0220 jmillier@slwsd.org	Water
<b>TECO Peoples Gas</b> 5101 NW 21 Ave, Suite 460 Ft Lauderdale, FL 33309	Max Chamorro 954-453-0812 mjchamorro@tecoenergy.com	Gas
<b>Town of Jupiter</b> 17403 Central Blvd. Jupiter, FL 33458	Amanda Barnes 561-741-2537 amandab@jupiter.fl.us	Water
<b>Tropicana Products Douglas Pipeline</b> Pittsburgh, PA 15234	c/o Douglas Pipeline, Andi Shacklett 412-531-2440 ashacklett@douglaspipeline.com	Pipeline

**Table 2-8: Utility Build Impacts and Cost Estimates**

Utility Agency Owner	Utility Type	Station	General Location	Size	Impact	Estimated Cost
<b>Turnpike Mainline</b>						
AT&T Corp.	BT	3625+00 to 3696+50	Center of TPK	2-2"	Horizontal alignment shift	Reimbursement not anticipated
AT&T Corp.	BT	100+00 to 2014+00	Center of TPK	2-2"	Horizontal alignment shift	Reimbursement not anticipated
AT&T Florida	OFO	1157+25 to 1174+75	East R/W	#216	Not anticipated	\$0
AT&T Florida	BFO	1174+75 to 1179+80	East R/W	#216	Not anticipated	\$0
AT&T Florida	BT	1535+75	Crossing TPK	100 PR Copper	Not anticipated	\$0
AT&T Florida	BT	1535+75 to 1549+00	West side of Service Plaza	100 PR Copper	Not anticipated	\$0

AT&T Florida	OT	1933+70	West R/W	Cable	Not anticipated	\$0
AT&T Florida	BFO	1945+50	East R/W	50 PR Copper	Not anticipated	\$0
AT&T Florida	BFO	1945+50 to 1958+00	East R/W	50 PR Copper	Ramp widening	\$100,000
City of Port St. Lucie	FM	1275+00	Crossing TPK	30" HDPE	Not anticipated	\$0
City of Port St. Lucie	Reclaimed WM	1275+20	Crossing TPK	30" HDPE	Not anticipated	\$0
City of Port St. Lucie	Fiber	1277+40	Crossing TPK	1-2"	Not anticipated	\$0
City of Port St. Lucie	FM	1289+30	Crossing TPK	8"	Not anticipated	\$0
City of Port St. Lucie	WM	1289+50	Crossing TPK	24"	Not anticipated	\$0
City of Port St. Lucie	WM	1470+10	Crossing TPK	16"	Not anticipated	\$0
City of Port St. Lucie	FM	1477+00	Crossing TPK	12"	Not anticipated	\$0
City of Port St. Lucie	FM	1537+70	Crossing TPK	6" Private (TPK)	Not anticipated	\$0
City of Port St. Lucie	WM	1545+60 (SW South Macedo)	East R/W	6" to Meter assembly	Not anticipated	\$0
City of Port St. Lucie	FM	1673+40	Crossing TPK	30"	Widening	\$100,000
City of Port St. Lucie	Fiber	1673+35	Crossing TPK	1-2"	Widening	\$100,000
City of Port St. Lucie	WM	1698+00	Crossing TPK	12"	Widening	\$100,000
City of Port St. Lucie	Concentrate Main	1698+10	Crossing TPK	16"	Widening	\$100,000
City of Port St. Lucie	FM	1698+20	Crossing TPK	12"	Widening	\$100,000
City of Port St. Lucie	WM	1753+00	Crossing TPK	12"	Widening	\$100,000
Crown Castle	OFO	512+00	Crossing TPK	Aerial FOC	Not anticipated	\$0
Crown Castle	OFO	1718+20	Crossing TPK	Aerial FOC	Not anticipated	\$0
Florida City Gas	GM	1595+90	Crossing TPK	8" Steel	Not anticipated	\$0

Florida City Gas	GM	1595+90 to 1698+20	West R/W	8" Steel	Not anticipated	\$0
Florida Gas Transmission	GM	3625+00 to 3696+50	East R/W	18", 24" & 30"	Widening	\$18,000,000
Florida Gas Transmission	GM	96+50 to 1980+00	East R/W	18", 24" & 30"	Widening	\$18,000,000
FPUA Fiber	Fiber	1810+45 L to 1960+23 L	West R/W	1-4"	Widening	Reimbursement not anticipated
FPUA Water & Sewer	FM	1876+00	Crossing TPK	6" HDPE	Not anticipated	\$0
FPL Distribution	UE	3635+40	Crossing TPK	2-6" 23kv	Not anticipated	\$0
FPL Distribution	OE	174+75	Crossing TPK	1-1/OT 23kv	Not anticipated	\$0
FPL Distribution	OE	731+80 R to 761+50 R	East R/W	568T 23kv	Not anticipated	\$0
FPL Distribution	OE	838+00 R to 861+90 R	East R/W	1-1/OT 7.6kv	Not anticipated	\$0
FPL Distribution	OE	886+00 L to 998+80 L	West R/W	1-1/OT 13kv	Not anticipated	\$0
FPL Distribution	OE	1014+00	Crossing TPK	3-3/OT 13kv	Not anticipated	\$0
FPL Distribution	OE	1000+00 R to 1179+50 R	East R/W	3-3/OT 23kv	Not anticipated	\$0
FPL Distribution	UE	1288+30	Crossing TPK	3-6" PVC Only	Not anticipated	\$0
FPL Distribution	OE	1334+80	Crossing TPK	3-3/OT 23kv	Not anticipated	\$0
FPL Distribution	OE	1329+50 R to 1422+50 R	East R/W	3-3/OT 23kv	Not anticipated	\$0
FPL Distribution	UE	1537+20	Crossing TPK	3-1000A XPE	Not anticipated	\$0
FPL Distribution	OE	1545+00	Crossing TPK	3-568T 23kv	Not anticipated	\$0
FPL Distribution	OE	1551+00	Crossing TPK	3-568T 23kv	Not anticipated	\$0
FPL Distribution	OE	1530+50 L to 1562+50 L	West R/W	3-1/OT 23kv	Not anticipated	\$0
FPL Distribution	UE	1811+00	Crossing TPK	3-1000A XPE	Bridge construction	\$200,000
FPL Distribution	OE	1813+00 R to 1820+00	East R/W	3-568T 23kv	Not anticipated	\$0
FPL Distribution	OE	1814+00 L to 1825+70 L	West R/W	1-1/OT 7.6kv	Ramp construction	\$200,000

FPL Distribution	OE	1833+75	Crossing TPK	3-568T 23kv	Bridge construction	\$200,000
FPL Distribution	UE	1841+00	Crossing TPK	3-1000A XPE	Not anticipated	\$0
FPL Distribution	OE	1988+00	Crossing TPK	3-568T 23kv	Not anticipated	\$0
FPL Distribution	OE	1991+00	Crossing TPK	3-568T 23kv	Not anticipated	\$0
FPL Distribution	OE	1885+50 R to 1997+25 R	East R/W	3-568T 23kv	Not anticipated	\$0
FPL Distribution	OE	1889+50 L to 1996L	West R/W	3-568T 23kv	Ramp construction	\$100,000
FPL Transmission	OE	512+00	Crossing TPK	230kv	Not anticipated	\$0
FPL Transmission	OE	667+80	Crossing TPK	230kv	Not anticipated	\$0
FPL Transmission	OE	935+00 R to 962+50 R	East R/W	230kv	Not anticipated	\$0
FPL Transmission	OE	962+50	Crossing TPK	230kv	Not anticipated	\$0
FPL Transmission	OE	963+00 L to 998+20 L	West R/W	230kv	Widening and ramp improvements	\$3,000,000
FPL Transmission	OE	998+20	Crossing TPK	230kv	Not anticipated	\$0
FPL Transmission	OE	997+30 R to 1423+00 R	East R/W	230kv	Bridge and interchange improvements	\$1,800,000
FPL Transmission	OE	1432+00 R to 1441+20 R	East R/W	230kv	Not anticipated	\$0
FPL Transmission	OE	1452+00 R to 1715+00 R	East R/W	230kv	Bridge construction	\$500,000
FPL Transmission	OE	1715+00 to 1718+50	Crossing TPK	(4) 230kv	Not anticipated	\$0
FPL Transmission	OE	1833+90	Crossing TPK	230kv	Bridge construction	\$500,000
FPL Transmission	OE	1841+75 to 1843+00	Crossing TPK	(3) 138kv	Not anticipated	\$0
Town of Jupiter	WM	3634+00	Crossing TPK	24" HPE, 18" HDPE	Not anticipated	\$0
Martin County	WM	721+50	Crossing TPK	12" in 24" casing	Widening	Reimbursement not anticipated
Martin County	RWM	721+50	Crossing TPK	12" in 24" casing	Widening	Reimbursement not anticipated

Martin County	RWM (IQ)	721+50	Crossing TPK	8" in 18" casing	Widening	Reimbursement not anticipated
Martin County	WM	724+00	Crossing TPK	24" HDPE	Not anticipated	\$0
Martin County	FM	724+00	Crossing TPK	20" HDPE	Not anticipated	\$0
Martin County	FM	724+00	Crossing TPK	16" HDPE	Not anticipated	\$0
Martin County	RWM	724+00	Crossing TPK	18" HDPE	Not anticipated	\$0
Martin County	FM	788+50	Crossing TPK	6" PVC	Widening	Reimbursement not anticipated
Martin County	WM	788+50	Crossing TPK	8" PVC	Widening	Reimbursement not anticipated
Martin County	FM	1036+00	Crossing TPK	6" PVC	Widening	Reimbursement not anticipated
Martin County	WM	1036+00	Crossing TPK	12" in 24" casing	Widening	Reimbursement not anticipated
Martin County	WM	1052+50	Crossing TPK	12" in 20" casing	Widening	Reimbursement not anticipated
<b>SE Bridge Road (SR 708)</b>						
AT&T Florida	BT	499+00	SE Bridge Rd south side	1-4" PVC	Not anticipated	\$0
AT&T Florida	BT	499+00	SE Bridge Rd south side	1-100 PR	Not anticipated	\$0
AT&T Florida	BT	499+00	SE Bridge Rd south side	48 FOC	Not anticipated	\$0
AT&T Florida	BT	498+90	SE Bridge Rd south side	1-4"	Not anticipated	\$0
AT&T Florida	BFO	499+00	SE Bridge Rd south side	#48	Not anticipated	\$0
AT&T Florida	BT	499+10	SE Bridge Rd south side	100 PR Copper	Not anticipated	\$0
FPL Distribution	OE	498+75	South side crossing TPK	3-3/OT 23kv	Not anticipated	\$0
<b>Kanner Highway (SR 76)</b>						
FPL Distribution	OE	739+25	South side crossing TPK	13kv	Bridge Construction	\$100,000
AT&T Florida	BT	739+80	South side crossing TPK	9-4"	Not anticipated	\$0

Comcast	Aerial	739+00	South side crossing TPK	Fiber	Not anticipated	\$0
Comcast	Fiber	740+00	East side of TPK to east	1-4"	Not anticipated	\$0
Crown Castle	BFO	739+50	South side crossing TPK	4-1.5" HDPE	Not anticipated	\$0
<b>SW Martin Highway (SR 714)</b>						
ITS Fiber (Indiantown)	BT	981+20	South side crossing TPK	N/A	Widening	Reimbursement not anticipated
FPL Distribution	OE	981+20	South side crossing TPK	3-568T	Widening	Reimbursement not anticipated
TECO	GM	981+00	South side crossing TPK	6"	Widening	Reimbursement not anticipated
AT&T Florida	BT	983+50	North side crossing TPK	8-4"	Not anticipated	\$0
Comcast	Aerial	981+40	South side crossing TPK	N/A	Widening	Reimbursement not anticipated
Crown Castle	BFO	983+50	North side crossing TPK	2-1.5" HDPE	Not anticipated	\$0
<b>SE Becker Road</b>						
FPL Distribution	UE	1182+20	South side crossing TPK	3-3/OT 23kv		
AT&T Florida	BFO	1179+80 R	East R/W	#216	Not anticipated	\$0
AT&T Florida	BFO	1182+10	North side crossing TPK	(2) # 216, (1) # 48 & 100 PR Copper	Widening	\$200,000
Florida City Gas	GM	1180+50	South side crossing TPK	6"	Not anticipated	\$0
City of Port St. Lucie	Fiber	1180+20	South side east of TPK	1-2"	Not anticipated	\$0
City of Port St. Lucie	Fiber	1180+20	North side crossing TPK	1-2"	Widening	Reimbursement not anticipated
City of Port St. Lucie	FM	1180+20	South side crossing TPK	20"	Not anticipated	\$0
City of Port St. Lucie	WM	1180+20	North side crossing TPK	20"	Widening	Reimbursement not anticipated
<b>Port St. Lucie Boulevard (SR 716)</b>						
FPL Distribution	OE	1422+00	South side crossing TPK	3-568T 23kv	Widening	\$200,000
FPL Transmission	OE	1426+00 (Bayshore Blvd)	East R/W	230kv	Widening	\$600,000

FPL Transmission	UE	1426+00 to 1480+00 (Bayshore Blvd)	East R/W	230kv	Widening	\$600,000
AT&T Florida	BFO	1426+20	South side crossing TPK	16- 4"	Not anticipated	\$0
Florida City Gas	GM	1427+75	North side crossing TPK (attached to bridge)	6" Steel	Widening	\$100,000
City of Port St. Lucie	FM	1426+20	South side crossing TPK east to SE Bayshore	8"	Widening	\$100,000
City of Port St. Lucie	WM	1426+70	South side crossing TPK east to SE Bayshore	16" Abandoned	Widening	Reimbursement not anticipated
City of Port St. Lucie	WM	1427+70	North side crossing TPK east to SE Bayshore Blvd	16"	Widening	Reimbursement not anticipated
City of Port St. Lucie	FM	1418+00 to 1427+50 (Bayshore Blvd)	West R/W	12"	Widening and new Interchange	\$100,000
City of Port St. Lucie	WM	1425+20	Crossing Bayshore Blvd	6"	Widening	Reimbursement not anticipated
City of Port St. Lucie	FM	1418+00 to 1433+00 (Bayshore Blvd)	East R/W	12"	Widening and new Interchange	\$100,000
City of Port St. Lucie	WM	1431+00 (Bayshore Blvd)	Crossing Bayshore Blvd	6"	Widening	Reimbursement not anticipated
Crown Castle	BFO	1428+00	North side crossing TPK	4-1.5" HDPE	Widening	Reimbursement not anticipated
Crown Castle	BFO	1426+00 to 1442+00 (Bayshore Blvd)	East R/W (Bayshore Blvd)	4-1.5" HDPE	Not anticipated	\$0
City of Port St. Lucie	FM	1426+00 to 1433+00 (Bayshore Blvd)	East R/W	4"	Widening	Reimbursement not anticipated
City of Port St. Lucie	Fiber	1426+00 to 1433+00 (Bayshore Blvd)	East R/W	1-2"	Widening	Reimbursement not anticipated



City of Port St. Lucie	Fiber	1427+90	North side crossing	1-2"	Widening	Reimbursement not anticipated
<b>Crosstown Parkway</b>						
City of Port St. Lucie	WM	1519+70	North side crossing TPK	18"	Not anticipated	\$0
City of Port St. Lucie	Fiber	1518+90	North side crossing TPK	1-2"	Not anticipated	\$0
Crown Castle	BFO	1518+00	South side crossing TPK	2-1.5" HDPE	Not anticipated	\$0
<b>St. Lucie West Boulevard</b>						
FPL Distribution	UE	1601+60	Crossing TPK	(2) 1000A XPE 23kv	Bridge construction	\$100,000
AT&T Florida	BT	1600+99	Crossing TPK	6-4"	Not anticipated	\$0
City of Port St. Lucie	FM	1600+00	Median crossing TPK	16"	Bridge construction	Reimbursement not anticipated
City of Port St. Lucie	WM	1599+50	South side crossing TPK	16"	Bridge construction	Reimbursement not anticipated
City of Port St. Lucie	Fiber	1599+45	South side crossing TPK	1-2"	Bridge construction	Reimbursement not anticipated
Crown Castle	BFO	1601+00	North side crossing TPK	2-1.5" HDPE	Not anticipated	\$0
<b>W Midway Road</b>						
FPUA Fiber	Fiber	1810+40	South side crossing TPK	1-4"	Widening	Reimbursement not anticipated
FPL Distribution	UE	1811+00	South side crossing TPK	3-1000A XPE 23kv	Widening and new interchange	\$200,000
AT&T Florida	BFO	1811+20	South side crossing TPK	#144 & #72	New Interchange	\$200,000
AT&T Florida	BT	1813+10 to 1820+00	East R/W	50 PR Copper	Not anticipated	\$0
City of Port St. Lucie	WM	1811+60	South side crossing TPK	16"	Widening and new interchange	\$200,000
<b>Glades Cut Off Road</b>						
Tropicana	GM	1834+00	North side crossing TPK	Douglas Pipeline	Bridge construction	Reimbursement not anticipated
AT&T Florida	BT	1830+90	South side crossing TPK	200 PR Copper	Not anticipated	\$0
AT&T Florida	BFO	1831+10	South side crossing TPK	#144	Not anticipated	\$0
AT&T Florida	BFO	1833+50	North side crossing TPK	#48	Bridge construction	Reimbursement not anticipated
FPUA Gas	GM	1832+80	North side crossing TPK	8"	Bridge construction	Reimbursement not anticipated

FPUA Water & Sewer	FM	1832+80	North side crossing TPK	6" PVC	Bridge construction	Reimbursement not anticipated
<b>Okeechobee Road (SR 70)</b>						
FPUA Fiber	Fiber	1960+20	South side crossing TPK	1-4"	Widening	\$100,000
AT&T Florida	BT	1961+00	South side crossing TPK	12-4"	Widening and new interchange	\$100,000
FPUA Water & Sewer	WM	1962+20	North side crossing TPK	12"	Widening and new interchange	\$100,000

\*Information in the table above was extracted from the Utility Assessment Package, February 2021.

*Aerial = aerial overhead fiber optics cable*

*BFO = buried fiber optics cable*

*BT = buried transmission line (electric)*

*Concrete Main = concrete water main*

*Fiber = fiber optics cable*

*FM = force main (water)*

*GM = gas main*

*OE = overhead electric cable*

*OFO = overhead fiber optics cable*

*OT = overhead transmission line (electric)*

*RWM = reuse water main (reuse water)*

*UE = underground electric*

*WM = water main*

### Railroads

Although there are no at-grade railroad crossings within the project limits, there is one location where the railroad passes underneath Florida's Turnpike (SR 91) mainline. The railroad has two tracks that run parallel to the south side of Glades Cut Off Road and cross underneath Bridge Nos. 940076 and 940951. The Primary Operating Railroad is the Florida East Coast Railroad Company (FEC) with an FDOT Crossing Inventory Number of 272254P. Based on information received from the U.S. Department of Transportation Federal Railroad Administration, there are a total of four trains during the day (6AM-6PM) and four trains during the night (6PM-6AM). The trains travel at speeds between 30 and 40 miles per hour (mph).

The existing bridge provides a substandard vertical clearance over the railroad. Due to the skewed orientation of the bridge, widening is not recommended. It is recommended that the existing bridge be replaced with a new structure to meet the minimum vertical clearance of 23.5 feet. Refer to the *Bridge Analysis Technical Memorandum*, March 2022, for more detail on the proposed bridge.

#### **2.4.5 Construction**

The construction activities associated with Florida's Turnpike (SR 91) corridor proposed improvements will result in temporary air, noise, vibration, water quality, traffic flow, and visual impacts for those residents and travelers within the immediate vicinity of the project. Air quality impacts will be temporary and primarily be in the form of exhaust emissions from trucks and construction equipment as well as fugitive dust from construction sites. Air pollution associated with the creation of airborne particles will be effectively controlled using watering or the application of other control materials in accordance with FDOT's *Standard Specifications for Road and Bridge Construction*.

Noise and vibration impacts may be generated by heavy equipment and construction activities such as pile driving and vibratory compaction of embankments. Noise control measures will be implemented as set forth in the FDOT's *Standard Specifications for Road and Bridge Construction*. Adherence to local construction noise and/or construction vibration ordinances by the construction contractor will also be required where applicable.

Water quality impacts resulting from erosion and sedimentation will be controlled in accordance with FDOT's *Standard Specifications for Road and Bridge Construction* and using Best Management Practices (BMPs).

Maintenance of traffic and sequence of construction will be planned and scheduled to minimize traffic delays during project construction. Signs will be used as appropriate to provide notice of road closures and other pertinent information to the travelling public. The local news media will be notified in advance of road closings and other construction-related activities which could inconvenience the community so that motorists, residents, and business persons can plan travel routes in advance.

Access to all businesses and residences will be maintained to the extent practical through controlled construction scheduling. Within the project study limits, the present traffic congestion may become worse during stages of construction where narrow lanes may be necessary. Traffic delays will be controlled to the extent possible where many construction operations are in progress at the same time.

Visual impacts associated with the storage of construction materials and establishment of temporary construction facilities will occur but are temporary and short term.

Construction of the roadway and bridges requires excavation of unsuitable material, placement of embankments, and the use of materials, such as lime rock, asphaltic concrete, and Portland cement concrete. The removal of structure and debris will be in accordance with local and state regulation agencies permitting this operation. The construction contractor will be responsible for controlling pollution on haul roads, in borrow areas, and areas used for disposal of waste materials from the project. Temporary erosion control features as specified in the FDOT's *Standard Specifications for Road and Bridge Construction*, Section 104, will consist of temporary grassing, sodding, mulching, sandbagging, slope drains, sediment basins, sediment checks, artificial coverings, and berms.

#### **2.4.6 Bicycles and Pedestrians**

Florida's Turnpike (SR 91) mainline is a limited access facility and does not accommodate bicycle facilities. Bicycle lanes are not proposed as part of the Preferred Alternative for Florida's Turnpike (SR 91) mainline.

Bicycle and pedestrian accommodations are proposed at five of the six project interchange locations. The only interchange location that does not include bicycle and pedestrian accommodations for the Preferred Alternative is Crosstown Parkway. The existing Crosstown Parkway overpass (Bridge No. 944018) does not require replacement or modification due to the mainline widening. The SW Martin Highway preferred interchange alternative proposes six-foot-wide sidewalks and seven-foot-wide buffered bicycle lanes, along the north and south sides of SW Martin Highway, from SW Martin Downs Boulevard to SW Leighton Farms Avenue/SW Deggeller Court. The SE Becker Road preferred interchange alternative proposes six-

foot-wide sidewalks and seven-foot-wide buffered bicycle lanes, along the north and south sides of SE Becker Road, from the existing traffic signal at the northbound on ramp to just west of SW Bradshaw Circle. The SW Port St. Lucie Boulevard preferred interchange alternative proposes six-foot-wide sidewalks and seven-foot-wide buffered bicycle lanes, along the north and south sides of SW Port St. Lucie Boulevard, from the intersection at SW Bayshore Boulevard to SW Cameo Boulevard. The W Midway Road preferred interchange alternative proposes six-foot-wide sidewalks and seven-foot-wide buffered bicycle lanes, along the north and south sides of W Midway Road, from the proposed intersection at S Jenkins Road/NW Milner Drive to Glades Cut Off Road. The Okeechobee Road (SR 70) preferred interchange alternative proposes six-foot-wide sidewalks and seven-foot-wide buffered bicycle lanes, along the north and south sides of Okeechobee Road (SR 70), from the intersection at S Kings Highway to Gordy Road. West of Gordy Road, a six-foot-wide sidewalk is proposed on the south side of Okeechobee Road (SR 70). The preferred interchange alternatives at SW Martin Highway, SE Becker Road, SW Port St. Lucie Boulevard, and W Midway Road provide two-foot-wide barrier wall, along the north and south sides of the overpass, in order to separate the pedestrians from the travel lanes.

#### **2.4.7 Navigation**

There are multiple navigable waterways, which cross Florida's Turnpike (SR 91) mainline within the project area. These waterways include St. Lucie Canal (C-44), Rim Ditch (C-24), Tenmile Creek, Loxahatchee River, and County Line Canal (C-23). The following determinations were made, regarding the respective waterways, through coordination with the United States Coast Guard (USCG).

##### St. Lucie Canal (C-44)

A permit amendment to the existing USCG Bridge Permit (No. 9-01-7) would be required for the St. Lucie Canal (C-44) crossing. The project design will maintain existing horizontal and vertical navigational clearances at this crossing. Refer to **Appendix E** for information regarding coordination with the USCG.

##### Rim Ditch (C-24)

A Bridge Permit would be required for the C-24 canal crossing and this waterway would not qualify for Advance Approval.

##### Tenmile Creek

No Bridge Permit is required for the Tenmile Creek crossing.

##### Loxahatchee River

No Bridge Permit is required for the Loxahatchee River crossing. The segment of the Loxahatchee River containing Florida's Turnpike (SR 91) bridge structure is not tidal and is presently not used for interstate commerce. In addition, because of the Wild & Scenic River designation on that segment of the river, improvements to the channel, making it usable to interstate commerce, is unlikely. Refer to **Appendix E** for information regarding coordination with the USCG.

County Line Canal (C-23)

A vertical weir in the County Line Canal (C-23) prevents navigation at the Florida's Turnpike (SR 91) crossing. No Bridge Permit is required for the C-23 canal.

A brief review of the smaller waterway crossings resulted in the USCG acknowledging that no further USCG coordination for these smaller waterway crossings would be required.

**APPENDIX B**  
*ETDM Programming Summary Report  
and Agency Comments*



## Florida Department of Transportation

**RICK SCOTT**  
GOVERNOR

605 Suwannee Street  
Tallahassee, FL 32399-0450

**MIKE DEW**  
SECRETARY

# ETDM Summary Report

Project #14295 - Turnpike Mainline Widening from Jupiter to Fort Pierce

Programming Screen - Published on 05/19/2017

Printed on: 1/22/2018

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## Introduction to Programming Screen Summary Report

The Programming Screen Summary Report shown below is a read-only version of information contained in the Programming Screen Summary Report generated by the ETDM Coordinator for the selected project after completion of the ETAT Programming Screen review. The purpose of the Programming Screen Summary Report is to summarize the results of the ETAT Programming Screen review of the project; provide details concerning agency comments about potential effects to natural, cultural, and community resources; and provide additional documentation of activities related to the Programming Phase for the project. Available information for a Programming Screen Summary Report includes:

- Screening Summary Report chart
- Project Description information (including a summary description of the project, a summary of public comments on the project, and community-desired features identified during public involvement activities)
- Purpose and Need information (including the Purpose and Need Statement and the results of agency reviews of the project Purpose and Need)
- Alternative-specific information, consisting of descriptions of each alternative and associated road segments; an overview of ETAT Programming Screen reviews for each alternative; and agency comments concerning potential effects and degree of effect, by issue, to natural, cultural, and community resources.
- Project Scope information, consisting of general project commitments resulting from the ETAT Programming Screen review, permits, and technical studies required (if any)
- Class of Action determined for the project
- Dispute Resolution Activity Log (if any)

The legend for the Degree of Effect chart is provided in an appendix to the report.

For complete documentation of the project record, also see the GIS Analysis Results Report published on the same date as the Programming Screen Summary Report.





# #14295 Turnpike Mainline Widening from Jupiter to Fort Pierce

**District:** District 4

**Phase:** Programming Screen

**County:** Martin , Palm Beach , St. Lucie

**From:** Indiantown Road (MP 116)

**Planning Organization:** Florida's Turnpike Enterprise

**To:** SR 70 (MP 152)

**Plan ID:** Not Available

**Financial Management No.:** 423374-1-22-01

**Federal Involvement:** Other Federal Permit USCG Bridge Permit

**Contact Information:** Brian Ribaric (407) 264-3095 brian.ribaric@dot.state.fl.us

**Snapshot Data From:** Project Published 5/19/2017

*Issues and Categories are reflective of what was in place at the time of the screening event.*

	Social and Economic						Cultural			Natural					Physical					Emergency Response	
	Land Use Changes	Social	Relocation Potential	Farmlands	Aesthetic Effects	Economic	Mobility	Section 4(f) Potential	Historic and Archaeological Sites	Recreation Areas	Wetlands and Surface Waters	Water Quality and Quantity	Floodplains	Wildlife and Habitat	Coastal and Marine	Noise	Air Quality	Contamination	Infrastructure		Navigation
Alternative #1 From: Indiantown Road (MP 116) To: SR 70 (MP 152) Published: 05/19/2017 Reviewed from 01/18/2017 to 03/04/2017	2	3	2	3	2	1	1	N/A	3	3	3	3	3	3	3	3	2	3	2	3	4

## Purpose and Need

### Purpose and Need

The purpose of the widening of Florida's Turnpike Mainline (SR 91) from Jupiter to Ft. Pierce is to add capacity that will accommodate future traffic volumes of freight and passenger vehicles linked to the projected growth in population and industry. Based on county-wide growth totals developed by the Bureau of Economic and Business Research at the University of Florida the population of St. Lucie County is anticipated to grow by almost 65% and employment is expected to increase by approximately 58% between 2010 and 2040, with the fastest job growth occurring in the industrial sector.

Establishment of two Freight Logistics Zones in St. Lucie County around the Treasure Coast International Airport and the Port of Ft. Pierce, and a 1,200-acre Intermodal Logistics Center located just north of the airport have the potential to significantly increase freight traffic to and from these areas in northern St. Lucie County. An increase in freight traffic throughout the project corridor will place strains on the capacity of the existing roadway and further reduce the safety of a stretch of roadway that has averaged 3.5 fatalities per year and over 145 injuries each year since 2011. The Florida's Turnpike Enterprise document entitled *Florida Traffic Trends Report, July 2015*, identifies the need to widen the mainline from four to six lanes by 2040 from Jupiter (MP 116) to Stuart (MP 133), by 2030 from Stuart (MP 133) to Port St. Lucie (MP 142) and by 2035 from Port St. Lucie (MP 142) to Ft. Pierce (MP 152). In addition, improvements at the Port St. Lucie Boulevard (MP 142) interchange are needed by 2020 and this improvement is identified as the number seven ranked unfunded interchange need improvement.

### Planning Consistency

The Martin MPO LRTP references a portion of the project from West Indiantown Road (MP 116) to St. Lucie County in the SIS 2040 Multi-Modal Unfunded Needs Plan. The project is consistent with the goals and objectives of the Martin MPO, the St. Lucie County TPO, and the Palm Beach MPO.

## Purpose and Need Reviews

### FL Department of Agriculture and Consumer Services

Acknowledgement	Date Reviewed	Reviewer	Comments
Understood	03/03/2017	Steve Bohl (Steve.Bohl@freshfromflorida.com)	No Purpose and Need comments found.

### FL Department of Economic Opportunity

Acknowledgement	Date Reviewed	Reviewer	Comments
Understood	03/03/2017	Matt Preston (matt.preston@deo.myflorida.com)	No Purpose and Need comments found.

### FL Department of Environmental Protection

Acknowledgement	Date Reviewed	Reviewer	Comments
Understood	02/10/2017	Suzanne Ray (plan.review@dep.state.fl.us)	No Purpose and Need comments found.

### FL Department of State

Acknowledgement	Date Reviewed	Reviewer	Comments
Understood	02/22/2017	Daniel McClarnon (daniel.mcclarnon@dos.myflorida.com)	No Purpose and Need comments found.

### FL Fish and Wildlife Conservation Commission

Acknowledgement	Date Reviewed	Reviewer	Comments
Understood	02/28/2017	Scott Sanders (scott.sanders@myfwc.com)	No Purpose and Need comments found.

### National Marine Fisheries Service

Acknowledgement	Date Reviewed	Reviewer	Comments
Understood	02/14/2017	Brandon Howard (Brandon.Howard@noaa.gov)	None

### National Park Service

Acknowledgement	Date Reviewed	Reviewer	Comments
Understood	03/10/2017	Anita Barnett (anita_barnett@nps.gov)	No Purpose and Need comments found.

### Natural Resources Conservation Service

Acknowledgement	Date Reviewed	Reviewer	Comments
Understood	01/19/2017	Rick Robbins (rick.a.robbins@fl.usda.gov)	No Purpose and Need comments found.

### South Florida Water Management District

Acknowledgement	Date Reviewed	Reviewer	Comments
Understood	02/20/2017	Mindy Parrott (mparrott@sfwmd.gov)	It is difficult to review a project without knowing where new interchanges will be, whether all the widening work will be completed within the existing right of way.

### US Army Corps of Engineers

Acknowledgement	Date Reviewed	Reviewer	Comments
Understood	02/22/2017	Tarrie Ostrofsky (Tarrie.L.Ostrofsky@usace.army.mil)	The Corps understands the purpose and need for this proposed project.

### US Coast Guard

Acknowledgement	Date Reviewed	Reviewer	Comments
Understood	02/24/2017	Randall Overton (randall.d.overton@uscg.mil)	No Purpose and Need comments found.

### US Environmental Protection Agency

Acknowledgement	Date Reviewed	Reviewer	Comments
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## US Fish and Wildlife Service

Acknowledgement	Date Reviewed	Reviewer	Comments
Understood	02/17/2017	John Wrublik (john_wrublik@fws.gov)	No Purpose and Need comments found.

## Project Description Data

### Project Description

The widening of Florida's Turnpike mainline (SR 91) from Jupiter to Fort Pierce will begin at milepost (MP) 116 in Palm Beach County and end at MP 152 in St. Lucie County, Florida. The total project length will be approximately 37 miles. The project consists of the widening of Florida's Turnpike from four to eight lanes by adding two express lanes in each direction. This portion of Florida's Turnpike includes numerous bridge structures that will need to be widened/reconstructed along with the mainline roadway. The project corridor contains a crossing of the Loxahatchee River and St. Lucie Canal. New interchange access locations will be considered as part of the PD&E study. These locations have not been determined as of this report.

### Summary of Public Comments

Summary of Public Comments is not available at this time.

#### Justification

A public meeting is planned for this project. The exact date has not been determined at this time.

### Planning Consistency Status

Planning Consistency Status

MPOs (if applicable) St. Lucie TPO

### Potential Lead Agencies

- FL Department of Transportation

### Exempted Agencies

Agency Name	Justification	Date
Federal Transit Administration	FTA has requested to be exempt from reviewing any non-transit projects.	09/02/2016

### Community Desired Features

No desired features have been entered into the database. This does not necessarily imply that none have been identified.

### User Defined Communities Within 500 Feet

- Jupiter
- Mid County
- Palm City
- Port Salerno / 76
- South County

### Census Places Within 500 Feet

- Fort Pierce
- Jupiter
- Palm City
- Port St. Lucie

## Alternative #1

### Alternative Description

Name	From	To	Type	Status	Total Length	Cost	Modes	SIS
Alternative was not named.	Indiantown Road (MP 116)	SR 70 (MP 152)	Widening	ETAT Review Complete	37.0 mi.		Roadway	Y

### Segment Description(s)

#### Location and Length

Segment No.	Name	Beginning Location	Ending Location	Length (mi.)	Roadway Id	BMP	EMP
Unnamed Segment	Unnamed Segment			36.96			

#### Jurisdiction and Class

Segment No.	Jurisdiction	Urban Service Area	Functional Class
Unnamed Segment			

#### Base Conditions

Segment No.	Year	AADT	Lanes	Config
Unnamed Segment				

#### Interim Plan

Segment No.	Year	AADT	Lanes	Config
Unnamed Segment				

#### Needs Plan

Segment No.	Year	AADT	Lanes	Config
Unnamed Segment				

#### Cost Feasible Plan

Segment No.	Year	AADT	Lanes	Config
Unnamed Segment				

#### Funding Sources

No funding sources found.

### Project Effects Overview for Alternative #1

Issue	Degree of Effect	Organization	Date Reviewed
<b>Social and Economic</b>			
Land Use Changes	N/A N/A / No Involvement	FL Department of Economic Opportunity	03/03/2017
Social	4 Substantial	US Environmental Protection Agency	03/22/2017
Farmlands	3 Moderate	Natural Resources Conservation Service	01/19/2017
Economic	N/A N/A / No Involvement	FL Department of Economic Opportunity	03/03/2017
<b>Cultural</b>			
Historic and Archaeological Sites	3 Moderate	FL Department of State	02/22/2017
Recreation Areas	3 Moderate	South Florida Water Management District	03/02/2017
Recreation Areas	3 Moderate	National Park Service	03/10/2017
<b>Natural</b>			
Wetlands and Surface Waters	4 Substantial	US Environmental Protection Agency	03/22/2017
Wetlands and Surface Waters	3 Moderate	US Fish and Wildlife Service	01/25/2017
Wetlands and Surface Waters	3 Moderate	National Marine Fisheries Service	02/14/2017

Wetlands and Surface Waters	3	Moderate	US Army Corps of Engineers	02/22/2017
Wetlands and Surface Waters	3	Moderate	South Florida Water Management District	03/02/2017
Wetlands and Surface Waters	3	Moderate	FL Department of Environmental Protection	03/03/2017
Water Quality and Quantity	3	Moderate	South Florida Water Management District	03/02/2017
Water Quality and Quantity	4	Substantial	US Environmental Protection Agency	03/22/2017
Floodplains	3	Moderate	South Florida Water Management District	03/02/2017
Wildlife and Habitat	4	Substantial	FL Fish and Wildlife Conservation Commission	02/28/2017
Wildlife and Habitat	3	Moderate	US Fish and Wildlife Service	01/25/2017
Wildlife and Habitat	0	None	FL Department of Agriculture and Consumer Services	03/03/2017
Coastal and Marine	3	Moderate	National Marine Fisheries Service	02/14/2017
Coastal and Marine	2	Minimal	South Florida Water Management District	03/02/2017
<b>Physical</b>				
Air Quality	2	Minimal	US Environmental Protection Agency	03/07/2017
Contamination	3	Moderate	US Environmental Protection Agency	03/22/2017
Contamination	2	Minimal	South Florida Water Management District	03/02/2017
Navigation	3	Moderate	US Coast Guard	02/24/2017
Navigation	2	Minimal	US Army Corps of Engineers	02/22/2017
<b>Special Designations</b>				
Special Designations	4	Substantial	US Environmental Protection Agency	03/22/2017
Special Designations	3	Moderate	South Florida Water Management District	03/02/2017
Special Designations	4	Substantial	US Fish and Wildlife Service	01/25/2017
<b>Emergency Response</b>				

## ETAT Reviews and Coordinator Summary: Social and Economic

### Land Use Changes

#### Project Effects

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 05/12/2017 by Florida's Turnpike Enterprise

#### Comments:

The Florida Department of Economic Opportunity (DEO) reviewed Comprehensive Plans for St. Lucie and Martin Counties, the Cities of Fort Pierce and Port St. Lucie, and the Town of Jupiter. The DEO identified that the Capital Improvements Plan (CIP) for St. Lucie County must be updated to include the project, while Martin County should update its Comprehensive Plan to discuss the project specifically. Similarly, the DEO determined that the City of Fort Pierce needs to update its Comprehensive Plan and CIP to include the project. The Town of Jupiter must also update its Comprehensive Plan to discuss the project specifically. The City of Port St. Lucie has indicated its desire to see a new exit established at Crosstown Parkway. The DEO also determined that the five jurisdictions have not yet identified the project within their future transportation map. Florida's Turnpike Enterprise will coordinate with each local agency to ensure the proposed project is included in the appropriate Comprehensive Plan, CIP, and future transportation map.

**Degree of Effect:** N/A *N/A / No Involvement* assigned 03/03/2017 by Matt Preston, FL Department of Economic Opportunity

**Coordination Document:** No Involvement

## Direct Effects

### Identified Resources and Level of Importance:

#### Comprehensive Plan(s) Reviewed:

*City of Fort Pierce Comprehensive Plan*, adopted in February of 2011; *City of Port St. Lucie Comprehensive Plan*, adopted on September 10, 2012; *St. Lucie County Comprehensive Plan*, adopted in 2010; *Town of Jupiter Comprehensive Plan*, adopted on October 6, 2016; and, *Martin County Comprehensive Growth Management Plan*, adopted on October 20, 2015.

### Comments on Effects to Resources:

#### Compatibility with Community Development Goals and Comprehensive Plan:

City of Fort Pierce: Policy 2.2.1 requires roadway improvement projects to be added to the CIP; Policy 2.61 requires coordination with local authorities for implementation of regional system improvements in the 5-year schedule of Capital Improvements; Policy 2.10.3 directs the City to participate in FDOT's five-year work program; no future number of lanes map; CIP out-of-date.

City of Port St. Lucie: The City of Port St. Lucie indicated that as this widening occurs, that a new exit off the Turnpike at Crosstown Parkway is desired. Policy 2.4.1.3 requires the City to annually review transportation improvements planned for the City and indicate the agency responsible for the improvement; roadway network was updated to include FDOT's infrastructure improvements through 2016.

Unincorporated St. Lucie County: Addition of two lanes for the Turnpike included in *Table 2-10, Roadway Needs Assessment*; not in CIP (ran through 2015), not in Long-term transportation CIP.

Town of Jupiter: The Comprehensive Plan requires coordination with FDOT, but project is not mentioned specifically; lanes not included TE Table 4 - Future conditions 2035.

Unincorporated Martin County: The Comprehensive Plan requires coordination with FDOT, but is not mentioned specifically; lanes not included in Figure 5-5C, 2035 Roadway Lanes & LOS.

#### Future Transportation Map:

City of Fort Pierce: The project is not identified on the Future Transportation Map. DEO staff recommends that the City of Ft. Pierce update its map to include this project.

City of Port St. Lucie: The project is not identified on the Future Transportation Map. DEO staff recommends that the City of Port St. Lucie update its map to include this project.

Unincorporated St. Lucie County: The project is not identified on the Future Transportation Map. DEO staff recommends that St. Lucie County update its map to include this project.

Town of Jupiter: The project is not identified on the Future Transportation Map. DEO staff recommends that the Town of Jupiter update its map to include this project.

Unincorporated Martin County: The project is not identified on the Future Transportation Map. DEO staff recommends that Martin County update its map to include this project.

#### Land Uses:

The following land uses surround the project:

City of Fort Pierce: General Commercial.

City of Port St. Lucie: Institutional, commercial Service, Open Space Recreation, Medium Density Residential, Low Density Residential, Utility, Open Space Conservation, High Density Residential, Light Industrial, Commercial General, Residential Golf Course, Commercial Highway.

Unincorporated St. Lucie County: Transportation/Utilities, Special District, Mixed Use Development, Residential/Conservation.

Town of Jupiter: Low Density Residential, Commercial, Not Designated (I-95), and Water.

Unincorporated Martin County: Public Conservation, Rural Density, Agricultural, General Institutional, Estate Density up to 2 du/acre, Industrial, Commercial Waterfront, Medium Density, Ag TEC, Agricultural Ranchette, and Recreational.

#### Parks:

City of Fort Pierce: Gordy Road Recreation Area.

City of Port St. Lucie: Girl Scout Friendship Park, Turtle Run Park, and Jessica Clinton Park.

Unincorporated St. Lucie County: None identified.

Town of Jupiter: None identified.

Unincorporated Martin County: Jonathan Dickinson State Park and Atlantic Ridge Preserve State Park.  
FDOT should analyze potential impacts to these 4(f) resources.

Area of Critical State Concern (ACSC), Coastal High Hazard Area (CHHA), and Military Bases:

The project is not located within an Area of Critical State Concern, or the CHHA; nor does it encroach on any military bases.

Other Planning-Related Items:

None.

Contact Information:

David Kemp (Town of Jupiter) - Phone Number: (561) 741-2452. Nikki Van Vonno (Martin County) - Phone Number: (772) 288-5520.

**Additional Comments (optional):**

**CLC Commitments and Recommendations:**

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## Social

### Project Effects

**Coordinator Summary Degree of Effect:** 3 *Moderate* assigned 05/18/2017 by Florida's Turnpike Enterprise

**Comments:**

The United States Environmental Protection Agency (USEPA) commented that there are significant low-income, minority, linguistically isolated, and other special populations in the project corridor. A Sociocultural Effects Evaluation will be conducted during the Project Development and Environment (PD&E) phase to verify the presence of these populations and determine potential impacts the project may have on surrounding communities. Public outreach will be conducted during the PD&E phase to solicit input from all residents in the project area, including these special populations. Based on comments from the USEPA, the 1/4-mile buffer distance has been applied using the data provided by the Environmental Screening Tool (EST). The results of this review are summarized below.

Within the 1/4-mile assessment area, there are eleven (11) parcels of public land, twelve (12) existing recreational trails, five (5) schools, five (5) religious centers, and 36 census block groups. The average housing vacancy rate within these block groups is approximately 13 percent; however, vacancy rates of individual block groups range from 6.1 percent to 22.5 percent. The average rate of limited English proficiency (LEP) among the assessment area's block groups is 2.5 percent; however, LEP rates among individual block groups range from zero (0) to ten (10) percent. Within the assessment area, there are 150 Census Blocks that have minority populations greater than 40 percent.

The USEPA also recommended that the project comply with Executive Order 13166, *Improving Access to Services for Persons with Limited English Proficiency* and Executive Order 13045, *Protection of Children from Environmental Health Risks and Safety Risks*. Accommodations for limited English proficiency will be provided during public involvement efforts. Additionally, a noise study report and air quality screening report will be developed as part of the PD&E study, which complies with Executive Order 13045, *Protection of Children from Environmental Health Risks and Safety Risks* per the PD&E manual.

**Degree of Effect:** 4 *Substantial* assigned 03/22/2017 by Kim Gates, US Environmental Protection Agency

**Coordination Document:** Tech Memo Required

**Coordination Document Comments:**

Sociocultural Effects (SCE) Evaluation Technical Memorandum (PD&E Manual, Part 2, Chapter 9; FDOT's Sociocultural Effects Evaluation Handbook)

Children's Health Assessment Report

Noise Study Report



## Direct Effects

### Identified Resources and Level of Importance:

Social impacts can be defined as any action or activity that has an effect on how people live, work, play, relate to one another, organize to meet their needs, and function as individuals and/or society. In the *Agency Operating and Funding Agreement for Continuing Participation in the Efficient Transportation Decision Making and Transportation Project Development Processes between United States Environmental Protection Agency and Federal Highway Administration and Florida Department of Transportation*, January 23, 2015, FDOT requested the USEPA's focus on Environmental Justice considerations for this issue. Therefore, the resources of particular concern are low-income, disadvantaged, minority, and other special populations.

The level of importance is discussed in FDOT's ETDM Manual, PD&E Manual, Public Involvement Handbook, and summarized in this succinct paragraph from the Sociocultural Effects Evaluation Handbook (page C-1):

"Historically, minority, disadvantaged, low-mobility, and low-income populations have been underrepresented in the transportation planning and project development process. Inadequate access to decision-making and information increases the potential that a specific population will be adversely affected by a transportation project and the likelihood that their specific needs or concerns will not be fully addressed. Since 1964, federal laws and policies have been developed to ensure that the civil rights of minority, disadvantaged, low-mobility and low-income populations will be protected and that the decision-making process for those projects is free from discrimination. Compliance with Title VI of the Civil Rights Act of 1964 cuts across all sociocultural considerations."

Applicable authorities include, but are not limited to:

Executive Order 12898: Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations [February 11, 1994];

USDOT Order 5610.2: Department of Transportation Actions to Address Environmental Justice in Minority Populations and Low-Income Populations [April 1997]; and

USDOT Order 6640.23: FHWA Actions to Address Environmental Justice in Minority Populations and Low-Income Populations [December 1998].

### Comments on Effects to Resources:

It is not clear why a 500-foot buffer area was used in the Preliminary Environmental Discussion Comments Report (PED) to assess the potential social impacts of the project. The FDOT PD&E Manual (Part 2, Chapter 9) prescribes use of a 1/4-mile buffer area in urban areas and a 1-mile buffer area for rural areas. Because the project corridor traverses urban and rural areas, both buffers should have been used. Please provide an explanation in the ETDM Summary Report for this deviation from FDOT policy and indicate whether the Sociocultural Effects Evaluation conducted during PD&E will be consistent with the PD&E Manual and FDOT's Sociocultural Effects Evaluation Handbook.

Of note is that the 1/4-mile buffer area should be used during PD&E. As stated in the Sociocultural Effects Evaluation Handbook (page 6-11), the "1/4 Mile buffer is the preferred buffer for SCE evaluations to allow for the inclusion of community facilities and address connectivity."

Review of the Census Block Group data in the EST revealed populations of concern in the City of Port St. Lucie, which is an urbanized area as defined by the US Census Bureau ([http://www2.census.gov/geo/maps/dc10map/UAUC\\_RefMap/ua/ua71479\\_port\\_st\\_lucie\\_fl/DC10UA71479.pdf](http://www2.census.gov/geo/maps/dc10map/UAUC_RefMap/ua/ua71479_port_st_lucie_fl/DC10UA71479.pdf)). Utilizing the prescribed 1/4-mile buffer and EJSCREEN\* (<http://www2.epa.gov/ejscreen>), the following demographics characterize the various populations adjacent to the project corridor and within the city limits:

Total Population = 24,402  
Minority Population = 43% [State Average = 43%]  
Low Income Population = 39% [State Average = 38%]  
Linguistically Isolated Population = 5% [State Average = 7%]  
Population with Less than High School Education = 13% [State Average = 14%]  
Population Under 5 Years of Age = 6% [State Average = 6%]  
Population Over 64 Years of Age = 20% [State Average = 18%]

Unfortunately, the averaging of Census Block Group data in EJSCREEN produces a demographic picture that mostly mirrors statewide averages. Drilling down to Block Group and Block levels is therefore necessary to understand the people who will be impacted by the project.

There are nineteen (19) Census Block Groups adjacent to Florida's Turnpike corridor, in the 1/4 mile buffer, and within the Port St. Lucie city limits. As indicated in **bold** in the following demographic data from EJSCREEN, all of the Block Groups (listed from north to south) have at least one EJ-related population that is larger than the state average. Consequently, it appears that the project could have disproportionately high and adverse human health

or environmental effects on low-income, disadvantaged, minority, and other special populations.

Block Group #121113821082

Total Population = 9,295

**Minority Population = 50%**

**Low Income Population = 41%**

Linguistically Isolated Population = 6%

Population with Less than High School Education = 9%

Population Under 5 Years of Age = 6%

Population Over 64 Years of Age = 11%

Block Group #121113815023

Total Population = 6,920

Minority Population = 38%

**Low Income Population = 44%**

Linguistically Isolated Population = 5%

**Population with Less than High School Education = 15%**

Population Under 5 Years of Age = 5%

**Population Over 64 Years of Age = 28%**

Block Group #121113821084

Total Population = 3,010

Minority Population = 18%

Low Income Population = 19%

Linguistically Isolated Population = 0%

Population with Less than High School Education = 6%

Population Under 5 Years of Age = 0%

**Population Over 64 Years of Age = 60%**

Block Group #121113815033

Total Population = 3,690

**Minority Population = 53%**

Low Income Population = 28%

Linguistically Isolated Population = 3%

Population with Less than High School Education = 14%

Population Under 5 Years of Age = 4%

Population Over 64 Years of Age = 11%

Block Group #121113815034

Total Population = 2,876

**Minority Population = 52%**

**Low Income Population = 43%**

Linguistically Isolated Population = 2%

**Population with Less than High School Education = 15%**

**Population Under 5 Years of Age = 8%**

Population Over 64 Years of Age = 14%

Block Group #121113820083

Total Population = 3,119

**Minority Population = 43%**

Low Income Population = 43%

Linguistically Isolated Population = 3%

**Population with Less than High School Education = 22%**

Population Under 5 Years of Age = 4%

Population Over 64 Years of Age = 16%

Block Group #121113821083

Total Population = 6,515

Minority Population = 35%

Low Income Population = 32%  
Linguistically Isolated Population = 5%  
Population with Less than High School Education = 10%  
Population Under 5 Years of Age = 3%  
**Population Over 64 Years of Age = 32%**

Block Group #121113820082

Total Population = 2,362  
**Minority Population = 68%**  
**Low Income Population = 58%**  
**Linguistically Isolated Population = 9%**  
**Population with Less than High School Education = 18%**  
**Population Under 5 Years of Age = 14%**  
**Population Over 64 Years of Age = 19%**

Block Group #121113820081

Total Population = 1,585  
**Minority Population = 47%**  
Low Income Population = 33%  
Linguistically Isolated Population = 4%  
Population with Less than High School Education = 14%  
Population Under 5 Years of Age = 5%  
Population Over 64 Years of Age = 6%

Block Group #121113821114

Total Population = 4,632  
**Minority Population = 59%**  
**Low Income Population = 44%**  
Linguistically Isolated Population = 7%  
**Population with Less than High School Education = 22%**  
Population Under 5 Years of Age = 2%  
Population Over 64 Years of Age = 11%

Block Group #121113820071

Total Population = 4,913  
**Minority Population = 48%**  
**Low Income Population = 39%**  
Linguistically Isolated Population = 2%  
**Population with Less than High School Education = 19%**  
Population Under 5 Years of Age = 6%  
Population Over 64 Years of Age = 15%

Block Group #121113821132

Total Population = 1,512  
Minority Population = 24%  
Low Income Population = 31%  
**Linguistically Isolated Population = 10%**  
**Population with Less than High School Education = 18%**  
Population Under 5 Years of Age = 5%  
Population Over 64 Years of Age = 12%

Block Group #121113820061

Total Population = 3,329  
Minority Population = 36%  
**Low Income Population = 45%**  
**Linguistically Isolated Population = 9%**  
Population with Less than High School Education = 6%  
**Population Under 5 Years of Age = 8%**  
Population Over 64 Years of Age = 16%

Block Group #121113821131

Total Population = 3,025

**Minority Population = 49%**

**Low Income Population = 69%**

**Linguistically Isolated Population = 10%**

Population with Less than High School Education = 13%

**Population Under 5 Years of Age = 9%**

Population Over 64 Years of Age = 9%

Block Group #121113820063

Total Population = 2,622

Minority Population = 20%

Low Income Population = 26%

Linguistically Isolated Population = 0%

Population with Less than High School Education = 4%

Population Under 5 Years of Age = 5%

**Population Over 64 Years of Age = 45%**

Block Group #121113821134

Total Population = 2,094

**Minority Population = 55%**

**Low Income Population = 66%**

**Linguistically Isolated Population = 16%**

Population with Less than High School Education = 14%

**Population Under 5 Years of Age = 13%**

Population Over 64 Years of Age = 10%

Block Group #121113820062

Total Population = 2,041

Minority Population = 34%

Low Income Population = 30%

Linguistically Isolated Population = 4%

Population with Less than High School Education = 7%

Population Under 5 Years of Age = 6%

**Population Over 64 Years of Age = 19%**

Block Group #121113821133

Total Population = 6,247

Minority Population = 43%

**Low Income Population = 39%**

**Linguistically Isolated Population = 8%**

**Population with Less than High School Education = 19%**

Population Under 5 Years of Age = 6%

Population Over 64 Years of Age = 13%

Block Group #121113821121

Total Population = 7,086

**Minority Population = 46%**

Low Income Population = 35%

**Linguistically Isolated Population = 9%**

Population with Less than High School Education = 13%

**Population Under 5 Years of Age = 13%**

Population Over 64 Years of Age = 14%

An additional Block Group that could be impacted by the project is located in Jupiter in Palm Beach County:

Block Group #120990002131

Total Population = 1,701

**Minority Population = 63%**

**Low Income Population = 69%**

**Linguistically Isolated Population = 15%**

**Population with Less than High School Education = 22%**

**Population Under 5 Years of Age = 10%**

Population Over 64 Years of Age = 9%

According to the PD&E Manual (Part 1, Chapter 3), the PED should have considered "the community demographics (e.g., age, income, minority populations), underserved populations/environmental justice concerns, community cohesion, safety/emergency response, community character, community goals, and describe potential involvement with them as appropriate." However, the only demographic discussed was "minority populations greater than 40 percent." Please explain in the ETDM Summary Report why impacts on other special populations were not considered.

Because demographic data indicates the presence of linguistically-isolated and less educated populations in the project corridor, the USEPA recommends complying with Executive Order 13166, *Improving Access to Services for Persons with Limited English Proficiency* (<https://www.fhwa.dot.gov/civilrights/programs/lep.cfm>), as described in the Federal Highway Administration's *How to Engage Low-Literacy and Limited-English-Proficiency Populations in Transportation Decisionmaking* ([www.fhwa.dot.gov/planning/publications/low-limited/](http://www.fhwa.dot.gov/planning/publications/low-limited/)). Please indicate in the ETDM Summary Report whether LEP considerations will be included in the public involvement process.

Moreover, please discuss the public involvement process in the ETDM Summary Report. The only reference to public involvement in the project documentation is the statement in the Purpose and Need that "[a] public meeting is planned for this project. The exact date has not been determined at this time." The USEPA does not understand how one public meeting will address public concerns along a 37-mile project corridor that impacts three cities (Fort Pierce, Port St. Lucie, and Jupiter), one Census Designated Place (Palm City), and unincorporated areas in three counties, and that will necessitate relocations to acquire right-of-way for the widening of Florida's Turnpike from four lanes to eight and adding new interchanges. It is also not clear how holding one public meeting is consistent with federal and state requirements, as well as the Department's own policies and procedures (e.g., PD&E Manual, Public Involvement Handbook, etc.), for conducting meaningful public involvement activities.

Executive Order 13045, Protection of Children from Environmental Health Risks and Safety Risks, directs federal agencies (and their designees) to minimize environmental health and safety risks to children, and to prioritize the identification and assessment of environmental health and safety risks that may have a disproportionate impact on children. Given the higher than average populations of children under 5 years of age in 7 Census Block Groups, the presence of 4 schools in the 500-foot buffer area (i.e., Citrus Grove Elementary School and South Fork High School in Martin County; Southeast Military Academy, and West Gate K-8 School in St. Lucie County), and 5 more schools in the 1000-foot buffer area (i.e., Bayshore Elementary School, Parkway Elementary School, Renaissance Charter School of St. Lucie, St. Lucie West K-8 School, and St. Lucie West Centennial High School in St. Lucie County), the USEPA recommends conducting a Children's Health Assessment during the PD&E phase to define these populations and identify all sensitive receptors that could be impacted, including preschools and childcare centers. The resulting report needs to specifically address potential air quality and noise impacts on children's health and safety. Information about protecting children's environmental health is available online at <https://www.epa.gov/children>.

The USEPA endorses conducting the noise study mentioned in the PED. Of particular concern is the proximity of several schools (i.e., West Gate K-8 School at 150 feet and South Fork High School at 225 feet) and residences (e.g., the Cove at St. Lucie apartments in Block Group #121113815023 at 150 feet, homes on NW North Macedo Blvd in Block Group #121113815033 at 220 feet, homes in Block Groups #121113821132 and 121113821131 at 180 feet, and a home in Savannah Estates in Martin County at 135 feet) to the turnpike and the impact of widening to eight lanes on buffers that may already be inadequate. The noise study should identify all sensitive receptors and determine if predicted noise levels will approach or exceed the Noise Abatement Criterion. Please confirm in the ETDM Summary Report that a Noise Study Report will be prepared.

\* EJSCREEN is a pre-decisional screening tool. It was not designed to be the basis for agency decision-making or determinations regarding the existence or absence of EJ concerns, nor should it be used to identify or label an area as an "EJ Community." Rather, EJSCREEN highlights locations that may be candidates for further review and/or outreach. EJSCREEN data needs to be supported by community-specific demographic information and local knowledge. ([https://www.epa.gov/sites/production/files/2015-05/documents/ejscreen\\_technical\\_document\\_20150505.pdf](https://www.epa.gov/sites/production/files/2015-05/documents/ejscreen_technical_document_20150505.pdf))

**Additional Comments (optional):**

Sociocultural Effects (SCE) Evaluation Technical Memorandum (PD&E Manual, Part 2, Chapter 9; FDOT's Sociocultural Effects Evaluation Handbook)

Children's Health Assessment Report

Noise Study Report

## Relocation Potential

### Project Effects

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 05/12/2017 by Florida's Turnpike Enterprise

**Comments:**

No ETAT reviews were submitted for this issue. However, 2008 geographic information systems (GIS) data provided in the Environmental Screening Tool (EST), indicates there are five residential areas (3.38 acres) within the 100-foot buffer, and 42 residential areas (543 acres) within the 500-foot buffer. A Conceptual Stage Relocation Plan will be prepared for this project if right-of-way acquisition results in the need for relocations.

None found

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## Farmlands

### Project Effects

**Coordinator Summary Degree of Effect:** 3 *Moderate* assigned 05/12/2017 by Florida's Turnpike Enterprise

**Comments:**

This project is being completed without a federal agency or financial or technical assistance from a federal agency. The documentation for this project is a State Environmental Impact Report (SEIR). Pursuant to Part 2, Chapter 28 of the FDOT PD&E Manual, the project is not subject to the provisions of the Farmland Protection Policy Act of 1981, 7 CFR Part 658.

The geographic information systems (GIS) data provided in the Environmental Screening Tool indicates that approximately 68 percent of the total project area is classified as Farmland of Unique Importance. If impacts to farmlands are anticipated, a GIS shapefile depicting these farmlands will be provided to the Natural Resource Conservation Service (NRCS).

**Degree of Effect:** 3 *Moderate* assigned 01/19/2017 by Rick Allen Robbins, Natural Resources Conservation Service

**Coordination Document:** To Be Determined: Further Coordination Required

**Coordination Document Comments:**

The GIS analysis data indicates that approximately 68 percent of the total project area is classified as Farmland of Unique Importance. The Farmland Protection Policy Act (FPPA) (PL 97-98; 7 U.S.C. 4201 et seq.) was enacted to protect the amount of open farmland which has substantially decreased as a result of land use changes. It states that Federal programs which contribute to the unnecessary and irreversible conversion of farmland to nonagricultural uses will be minimized. Agencies are also to consider alternative actions and ensure that their programs are compatible with state and local government programs.

Environmental assessments must be prepared for actions which may adversely affect such unique geographic characteristics as prime farmlands. The regulations apply to construction activities, development grants and loans, and certain Federal land management decisions that contribute either directly or indirectly to loss of farmland.

**A Farmland Protection Policy Act form (AD-1006) may be required for this project.** Please refer to the link below for more information:

<https://www.nrcs.usda.gov/wps/portal/nrcs/detail/fl/soils/?cid=stelprdb1101661>

### Direct Effects

**Identified Resources and Level of Importance:**

The USDA-NRCS considers soil map units with important soil properties for agricultural uses to be Prime Farmland (Important Farmland soils). Prime Farmland (as defined in ETDM) is classified in several different categories based on specific criteria. **Prime Farmland** must meet specific soil-related criteria, as defined by the USDA-Natural Resources Conservation Service. **Farmland of Unique Importance** is based on the ability of the soil to grow very specific crops, such as citrus, vegetables, sugar cane, and other high-value specialty crops. It is also based on the extent that a soil is used for these crops within a specific county. Therefore, a soil in one county may be Unique Farmland, but not in an adjacent county. **Farmland of Local Importance** is classified as being important to the local entities (counties) and worthy of special consideration. Locally Important Farmland soils were designated by local governance (Soil and Water Conservation Districts).

Nationally, there has been a reduction in the overall amount of Prime, Locally Important, and Unique Farmlands through conversion to non-farm uses. This trend has the possibility of impacting the nation's food supply and exporting capabilities.

**Comments on Effects to Resources:**

Conducting GIS analysis of Prime Farmland (using USDA-NRCS data) and Important Farmland Analysis (using 2008 SFWMD data and 2015 SSURGO

data) has resulted in the determination that there are soils designated as Farmland of Unique Importance at all buffer widths within the Project footprint. In addition, there are areas currently used for agricultural production at all buffer widths.

At the 100 foot buffer width, there are 616.5 acres of Farmland of Unique Importance. At the 200 foot buffer width, there are 1231.3 acres of Farmland of Unique Importance. At the 500 foot buffer width, there are 3068.4 acres of Farmland of Unique Importance.

Land used for agricultural production (SFWMD Ag Lands 2008-*primarily pasture and citrus*, but includes others) ranges from 2.6 acres at the 100 foot buffer width to 530.8 acres at the 500 foot buffer width.

**More importantly**, land in agricultural use (primarily -*see list above*) that also classifies as Farmland of Unique Importance ranges from 2.6 acres at the 100 foot buffer width to 512.9 acres at the 500 foot buffer width. This combination of Important Farmland that is agricultural production amounts to less than 1 to 11% of the Project footprint (depending on buffer width).

An evaluation of more recent orthoimagery suggests that there has been a reduction of citrus production since the 2008 SFWMD land use data was gathered. Many of the groves within the proposed project footprint have transitioned from citrus due to citrus greening and other diseases. Under normal circumstances, the USDA-NRCS would have rated the Degree of Effect as Substantial. However, due to land use changes, project design (widening), and the proximity of the project to other existing transportation corridors we have downgraded the Degree of Effect to Moderate.

**Additional Comments (optional):**

The GIS analysis data indicates that approximately 68 percent of the total project area is classified as Farmland of Unique Importance. The Farmland Protection Policy Act (FPPA) (PL 97-98; 7 U.S.C. 4201 et seq.) was enacted to protect the amount of open farmland which has substantially decreased as a result of land use changes. It states that Federal programs which contribute to the unnecessary and irreversible conversion of farmland to nonagricultural uses will be minimized. Agencies are also to consider alternative actions and ensure that their programs are compatible with state and local government programs.

Environmental assessments must be prepared for actions which may adversely affect such unique geographic characteristics as prime farmlands. The regulations apply to construction activities, development grants and loans, and certain Federal land management decisions that contribute either directly or indirectly to loss of farmland.

**A Farmland Protection Policy Act form (AD-1006) may be required for this project.** Please refer to the link below for more information:

<https://www.nrcs.usda.gov/wps/portal/nrcs/detail/fl/soils/?cid=stelprdb1101661>

**CLC Commitments and Recommendations:**

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**Aesthetic Effects**

**Project Effects**

**Coordinator Summary Degree of Effect:** 2 Minimal assigned 05/12/2017 by Florida's Turnpike Enterprise

**Comments:**

No ETAT reviews were submitted for this issue. The project is not likely to create any adverse impacts to aesthetics. Public involvement will solicit public opinion on project effects and general design concepts related to aesthetics.

None found

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**Economic**

**Project Effects**

**Coordinator Summary Degree of Effect:** 1 Enhanced assigned 05/12/2017 by Florida's Turnpike Enterprise

**Comments:**

The proposed improvements have the potential to stimulate new development and generate additional employment opportunities.

**Degree of Effect:** N/A N/A / No Involvement assigned 03/03/2017 by Matt Preston, FL Department of Economic Opportunity

**Coordination Document:** No Involvement

**Direct Effects**

**Identified Resources and Level of Importance:**

Comprehensive Plan(s) Reviewed:

*City of Fort Pierce Comprehensive Plan*, adopted in February of 2011; *City of Port St. Lucie Comprehensive Plan*, adopted on September 10, 2012; *St.*

Lucie County Comprehensive Plan, adopted in 2010; Town of Jupiter Comprehensive Plan, adopted on October 6, 2016; and, Martin County Comprehensive Growth Management Plan, adopted on October 20, 2015.

**Comments on Effects to Resources:**

The project is *not* located within a Rural Area of Opportunity.

The project does have the potential to attract new development and create additional employment opportunities.

**Additional Comments (optional):**

**CLC Commitments and Recommendations:**

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**Mobility**

**Project Effects**

**Coordinator Summary Degree of Effect:** 1 *Enhanced* assigned 05/12/2017 by Florida's Turnpike Enterprise

**Comments:**

No ETAT reviews were submitted for this issue. The project will increase roadway capacity throughout the project limits and create the potential to attract new development in the surrounding areas.

None found

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**ETAT Reviews and Coordinator Summary: Cultural**

**Section 4(f) Potential**

**Project Effects**

**Coordinator Summary Degree of Effect:** N/A *N/A / No Involvement* assigned 05/12/2017 by Florida's Turnpike Enterprise

**Comments:**

No ETAT reviews were submitted for this issue. Section 4(f) is not applicable for state-funded projects.

None found

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**Historic and Archaeological Sites**

**Project Effects**

**Coordinator Summary Degree of Effect:** 3 *Moderate* assigned 05/12/2017 by Florida's Turnpike Enterprise

**Comments:**

The Florida Department of State and the Seminole Tribe of Florida commented that a Cultural Resource Assessment Survey (CRAS) will be required for the project. Florida's Turnpike Enterprise will prepare a CRAS in coordination with the State Historic Preservation Office (SHPO).

**Degree of Effect:** 3 *Moderate* assigned 02/22/2017 by Daniel McClarnon, FL Department of State

**Coordination Document:** PD&E Support Document As Per PD&E Manual

**Direct Effects**

**Identified Resources and Level of Importance:**

The Area of Potential Effect will need to be surveyed. Our office looks forward to reviewing the CRAS.

**Comments on Effects to Resources:**

n/a

**Additional Comments (optional):**

**CLC Commitments and Recommendations:**

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**Recreation Areas**



## Project Effects

**Coordinator Summary Degree of Effect:** 3 Moderate assigned 05/18/2017 by Florida's Turnpike Enterprise

### Comments:

The National Park Service (NPS) noted that the Loxahatchee River, crossed by the Florida's Turnpike mainline, is one of two rivers in Florida designated as a *Wild and Scenic River*. The NPS also identified two Land and Water Conservation Fund (LWCF) sites within the 200- and 500-foot buffers, as well as two LWCF sites within one mile of the proposed project. Any right-of-way (R/W) that converts the use of these sites to any use other than public outdoor recreation will constitute a Section 6(f)(3) conversion, which will require replacement property of equal or greater financial and ecological value. The NPS also recommended an Environmental Assessment (EA) or an Environmental Impact Survey (EIS) and 4(f) evaluation be prepared. The South Florida Water Management District (SFWMD) noted additional criteria will be required for protecting the Loxahatchee River, an aquatic preserve. It also indicated that the Loxahatchee River and Ten Mile Creek area are both part of Comprehensive Everglades Restoration Plan (CERP) projects, and will require additional coordination with CERP leadership.

Florida's Turnpike Enterprise will coordinate with SFWMD staff and CERP project managers during the PD&E study.

**Degree of Effect:** 3 Moderate assigned 03/02/2017 by Mindy Parrott, South Florida Water Management District

**Coordination Document:** To Be Determined: Further Coordination Required

### Coordination Document Comments:

For these issues additional coordination is required, especially if new on ramps/ exits or expanded intersections are proposed, or if the project components will occur outside of the current FDOT right of way. Coordination with CERP project managers, SFWMD land management staff, SFWMD Right of Way permitting staff and FDEP state park staff is highly recommended.

Also, for the area of the project within the Loxahatchee River, review by the Loxahatchee River Management Coordinating Council is needed.

## Direct Effects

### Identified Resources and Level of Importance:

The project is within or adjacent to substantial public lands that serve as important habitat and are also used for recreation:

- Jonathan Dickinson State Park,
- the Wild and Scenic section of the Loxahatchee River
- the Loxahatchee River Aquatic Preserve.
- SFWMD Lands: Cypress Creek and Loxahatchee River Management Area and the Ten Mile Creek parcels.

The Ten Mile Creek area and the Loxahatchee River area are part of a CERP project.

The C-23, C-24 and C-25 canals are SFWMD Right of Way.

There are several existing conservation easements dedicated to the SFWMD adjacent to the Turnpike Right of Way.

The Loxahatchee River, Cypress Creek, South Fork of the St. Lucie River, Ten Mile Creek, possibly Bessey Creek are likely state owned lands that require public easements from the Board of Trustees, or modification of existing easements.

### Comments on Effects to Resources:

To protect the Loxahatchee River, there are additional criteria that apply for aquatic preserves. Use of submerged lands also has additional criteria, as do use of SFWMD Right of Way.

The Loxahatchee River and Ten Mile Creek area are both CERP -related projects. While designing the project, efforts should be taken to avoid construction outside of the existing right of ways in these areas.

### Additional Comments (optional):

For these issues additional coordination is required, especially if new on ramps/ exits or expanded intersections are proposed, or if the project components will occur outside of the current FDOT right of way. Coordination with CERP project managers, SFWMD land management staff, SFWMD Right of Way permitting staff and FDEP state park staff is highly recommended.

Also, for the area of the project within the Loxahatchee River, review by the Loxahatchee River Management Coordinating Council is needed.

### CLC Commitments and Recommendations:

**Degree of Effect:** 3 *Moderate* assigned 03/10/2017 by Anita Barnett, National Park Service

**Coordination Document:** To Be Determined: Further Coordination Required

#### **Direct Effects**

##### **Identified Resources and Level of Importance:**

The National Park Service has reviewed the Efficient Transportation Decision Making (ETDM) project # 14295 for Turnpike Mainline Widening from Juniper to Fort Pierce and offers the following comments:

##### **Wild and Scenic Rivers**

The Loxahatchee River was designated as a National Wild and Scenic River under Section 2(a)(ii) of the Wild and Scenic Rivers Act in 1985. It was designated by the Secretary of the Interior at the request of the Governor of the State of Florida, and is therefore considered a "state-administered" Wild and Scenic River. Although the federal government is precluded by Section 2(a)(ii) from owning lands or directly managing the river corridor, the National Park Service (NPS) is tasked with ensuring that any federally-assisted, permitted, licensed, or funded water resources project (e.g., bridges or highway spans) will not adversely affect the values for which the river was designated. In accordance with Section 7(a) of the Wild and Scenic Rivers Act, the NPS must conclude, based on information provided by the project proponent that the project will not have a "direct and adverse" effect on the free flowing condition, water quality, or the outstandingly remarkable values for which the Loxahatchee River was designated. Open consultation in the early stages of the project will be paramount to ensuring a timely and successful conclusion to project planning. Please coordinate with Jeffrey R. Duncan, PhD, at National Park Service-Southeast Region, Science and Natural Resources Division, Fisheries and Aquatic Resource, 100 West Martin Luther King, Jr. Blvd, Suite 214, Chattanooga, TN 37402. Mr. Duncan can be reached at 423-987-6127.

##### **Land and Water Conservation Fund**

The following Land and Water Conservation Fund sites are within the area of potential affect: 1) Phipp park and Recreation, Land and Water Conservation Fund (LWCF) site # 12-00626 is within 200 feet of the proposed widening, 2) South Fork St Lucie River Park has changed its name to Sandsprit Park, LWCF site # 12-00300 is with located with 500 feet, 3&4) Both the Community Center Park LWCF# 12-00457 and Cypress Creek Natural Area, LWCF # 12-00627 are within a mile of the proposed project.

Further information is needed to determine the degree of effect and mitigation. We recommend that an EA or EIS and 4F evaluation be prepared. Any right of way needed from the LWCF sites listed above, in whole or in part, that converts the use of a portion of the park to other than public outdoor recreation, would constitute a Section 6(f)(3) conversion. A conversion of use will include the Florida Department of Transportation providing replacement property that not only is equal or greater in fair market value to the converted site, but also, is of reasonable equivalent usefulness. Also, all NEPA requirements must be satisfactorily completed as well as other requirements as outlined in the LWCF Act (36 CFR 59.3)

If a conversion should occur, the Florida Department of Environmental Protection, Division of State Lands, 3900 Commonwealth Blvd, Tallahassee, FL 32399-3000, should be contacted for early coordination. If you have any questions concerning a conversion, contact Linda Reeves, Operations Management Consultant Manager--linda.reeves@dep.state.fl.us--(850) 245-2501.

Please keep us informed as the project progresses through the PD&E phase and if the proposed project changes please contact Anita Barnett at National Park Service, 100 Alabama Street, 1924 Bldg., Atlanta Georgia, 30303, 404-507-5706; Anita\_Barnett@nps.gov. Thank you for the opportunity to review and provide comments.

##### **Comments on Effects to Resources:**

The National Park Service has reviewed the Efficient Transportation Decision Making (ETDM) project # 14295 for Turnpike Mainline Widening from Juniper to Fort Pierce and offers the following comments:

##### **Wild and Scenic Rivers**

The Loxahatchee River was designated as a National Wild and Scenic River under Section 2(a)(ii) of the Wild and Scenic Rivers Act in 1985. It was designated by the Secretary of the Interior at the request of the Governor of the State of Florida, and is therefore considered a "state-administered" Wild and Scenic River. Although the federal government is precluded by Section 2(a)(ii) from owning lands or directly managing the river corridor, the National Park Service (NPS) is tasked with ensuring that any federally-assisted, permitted, licensed, or funded water resources project (e.g., bridges or highway spans) will not adversely affect the values for which the river was designated. In accordance with Section 7(a) of the Wild and Scenic Rivers Act, the NPS must conclude, based on information provided by the project proponent that the project will not have a "direct and adverse" effect on the free flowing condition, water quality, or the outstandingly remarkable values for which the Loxahatchee River was designated. Open consultation in the early stages of the project will be paramount to ensuring a timely and successful conclusion to project planning. Please coordinate with Jeffrey R. Duncan, PhD, at National Park Service-Southeast Region, Science and Natural Resources Division, Fisheries and Aquatic Resource, 100 West Martin Luther King, Jr. Blvd, Suite 214, Chattanooga, TN 37402. Mr. Duncan can be reached at 423-987-6127.

##### **Land and Water Conservation Fund**

The following Land and Water Conservation Fund sites are within the area of potential affect: 1) Phipp park and Recreation, Land and Water

Conservation Fund (LWCF) site # 12-00626 is within 200 feet of the proposed widening, 2) South Fork St Lucie River Park has changed its name to Sandsprit Park , LWCF site # 12-00300 is with located with 500 feet, 3&4) Both the Community Center Park LWCF# 12-00457 and Cypress Creek Natural Area, LWCF # 12-00627 are within a mile of the proposed project.

Further information is needed to determine the degree of effect and mitigation. We recommend that an EA or EIS and 4F evaluation be prepared. Any right of way needed from the LWCF sites listed above, in whole or in part, that converts the use of a portion of the park to other than public outdoor recreation, would constitute a Section 6(f)(3) conversion. A conversion of use will include the Florida Department of Transportation providing replacement property that not only is equal or greater in fair market value to the converted site, but also, is of reasonable equivalent usefulness. Also, all NEPA requirements must be satisfactorily completed as well as other requirements as outlined in the LWCF Act (36 CFR 59.3)

If a conversion should occur, the Florida Department of Environmental Protection, Division of State Lands, 3900 Commonwealth Blvd, Tallahassee, FL 32399-3000, should be contacted for early coordination. If you have any questions concerning a conversion, contact Linda Reeves, Operations Management Consultant Manager--linda.reeves@dep.state.fl.us--(850) 245-2501.

Please keep us informed as the project progresses through the PD&E phase and if the proposed project changes please contact Anita Barnett at National Park Service, 100 Alabama Street, 1924 Bldg., Atlanta Georgia, 30303, 404-507-5706; Anita\_Barnett@nps.gov . Thank you for the opportunity to review and provide comments.

#### Additional Comments (optional):

#### CLC Commitments and Recommendations:

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## ETAT Reviews and Coordinator Summary: Natural

### Wetlands and Surface Waters

#### Project Effects

**Coordinator Summary Degree of Effect:** 3 *Moderate* assigned 05/12/2017 by Florida's Turnpike Enterprise

#### Comments:

During the PD&E phase, a wetland evaluation will be conducted as part of the Natural Resource Evaluation (NRE) report, in accordance with Part 2, Chapter 18 of the FDOT PD&E Manual, to determine the potential adverse impacts to wetlands. All necessary measures will be taken to avoid and/or minimize impacts to wetlands to the greatest extent feasible during project design. Should avoidance and/or minimization not be feasible, a mitigation plan will be prepared. An Environmental Resource Permit (ERP) will be obtained for the project, as well as a Section 404 Permit.

Florida's Turnpike Enterprise will continue to coordinate with the US Army Corps of Engineers (USACE), United States Environmental Protection Agency (USEPA), United States Fish and Wildlife Service (USFWS), Florida Fish and Wildlife Conservation Commission (FWC), Florida Department of Environmental Protection (FDEP), South Florida Water Management District(SFWMD), and National Marine Fisheries Service (NMFS) during the PD&E and design phases of the project.

**Degree of Effect:** 4 *Substantial* assigned 03/22/2017 by Kim Gates, US Environmental Protection Agency

**Coordination Document:** PD&E Support Document As Per PD&E Manual

#### Coordination Document Comments:

Water Quality Impact Evaluation (PD&E Manual, Part 2, Chapter 20), and Wetlands Evaluation Report (PD&E Manual, Part 2, Chapter 18).

#### Direct Effects

#### Identified Resources and Level of Importance:

Wetlands and other surface waters provide important and beneficial functions, including providing essential fish and wildlife habitat, buffering water quality impacts, storing floodwaters, and maintaining surface water flow during dry periods. However, "[a]s development increased and more paved areas covered the land, stormwater runoff became the primary source of pollution to surface waters in Florida"

([http://www.sfwmd.state.fl.us/publications/files/stormwater\\_systems.pdf](http://www.sfwmd.state.fl.us/publications/files/stormwater_systems.pdf)). The most common contaminants in highway runoff are heavy metals, inorganic salts, volatile organics, petroleum hydrocarbons, bacteria, pesticides/herbicides, and suspended solids that accumulate on the road surface as a result of regular highway operation and maintenance activities.

Various federal, state, and local laws have been enacted to protect surface water resources. The Clean Water Act (CWA) established the basic structure for regulating discharges of pollutants into the waters of the United States (which include wetlands) and provides statutory authority for various regulatory programs. CWA Section 402 requires permitting of all municipal, industrial and commercial facilities that discharge wastewater or stormwater directly from a point source into a surface water of the United States. These National Pollutant Discharge Elimination System (NPDES) permits are written to ensure receiving waters will achieve Water Quality Standards established pursuant to CWA Section 303(c). In October 2000, the USEPA delegated authority to the Florida Department of Environmental Protection (FDEP) to implement the NPDES permitting program, which includes Municipal Separate Storm Sewer System (MS4) permits.

A separate type of permit is required to dispose of dredged or fill material in the nation's waters, including wetlands. Authorized by CWA Section 404, this permit program is administered by the U.S. Army Corps of Engineers, subject to and using the Section 404(b)(1) Guidelines developed by the USEPA in coordination with the Corps and codified in 40 CFR Part 230 (<https://www.epa.gov/cwa-404/section-404b1-guidelines-40-cfr-230>).

**Comments on Effects to Resources:**

As described in the Preliminary Environmental Discussion Comments Report (PED), the EST identified approximately 382 acres of Palustrine wetlands, 51 acres of Riverine wetlands, 34.5 acres of Lacustrine wetlands and 17 acres of Estuarine wetlands within the 500-foot buffer.

**Additional Comments (optional):**

Water Quality Impact Evaluation (PD&E Manual, Part 2, Chapter 20), and Wetlands Evaluation Report (PD&E Manual, Part 2, Chapter 18).

**CLC Commitments and Recommendations:**

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**Degree of Effect:** 3 *Moderate* assigned 01/25/2017 by John Wrublik, US Fish and Wildlife Service

**Coordination Document:** To Be Determined: Further Coordination Required

**Direct Effects**

**Identified Resources and Level of Importance:**

Wetlands

**Comments on Effects to Resources:**

Wetlands provide important habitat for fish and wildlife. Wetlands may occur within and near the project site. We recommend that these valuable resources be avoided to the greatest extent practicable. If impacts to these wetlands are unavoidable, we recommend the FDOT provide mitigation that fully compensates for the loss of important resources.

**Additional Comments (optional):**

**CLC Commitments and Recommendations:**

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**Degree of Effect:** 3 *Moderate* assigned 02/14/2017 by Brandon Howard, National Marine Fisheries Service

**Coordination Document:** Tech Memo Required

**Coordination Document Comments:**

EFH Assessment

**Direct Effects**

**Identified Resources and Level of Importance:**

Magnuson-Stevens Act and Fish and Wildlife Coordination Act: Where the following waterbodies and the Turnpike cross are essential fish habitat (EFH):

- The Loxahatchee River and its tributary crossings at the following 3 locations: 26.954450 , -80.165199 ; 26.964772 , -80.173156 ; and 26.972063 , -80.178423 .
- The South Fork of the St. Lucie River at 27.117673 , -80.274832 .
- The tributary to the South Fork of the St. Lucie River at 27.128295 , -80.282304 .

- The South Florida Water Management District canal at 27.262455 , -80.352264 .
- Ten Mile Creek at 27.402611 , -80.397842 .

These water bottoms and wetlands are EFH for white shrimp (*Litopenaeus setiferus*). These habitats vary in quality from low to very high. The South Atlantic Fishery Management Council (SAFMC) designates forested palustrine wetlands as EFH for juvenile white shrimp. If FDOT requires additional information on the EFH in the project area, the South Atlantic Fishery Management Council provides detailed information on EFH in amendments to fishery management plans and in *Fishery Ecosystem Plan of the South Atlantic Region* (available at [www.safmc.net](http://www.safmc.net)).

**Comments on Effects to Resources:**

Impacts to these wetlands should be sequentially avoided, minimized, and compensated with in-kind mitigation. The NMFS is not aware of a mitigation bank servicing the area providing tidal freshwater credits. Freshwater wetlands not receiving tidal influence impacted by the project may be offset at mitigation banks after avoidance and minimization have taken place. If the project continues to PD&E without this sequential mitigation, NMFS would likely find it necessary to issue EFH conservation recommendations.

With construction of the new lanes, impervious surface area will be replaced or expanded. Surface and stormwater runoff into the surrounding waters may result. The discharge of hydrocarbons and other contaminants may degrade water quality. Subsequently, NOAA trust resources located in the receiving waters could be adversely affected. To the extent practicable, runoff from the new roads should be treated before being discharged into the canal.

**Additional Comments (optional):**

EFH Assessment

**CLC Commitments and Recommendations:**

**Degree of Effect:** 3 *Moderate* assigned 02/22/2017 by Tarrie L Ostrofsky, US Army Corps of Engineers

**Coordination Document:** Permit Required

**Coordination Document Comments:**

According to the information provided, there would be impacts to waters of the United States for this proposed project. Therefore, a Section 404 permit would be required. This may be a Nationwide Permit, Regional General Permit, or a Standard Permit, depending on the proposed impact totals.

**Direct Effects**

**Identified Resources and Level of Importance:**

According to the information provided, the National Wetlands Inventory data indicated that there are approximately 382 acres of Palustrine wetlands, 51 acres of Riverine wetlands, 35 acres of Lacustrine wetlands and 17 acres of Estuarine wetlands within the 500-foot buffer. Also, National Inventory of Dams data indicates that there is one Major Dam located within the 500-foot project buffer (Control Structure #2/Gordy Road Structure). Control Structure #2 is owned by St. Lucie County and is within the North St. Lucie Water Control District. Given the high acreage of wetlands identified, the initial determination of the potential direct effects on resources is moderate. Moderate has been selected due to the potential that much of the identified wetland acreage may be located near the outer limits of the 500-foot buffer and may not be directly impacted. Also, given the nature of the project being to widen an existing roadway, it is likely that the wetlands located adjacent to the existing roadway have been disturbed and possibly maintained.

**Comments on Effects to Resources:**

Direct effects to resources would involve permanent filling of wetlands and surface waters for the placement of the roadway. Temporary direct effects may occur for construction activities for access, temporary staging areas, best management practices, etc. Hydrology may be directly affected in areas where permanent fill is proposed. Crossings of streams may result in direct fill in the waterways for the construction of the crossings. Reduced channel widths may also result. If surface waters are present and filled, but not re-established elsewhere within the project limits, hydrology would be further affected.

**Additional Comments (optional):**

According to the information provided, there would be impacts to waters of the United States for this proposed project. Therefore, a Section 404 permit would be required. This may be a Nationwide Permit, Regional General Permit, or a Standard Permit, depending on the proposed impact totals.

## CLC Commitments and Recommendations:

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**Degree of Effect:** 3 *Moderate* assigned 03/02/2017 by Mindy Parrott, South Florida Water Management District

**Coordination Document:** Permit Required

**Coordination Document Comments:**

An ERP is required. Pre- application meetings are strongly encouraged.

### Direct Effects

#### Identified Resources and Level of Importance:

There are wetlands and mitigation areas along the Turnpike in Palm Beach, Martin and St. Lucie Counties. There are wetlands on state park and other state and county preservelands adjacent or near the Turnpike in these counties. Wetlands associated with rivers and creeks are of particular concern due to downstream connectivity for water, fish and wildlife.

#### Comments on Effects to Resources:

Without information on the potential new interchanges, locations of stormwater facilities and bridge widening needs, it is difficult to determine the effects of this project. If significant widening is proposed within wetlands or within preserve lands the level of effect may be Substantial. Reduction and elimination of impacts, particularly in the Loxahatchee River and other river/ creek systems is important. The project must meet the criteria in the ERP Applicant's Handbook Vol. I, Section 10 and additional criteria for Outstanding Florida Waters and Aquatic Preserves, including mitigation. Impacts to wetlands under conservation easement may require additional mitigation.

#### Additional Comments (optional):

An ERP is required. Pre- application meetings are strongly encouraged.

## CLC Commitments and Recommendations:

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**Degree of Effect:** 3 *Moderate* assigned 03/03/2017 by Suzanne E. Ray, FL Department of Environmental Protection

**Coordination Document:** PD&E Support Document As Per PD&E Manual

### Direct Effects

#### Identified Resources and Level of Importance:

According to the information provided, the National Wetlands Inventory data indicated that there are approximately 382 acres of Palustrine wetlands, 51 acres of Riverine wetlands, 35 acres of Lacustrine wetlands and 17 acres of Estuarine wetlands within the 500-foot buffer. There are wetlands on state park and other state and county preservelands adjacent or near the Turnpike in these counties. Wetlands associated with rivers and creeks are of particular concern due to downstream connectivity for water, fish and wildlife.

#### Comments on Effects to Resources:

With construction of the new lanes, impervious surface area will be replaced or expanded. Surface and stormwater runoff into the surrounding waters may result. Runoff from the new roads should be treated before being discharged into the canal. If new on-ramps/exits or expanded intersections are proposed, or if the project components will occur outside of the current FDOT right of way, coordination with CERP project managers, SFWMD land management and permitting staff and FDEP state park staff is highly recommended.

#### Additional Comments (optional):

## CLC Commitments and Recommendations:

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## Water Quality and Quantity

### Project Effects

**Coordinator Summary Degree of Effect:** 3 *Moderate* assigned 05/12/2017 by Florida's Turnpike Enterprise

#### Comments:

The PD&E phase will include a Water Quality Impact Evaluation (WQIE) in accordance with Part 2, Chapter 20 of the FDOT PD&E Manual which will identify potential effects on the surface and groundwater resources, identify the impaired waters and other waterbody classifications (Class I, II, Outstanding Florida Water, etc.) that could be affected by this project. In addition, a pond siting evaluation will be conducted to identify alternatives for stormwater management and treatment. The effects on water quality and means to avoid, minimize and mitigate impacts will be evaluated during the study based on the project-specific effects from the alternatives developed during the study. Florida's Turnpike Enterprise will continue to coordinate with the United States Environmental Protection Agency (USEPA), United States Army Corps of Engineers (USACE), South Florida Water Management



District (SFWMD), and Florida Department of Environmental Protection (FDEP) during PD&E and Design phases of the project.

**Degree of Effect:** **3** *Moderate* assigned 03/02/2017 by Mindy Parrott, South Florida Water Management District

**Coordination Document:** Permit Required

**Coordination Document Comments:**

An Environmental Resource Permit is required. Existing SFWMD Permit 43-00568-S in Martin County may be modified. A pre-application meeting is strongly encouraged.

#### Direct Effects

**Identified Resources and Level of Importance:**

Surface waters, including Outstanding Florida Waters and waters that may discharge to Aquatic Preserves.

**Comments on Effects to Resources:**

The project should be designed to meet the storm water quality and quantity criteria of the ERP Applicant's Handbook Vols. I & II, including Vol. II, Appendix E: Procedure for Environmental Resource Permit Water Quality Evaluations for Applications Involving Discharges to Outstanding Florida Waters and Water Bodies that Do Not Meet State Water Quality Standards.

**Additional Comments (optional):**

An Environmental Resource Permit is required. Existing SFWMD Permit 43-00568-S in Martin County may be modified. A pre-application meeting is strongly encouraged.

**CLC Commitments and Recommendations:**

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**Degree of Effect:** **4** *Substantial* assigned 03/22/2017 by Kim Gates, US Environmental Protection Agency

**Coordination Document:** PD&E Support Document As Per PD&E Manual

**Coordination Document Comments:**

Water Quality Impact Evaluation (PD&E Manual, Part 2, Chapter 20)

#### Direct Effects

**Identified Resources and Level of Importance:**

"Untreated stormwater runoff is now considered the state's leading source of pollution"

(<http://www.broward.org/WATERMATTERS/Pages/waterquality.aspx>). Stormwater from impervious surfaces in urban environments, including roadways, conveys contaminants to surface water bodies, wetlands, and groundwater. The most common pollutants in highway runoff are heavy metals, inorganic salts, volatile organics, petroleum hydrocarbons, bacteria, pesticides/herbicides, and suspended solids that accumulate on the road surface as a result of regular highway operation and maintenance activities.

The principal law governing pollution of the nation's surface waters is the Federal Water Pollution Control Act, or Clean Water Act. Prior to 1987, surface water protection programs were primarily directed at point source pollution (i.e., wastes discharged from discrete sources, such as pipes from manufacturing facilities and wastewater treatment plants). Recognizing the need to address nonpoint source pollution, including stormwater, the U.S. Congress revised the Clean Water Act in 1987. The USEPA responded to this legislation by implementing the Municipal Separate Storm Sewer System (MS4) permitting program via the Phase I (1990) and Phase II (1999) stormwater regulations. In October 2000, the USEPA delegated authority to the Florida Department of Environmental Protection (FDEP) to implement the MS4 programs.

As a permitted Phase II MS4 operator in the St. Lucie watershed (Permit #FLR04E049) and co-permittee on Palm Beach County's Phase I MS4 permit (#FLS000018), Florida's Turnpike Enterprise (FTE) is required to develop and implement a comprehensive Stormwater Management Program (SWMP) that includes illicit discharge detection and control, construction and post-construction runoff control, pollution prevention measures, and public education and involvement. The specific SWMP requirements are discussed in the FDOT Statewide Stormwater Management Plan for Phase I MS4s (<http://www.fdot.gov/maintenance/FDOTStormWaterMgmtPlan2012.pdf>) and on FDEP's website for Phase II MS4s ([http://www.dep.state.fl.us/water/stormwater/npdes/MS4\\_5.htm](http://www.dep.state.fl.us/water/stormwater/npdes/MS4_5.htm)).

In addition to the delegated NPDES program, the State administers its own Environmental Resource Permitting (ERP) program for activities involving the alteration of surface water flows (<http://www.dep.state.fl.us/WATER/wetlands/erp/index.htm>). The ERP program is implemented by the Florida Department of Environmental Protection (FDEP) and the five Water Management Districts.

**Comments on Effects to Resources:**

As discussed in FDOT's PD&E Manual (Part 1, Chapter 3, page 3-7), the Preliminary Environmental Discussion Comments Report (PED) should include "a brief description of existing stormwater treatment, additional treatment which may be required and the possible options for treatment." The PD&E Manual (in Part 2, Chapter 20) also specifies inclusion of the following surface water information in the PED:

- Identification of surface waterbody to which the stormwater ultimately discharges;
- Any special designations of receiving waterbodies (Outstanding Florida Water (OFW), Aquatic Preserve, etc.);
- Whether the project is within a permitted MS4;
- Waterbody Identification Number(s) (WBIDs) in which the project is located, and associated FDEP Group Number and Name;
- Water Management District (WMD) in which the project is located;
- Water Control District (if applicable);
- Waterbody Class (e.g., Class I, II, III, etc.);
- Listing status (i.e., whether the WBID is identified as impaired, has a TMDL and/or is in a BMAP or RAP);
- The appropriate numeric nutrient criteria waterbody classification and related numeric nutrient limits (e.g., TMDL, Lakes, Spring Vents, Streams, Estuaries, etc.) if applicable; and
- If project discharges to a waterbody identified as impaired, identify the pollutant(s) of concern, numeric criteria or TMDL (whichever applies).

However, this information was not provided in the project documentation. Verified impaired waters in the 500-foot buffer area were identified, but the PED did not indicate if stormwater outfalls discharged to any of them. In light of the requirement in Chapter 62-624, F.A.C., to maintain an inventory describing all existing controls and major outfalls that discharge from MS4s, this information should be readily available. Please discuss the current stormwater management system in the ETDM Summary Report.

Furthermore, "if a TMDL is approved for any waterbody into which the MS4 discharges, and the TMDL includes requirements for control of stormwater discharges, the operator must review its stormwater management program for consistency with the TMDL allocation" ([http://www.dep.state.fl.us/water/stormwater/npdes/docs/Phase\\_II\\_MS4\\_GP.pdf](http://www.dep.state.fl.us/water/stormwater/npdes/docs/Phase_II_MS4_GP.pdf)). Although the GIS Analysis Report generated by the EST listed 7 TMDLs in the 100-foot buffer area and another one in the 200-foot buffer, none of them were discussed in the PED - nor was the Basin Management Action Plan (BMAP) for implementation of the nutrients and DO TMDLs in the St. Lucie River and Estuary Basin (<http://www.dep.state.fl.us/water/watersheds/docs/bmap/stlucie-estuary-nutr-bmap.pdf>). Despite the *de minimis* Total Nitrogen and Total Phosphorus load allocations, the BMAP should have been acknowledged in the PED. At a minimum, please discuss the TMDLs with Wasteload Allocations (WLAs) assigned to FTE in the ETDM Summary Report.

**Additional Comments (optional):**

Water Quality Impact Evaluation (PD&E Manual, Part 2, Chapter 20)

**CLC Commitments and Recommendations:**

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**Floodplains**

**Project Effects**

**Coordinator Summary Degree of Effect:** 3 *Moderate* assigned 05/12/2017 by Florida's Turnpike Enterprise

**Comments:**

Improvements to the Turnpike mainline may result in impacts to floodplains. An analysis of the potential floodplain effects will be conducted in accordance with the Part 2, Chapter 24 of the FDOT PD&E Manual. All new floodplain impacts and previously permitted floodplain impacts will be adequately mitigated for, in accordance with the South Florida Water Management District (SFWMD) Applicant's Handbook Volumes I & II, including Volume II, Appendix E: Procedure for Environmental Resource Permit Water Quality Evaluations for Application Involving Discharges to Outstanding Florida Waters and Water Bodies that Do Not Meet State Water Quality Standards. A Location Hydraulics Report will be prepared during the PD&E phase to determine potential impacts to area floodplains.

**Degree of Effect:** 3 *Moderate* assigned 03/02/2017 by Mindy Parrott, South Florida Water Management District

**Coordination Document:** Permit Required

**Coordination Document Comments:**

An Environmental Resource Permit is required. Existing SFWMD Permit 43-00568-S in Martin County may be modified. A pre-application meeting is strongly encouraged.



## Direct Effects

### Identified Resources and Level of Importance:

Surface waters, including Outstanding Florida Waters and waters that may discharge to Aquatic Preserves.

### Comments on Effects to Resources:

The project should be designed to meet the storm water quality and quantity criteria of the ERP Applicant's Handbook Vols. I & II, including Vol. II, Appendix E: Procedure for Environmental Resource Permit Water Quality Evaluations for Applications Involving Discharges to Outstanding Florida Waters and Water Bodies that Do Not Meet State Water Quality Standards.

### Additional Comments (optional):

An Environmental Resource Permit is required. Existing SFWMD Permit 43-00568-S in Martin County may be modified. A pre-application meeting is strongly encouraged.

### CLC Commitments and Recommendations:

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## Wildlife and Habitat

### Project Effects

**Coordinator Summary Degree of Effect:** 3 *Moderate* assigned 05/12/2017 by Florida's Turnpike Enterprise

#### Comments:

A Natural Resources Evaluation (NRE) will be prepared in accordance with Part 2, Chapter 27, of the FDOT PD&E Manual. Surveys will be conducted for listed species potentially occurring in the study area, and the effects on the listed species will be evaluated. Avoidance, minimization and mitigation for unavoidable impacts will be assessed during the alternatives development. Best management practices will be used to avoid adverse impacts to wetlands, aquatic resources, and wildlife habitats. Florida's Turnpike Enterprise will continue to coordinate with the Florida Department of Agriculture and Consumer Services (FDACS), Florida Fish and Wildlife Conservation Commission (FWC), and United States Fish and Wildlife Service (USFWS) during the PD&E phase.

**Degree of Effect:** 4 *Substantial* assigned 02/28/2017 by Scott Sanders, FL Fish and Wildlife Conservation Commission

**Coordination Document:** To Be Determined: Further Coordination Required

## Direct Effects

### Identified Resources and Level of Importance:

Florida Fish and Wildlife Conservation Commission (FWC) staff has reviewed ETDM #14295, St. Lucie, Martin, and Palm Beach Counties, and provides the following comments related to potential effects to fish and wildlife resources of this Programming Phase project.

The Project Description Summary states that this project involves the widening Florida's Turnpike from four lanes to eight lanes within the project limits by adding two express lanes in each direction. The total length of the project is approximately 37 miles. Since the Turnpike has no expansion space in the median, construction will involve expansion along the outer edges of the highway. Numerous new or expanded bridges will be required over streams and canals, with the most notable being Tenmile Creek, Rim Ditch, Winters Creek, County Line Canal, Danforth Creek, Mapps Creek, Saint Lucie Canal, Cypress Creek, and the Loxahatchee River.

An assessment of the project area was performed on lands within 500 feet of the proposed alignment to determine potential impacts to habitat which supports listed species and other fish and wildlife resources. Our inventory included a review of aerial and ground-level photography, various wildlife observation and landcover data bases, along with coordination with FWC biologists and other State and Federal agencies. A GIS analysis was performed using the Florida Department of Transportation's (FDOT) Environmental Screening Tool to determine the potential quality and extent of upland and wetland habitat, and other wildlife and fisheries resource information. We have reviewed the Preliminary Environmental Discussion Comments Report provided by the FDOT, and offer the following comments and recommendations.

Although 63.7% of the assessment area is classified as Urban landcover, primarily in the City of Port St. Lucie, a variety of other landcover types in the study area include: Agriculture (9.49%, 422.47 acres), Mesic Flatwoods (8.29%, 372.95 acres), Cultural-Lacustrine (stormwater and borrow lakes - 3.67%, 165.01 acres), Wet Prairie (2.41%, 108.6 acres), Rural (2.02%, 91.03 acres), Cultural-Riverine (canals - 1.40%, 63.08 acres), Pine Flatwoods and Dry Prairie (1.32%, 59.16 acres), Wet Flatwoods (1.18%, 52.9 acres), Marshes (1.16%, 52.11 acres), Freshwater Forested Wetlands (1.04%, 46.95 acres), Shrub and Brushland (0.86%, 38.7 acres), Mixed Hardwood-Coniferous (0.72%, 32.49 acres), Exotic Plants (0.65%, 29.33 acres), Palmetto Prairie (0.48%, 21.79 acres), Cypress (0.39%, 17.72 acres), Cypress/Tupelo (0.38%, 17.27 acres), Natural Rivers and Streams (0.17%, 7.78 acres), Freshwater Non-Forested Wetlands (0.16%, 7.09 acres), Scrubby Flatwoods (0.14%, 6.45 acres), High Pine and Scrub (0.13%, 5.68 acres), Scrub

(0.11%, 4.99 acres), Other Hardwood Wetlands (0.09%, 3.98 acres), Bare Soil (0.06%, 2.77 acres), Riverine (0.05%, 2.08 acres), and Dome Swamp (0.01%, 0.37 acres).

The most valuable wildlife habitats within the project area are within the complex of public conservation lands adjacent to the southern portion of the project. Most prominent of these is the 11,459-acre Jonathan Dickinson State Park, managed by the Department of Environmental Protection's Division of Parks and Recreation. The Turnpike crosses the streambeds and forested floodplains of the Loxahatchee River and its tributary Cypress Creek within the boundaries of the state park. The Loxahatchee River is one of only two rivers in Florida federally-designated as a Wild and Scenic River. Other adjacent lands in this area include the 16,969-acre John C. and Mariana Jones/Hungryland Wildlife and Environmental Area, managed by the FWC and owned by the South Florida Water Management District (SFWMD), the 4,438-acre Cypress Creek/Loxahatchee tract, owned and managed by SFWMD, and the 2,083-acre Cypress Creek Natural Area, owned and managed by Palm Beach County. All of these public lands were purchased for the protection of unique natural systems with native plant communities that provide excellent habitat for fish and wildlife resources. Additional lands in this area are proposed for acquisition as part of the Pal-Mar Florida Forever project.

Other public conservation lands adjacent to the Turnpike right-of-way (ROW) include Ten Mile Creek Natural Area, Varn Parcel, and Hendler Parcels, all owned and managed by St. Lucie County; Ten Mile Creek, owned and managed by the SFWMD; and Phipps Park Conservation Area, owned and managed by Martin County. Not adjacent, but within the 500-foot assessment area is the South Fork Addition, owned by the SFWMD and managed by Martin County.

Based on range and preferred habitat type, the following species listed by the Federal Endangered Species Act and the State of Florida as Federally Endangered (FE), Federally Threatened (FT), State-Threatened (ST), or State Species of Special Concern (SSC) may occur along the project area: American alligator (FT based on similarity of appearance to American crocodile), Eastern indigo snake (FT), crested caracara (FT), snail kite (FE), Florida scrub jay (FT), red-cockaded woodpecker (FE), wood stork (FT), Florida manatee (FE), Florida pine snake (ST), gopher tortoise (ST), burrowing owl (ST), southeastern American kestrel (ST), Florida sandhill crane (ST), least tern (FT), little blue heron (ST), tricolored heron (ST), roseate spoonbill (ST), and Sherman's fox squirrel (SSC). All of these species either likely or potentially utilize appropriate habitats in the project vicinity.

The GIS analysis revealed several specific characteristics associated with lands along the project alignment that provide an indication of potential habitat quality or sensitivity that will require field studies to verify the presence or absence of listed wildlife species and the quality of wildlife habitat resources. Within the assessment area, 1,182.66 acres (26.30%) are classified by the FWC's Integrated Wildlife Habitat Ranking System as high, moderately high, or medium value, and 632.69 acres (14.06%) have a medium or moderately high value on the FWC's Potential Habitat Richness ranking. The FWC's Strategic Habitat Conservation Area Priority Rankings classify 43.81 acres of the assessment area as high or medium value, based on habitat suitability for either the swallow-tailed kite, Cooper's hawk, or the snail kite. In the Florida Natural Areas Inventory Critical Lands and Waters Identification Project (CLIP), 349.38 acres (7.77%) is ranked Priority 1 or 2 (high) for Biodiversity Resources. Drainage basins crossed by the project are inhabited by the ironcolor shiner, river goby, and opossum pipefish, which are species classified as Rare and Imperiled Fish. The project is within U.S. Fish and Wildlife Service Consultation Areas for Caracara, Florida Grasshopper Sparrow, Scrub Jay, Red-cockaded Woodpecker, Snail Kite, and Manatee, and the core foraging area of wood stork rookeries.

Primary wildlife issues associated with this project include: potential loss of valuable floodplain swamp habitat within Jonathan Dickinson State Park resulting from expansion of the Loxahatchee River Bridge and the Cypress Creek Bridge; potential loss of public conservation lands and valuable wildlife habitat via expansion of the ROW; potential adverse effects to a moderate number of species listed by the Federal Endangered Species Act as Endangered or Threatened, or by the State of Florida as Threatened or Species of Special Concern; potential impacts to manatees from in-water work associated with construction of numerous bridges over waterways with manatee access; potential for increased wildlife roadkill; and potential water quality degradation as a result of additional stormwater runoff from the expanded impervious surface draining into adjacent wetlands, streams, canals, the Loxahatchee River, the St. Lucie River, and the Indian River Lagoon.

#### **Comments on Effects to Resources:**

Based on the project information provided, we believe that direct and indirect effects of this project could be substantial, because the 37-mile length of proposed construction will impact a wide variety of natural wildlife habitats along the edge of the ROW, including public conservation lands. Effects of the project could be reduced by confining construction to areas of cleared ROW to the greatest degree possible, placing any new drainage retention areas (DRAs) away from areas of undisturbed natural habitat, and adopting Best Management Practices in the project design to avoid degradation of adjacent or downstream water quality.

#### **Additional Comments (optional):**

#### **CLC Commitments and Recommendations:**

**Degree of Effect:** 3 *Moderate* assigned 01/25/2017 by John Wrublik, US Fish and Wildlife Service

**Coordination Document:** To Be Determined: Further Coordination Required

#### **Direct Effects**

**Identified Resources and Level of Importance:**

Federally listed species and fish and wildlife resources

**Comments on Effects to Resources:**

Federally-listed species -

The Service has reviewed our Geographic Information Systems (GIS) database for recorded locations of Federally listed threatened and endangered species on or adjacent to the project study area. The GIS database is a compilation of data received from several sources. Based on review of our GIS database, the Service notes that the following Federally listed species may occur in or near the project area.

**Wood Stork**

The project corridor is located in the Core Foraging Areas (CFA)(within 18.6 miles ) of five active nesting colonies of the endangered wood stork (*Mycteria americana*). The Service believes that the loss of wetlands within a CFA due to an action could result in the loss of foraging habitat for the wood stork. To minimize adverse effects to the wood stork, we recommend that any lost foraging habitat resulting from the project be replaced within the CFA of the affected nesting colony. Moreover, wetlands provided as mitigation should adequately replace the wetland functions lost as a result of the action. The Service does not consider the preservation of wetlands, by itself, as adequate compensation for impacts to wood stork foraging habitat, because the habitat lost is not replaced. Accordingly, any wetland mitigation plan proposed should include a restoration, enhancement, or creation component. In some cases, the Service accepts wetlands compensation located outside the CFA of the affected wood stork nesting colony. Specifically, wetland credits purchased from a "Service Approved" mitigation bank located outside of the CFA would be acceptable to the Service, provided that the impacted wetlands occur within the permitted service area of the bank.

For projects that impact 5 or more acres of wood stork foraging habitat, the Service requires a functional assessment be conducted using our "Wood Stork Foraging Analysis Methodology" (Methodology) on the foraging habitat to be impacted and the foraging habitat provided as mitigation. The Methodology can be found at: <http://www.fws.gov/verobeach/ListedSpeciesBirds.html> .

**Florida Scrub-Jay**

The project occurs within the geographic range of the threatened Florida Scrub-Jay (*Aphelocoma coerulescens*). If suitable habitat occurs in or near the project footprint, we recommend that nest surveys based on Service protocol be conducted to determine the status of Florida scrub-jay in the project area. The Service's Florida scrub-jay survey guidance can be found at <https://www.fws.gov/verobeach/BirdsPDFs/FSJConservationGuidelinesALLINCLUSIVE1.pdf>

**Audubon's crested caracara**

The project occurs within the geographic range of the threatened Audubon's crested caracara (*Polyborus cheriway = Polyborus plancus audubonii*). If suitable habitat occurs in or near the project footprint, we recommend that nest surveys based on Service protocol be conducted to determine the status of caracara nesting in the project area. The Service's caracara nest survey guidance can be found at: <http://www.fws.gov/verobeach/ListedSpeciesBirds.html>

The Service believes that the following federally listed species have the potential to occur in or near the project site: eastern indigo snake (*Drymarchon couperi = Drymarchon corais couperi*), Florida scrub-jay, Audubon's crested caracara, West Indian manatee (*Trichechus manatus*), Everglade snail kite (*Rostrhamus socialbilis plumbeus*), wood stork, and Federally listed plants (<http://www.fws.gov/verobeach/ListedSpeciesPlants.html>).Accordingly, the Service recommends that the Florida Department of Transportation (FDOT) prepare a Biological Assessment for the project (as required by 50 CFR 402.12) during the FDOT's Project Development and Environment process.

**Fish and Wildlife Resources -**

Wetlands provide important habitat for fish and wildlife. Wetlands may occur within and near the project site. We recommend that these valuable resources be avoided to the greatest extent practicable. If impacts to these wetlands are unavoidable, we recommend the FDOT provide mitigation that fully compensates for the loss of important resources.

**Other Comments -**

To benefit pollinators, provide a more aesthetically appealing project site, and assist in native plant conservation, the Service recommends that the FDOT consider planting native flowering plants to stabilize the soil surrounding the proposed roadway, and include the planting of native shrubs and trees as part of the landscaping for the proposed roadway. We understand that these areas are usually planted with sod to stabilize the soil. The Service notes that small native flowering plants can be managed by mowing, a management technique that is typically employed for areas stabilized with sod only. Due to the uncertainty of native plant availability, soil stabilization of the entire project area with native plants may not be achievable at this time if

this is the case, we recommend that the FDOT develop a soil stabilization design that incorporates the planting, or creation through seeding, of small islands of small native flowering plant species at regular intervals along the project corridor, along with various native tree and shrub species.

**Additional Comments (optional):**

**CLC Commitments and Recommendations:**

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**Degree of Effect:** 0 None assigned 03/03/2017 by Steve Bohl, FL Department of Agriculture and Consumer Services

**Coordination Document:** No Involvement

**Direct Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

**Additional Comments (optional):**

**CLC Commitments and Recommendations:**

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**Coastal and Marine**

**Project Effects**

**Coordinator Summary Degree of Effect:** 3 Moderate assigned 05/12/2017 by Florida's Turnpike Enterprise

**Comments:**

This project is subject to Coastal Zone Consistency Determination, as required by Code of Federal Regulations (CFR) Title 15 930 (15 CFR 930). This project is located in coastal counties; therefore, Coastal Barrier Resources Act applies.

Florida's Turnpike Enterprise will make every attempt to avoid or minimize impacts to coastal and marine resources, identified within the project area. Further, Florida's Turnpike Enterprise will adhere to any additional criteria for in-water work so as not to harm the Florida manatee. Florida's Turnpike Enterprise will continue coordination with the South Florida Water Management District (SFWMD) and National Marine Fisheries Service (NMFS) during the PD&E phase.

**Degree of Effect:** 3 Moderate assigned 02/14/2017 by Brandon Howard, National Marine Fisheries Service

**Coordination Document:** Tech Memo Required

**Coordination Document Comments:**

EFH Assessment

**Direct Effects**

**Identified Resources and Level of Importance:**

Magnuson-Stevens Act and Fish and Wildlife Coordination Act: Where the following waterbodies and the Turnpike cross are essential fish habitat (EFH):

- The Loxahatchee River and its tributary crossings at the following 3 locations: 26.954450 , -80.165199 ; 26.964772 , -80.173156 ; and 26.972063 , -80.178423 .
- The South Fork of the St. Lucie River at 27.117673 , -80.274832 .
- The tributary to the South Fork of the St. Lucie River at 27.128295 , -80.282304 .
- The South Florida Water Management District canal at 27.262455 , -80.352264 .
- Ten Mile Creek at 27.402611 , -80.397842 .

These water bottoms and wetlands are EFH for white shrimp (*Litopenaeus setiferus*). These habitats vary in quality from low to very high. The South Atlantic Fishery Management Council (SAFMC) designates forested palustrine wetlands as EFH for juvenile white shrimp. If FDOT requires additional information on the EFH in the project area, the South Atlantic Fishery Management Council provides detailed information on EFH in amendments to

fishery management plans and in *Fishery Ecosystem Plan of the South Atlantic Region* (available at [www.safmc.net](http://www.safmc.net)).

**Comments on Effects to Resources:**

Impacts to these wetlands should be sequentially avoided, minimized, and compensated with in-kind mitigation. The NMFS is not aware of a mitigation bank servicing the area providing tidal freshwater credits. Freshwater wetlands not receiving tidal influence impacted by the project may be offset at mitigation banks after avoidance and minimization have taken place. If the project continues to PD&E without this sequential mitigation, NMFS would likely find it necessary to issue EFH conservation recommendations.

With construction of the new lanes, impervious surface area will be replaced or expanded. Surface and stormwater runoff into the surrounding waters may result. The discharge of hydrocarbons and other contaminants may degrade water quality. Subsequently, NOAA trust resources located in the receiving waters could be adversely affected. To the extent practicable, runoff from the new roads should be treated before being discharged into the canal.

**Additional Comments (optional):**

EFH Assessment

**CLC Commitments and Recommendations:**

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**Degree of Effect:** 2 *Minimal* assigned 03/02/2017 by Mindy Parrott, South Florida Water Management District

**Coordination Document:** To Be Determined: Further Coordination Required

**Direct Effects**

**Identified Resources and Level of Importance:**

Several rivers, creeks and canals connect to coastal waters. However, coastal resources (mangroves, seagrasses) are minimal in the project area due to low salinity.

**Comments on Effects to Resources:**

The water bodies crossed by the Turnpike may be accessible to manatees. Additional criteria will apply for any in-water work.

**Additional Comments (optional):**

**CLC Commitments and Recommendations:**

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**ETAT Reviews and Coordinator Summary: Physical**

**Noise**

**Project Effects**

**Coordinator Summary Degree of Effect:** 3 *Moderate* assigned 05/12/2017 by Florida's Turnpike Enterprise

**Comments:**

No ETAT reviews were submitted for this issue. A noise study will be conducted as part of the PD&E study, in accordance with Part 2, Chapter 17 of the PD&E Manual.

None found

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**Air Quality**

**Project Effects**

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 05/12/2017 by Florida's Turnpike Enterprise

**Comments:**

The United States Environmental Protection Agency (USEPA) noted that the project is currently in attainment with the National Ambient Air Quality Standards set forth by the USEPA. An air quality screening evaluation will be conducted in accordance with Part 2, Chapter 16 of the FDOT PD&E Manual.

**Degree of Effect:** 2 *Minimal* assigned 03/07/2017 by Kim Gates, US Environmental Protection Agency

**Coordination Document:** PD&E Support Document As Per PD&E Manual

**Coordination Document Comments:**

### Direct Effects

#### Identified Resources and Level of Importance:

Resource: Air quality that complies with standards established by the USEPA pursuant to the federal Clean Air Act.

Level of Importance: To protect public health and welfare nationwide, the USEPA has established National Ambient Air Quality Standards (NAAQS) for six "criteria pollutants": particulate matter, ozone, sulfur dioxide, nitrogen dioxide, carbon monoxide, and lead. States are required to adopt enforceable plans to achieve and maintain air quality that meets these standards.

#### Comments on Effects to Resources:

The project area is currently in attainment with the National Ambient Air Quality Standards. The USEPA does not anticipate emissions of criteria pollutants from the project being significant enough to impact the area's attainment status.

#### Additional Comments (optional):

Air Quality Technical Memorandum (PD&E Manual, Part 2, Chapter 16)

#### CLC Commitments and Recommendations:

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## Contamination

### Project Effects

**Coordinator Summary Degree of Effect:** 3 *Moderate* assigned 05/12/2017 by Florida's Turnpike Enterprise

#### Comments:

Data from the Environmental Screening Tool (EST) identified the following contaminated sites within the 500-foot buffer of the project:

- 10 Hazardous Waste Facilities
- 21 Petroleum Contamination Monitoring Sites
- 2 Solid Waste Facilities
- 22 Storage Tank Monitoring Sites

A Contamination Screening Evaluation Report (CSER) will be prepared as part of the PD&E Study in accordance with Part 2, Chapter 22 of the FDOT PD&E Manual.

**Degree of Effect:** 3 *Moderate* assigned 03/22/2017 by Kim Gates, US Environmental Protection Agency

**Coordination Document:** PD&E Support Document As Per PD&E Manual

#### Coordination Document Comments:

Contamination Screening Evaluation Report (PD&E Manual, Part 2, Chapter 22)

### Direct Effects

#### Identified Resources and Level of Importance:

"Contamination in soil, groundwater, surface water and structures may have the following impacts to an FDOT project: i) human exposure, ii) potential or actual human health concerns, iii) exacerbation of the contamination by FDOT construction activities, iv) design modifications or special construction provisions for work within contaminated areas, and v) requirements for the proper handling and disposal of contaminated material." FDOT defines 'Contamination' as: "The presence of any regulated material or chemical contained within the soil, surface water or groundwater on or adjacent to FDOT property, or proposed property, that may require assessment, remediation, or special handling, or that has a potential for liability. These materials would include, but not be limited to, those substances normally referred to as petroleum or petroleum products, solvents, organic and inorganic substances, metals, hazardous materials or substances." [Source: FDOT PD&E Manual, Part 2, Chapter 22 Contamination]

Major federal laws govern the remediation of contaminated sites, including the Resource Conservation and Recovery Act of 1976 (RCRA), as amended; and the Comprehensive Environmental Response, Compensation, and Liability Act of 1980 (CERCLA), as amended, which includes the Small Business Liability Relief and Brownfields Revitalization Act (Brownfields).

## Comments on Effects to Resources:

The EST reports RCRA-regulated facilities in several categories: Hazardous Waste (HW) Facilities; Large Quantity Generators (LQGs) of Hazardous Waste; Treaters, Storers, and Disposers (TSDs) of Hazardous Waste; and USEPA RCRA-Regulated Facilities. However, only the HW Facilities category includes USEPA RCRA ID numbers.

According to the EST, the following populations of facilities that generate, treat, store, or dispose of hazardous waste are located in the 500-foot buffer area of the project:

HW Facilities = 10

LQGs = 0

TSDs = 0

USEPA RCRA-Regulated Facilities = 15

Review of the lists revealed 8 duplications, 1 facility in the HW Facilities list that is not in the USEPA RCRA-Regulated Facilities category, 1 invalid USEPA ID number (Treasure Coast Marble & Granite, FLR 000 108 142), and 7 facilities identified as USEPA RCRA-Regulated Facilities that were not included in the HW Facilities category. As a result, it appears that the following 16 RCRA-regulated facilities (i.e., facilities with USEPA RCRA ID numbers) are located in the 500-foot buffer:

### Armellini Express Lines

3446 SW Armellini Avenue, Palm City 34990

USEPA ID #FLD 982 171 027

FDEP documents available online, [https://fldeploc.dep.state.fl.us/www\\_rcra/reports/handler\\_results\\_docs.asp?epaid=FLD982171027](https://fldeploc.dep.state.fl.us/www_rcra/reports/handler_results_docs.asp?epaid=FLD982171027)

### Dixie Clamp & Scaffold

3510 SW Martin Hwy, Palm City

USEPA ID #FLT 950 050 799

No information available in FDEP ([https://fldeploc.dep.state.fl.us/www\\_rcra/reports/handler\\_sel.asp](https://fldeploc.dep.state.fl.us/www_rcra/reports/handler_sel.asp)) or EPA

(<https://www3.epa.gov/enviro/facts/rcrainfo/search.html>) online databases

### Expert Shutter Service

1626 SW Biltmore Street, Port St. Lucie

USEPA ID #FLR 000 197 111

FDEP documents available online, [https://fldeploc.dep.state.fl.us/www\\_rcra/reports/handler\\_results\\_docs.asp?epaid=FLR000197111](https://fldeploc.dep.state.fl.us/www_rcra/reports/handler_results_docs.asp?epaid=FLR000197111)

### Florida RF Labs

8851 SW Old Kansas Ave, Stuart 34997

USEPA ID #FLR 000 019 380

FDEP documents available online, <http://webapps.dep.state.fl.us/DepNexus/public/electronic-documents/FLR000019380/gis-facility/search>

### Highway Enterprises Inc.

3584 SW Armellini Avenue, Palm City 34990

USEPA ID #FLR 000 078 402

FDEP documents available online, [https://fldeploc.dep.state.fl.us/www\\_rcra/reports/handler\\_results\\_docs.asp?epaid=FLR000078402](https://fldeploc.dep.state.fl.us/www_rcra/reports/handler_results_docs.asp?epaid=FLR000078402)

### Martin County Petroleum

3586 SW Martin Hwy, Palm City 34990

USEPA ID #FLT 010 069 151

No information available in FDEP or EPA online databases

### Martin County Utilities - Martin Downs WWTP

4450 SW Mallard Creek Trail, Palm City

USEPA ID #FLR 000 027 367

FDEP documents available online, [https://fldeploc.dep.state.fl.us/www\\_rcra/reports/handler\\_results\\_docs.asp?epaid=FLR000027367](https://fldeploc.dep.state.fl.us/www_rcra/reports/handler_results_docs.asp?epaid=FLR000027367)

### Mavidon Corp.

3953 SW Bruner Terrace, Palm City 34990

USEPA ID #FLD 984 219 949



FDEP documents available online, [https://fldeploc.dep.state.fl.us/www\\_rcra/reports/handler\\_results\\_docs.asp?epaid=FLD984219949](https://fldeploc.dep.state.fl.us/www_rcra/reports/handler_results_docs.asp?epaid=FLD984219949)

NAPA Auto & Truck Parts

1974 SW Biltmore Street, Suite 201, Port St. Lucie 34984

USEPA ID #FLD 984 231 720

FDEP documents available online, [https://fldeploc.dep.state.fl.us/www\\_rcra/reports/handler\\_results\\_docs.asp?epaid=FLD984231720](https://fldeploc.dep.state.fl.us/www_rcra/reports/handler_results_docs.asp?epaid=FLD984231720)

St. Lucie Cultural Marble

1266 SW Biltmore Street, Fort Pierce 34983

USEPA ID #FLR 000 025 833

FDEP documents available online, [https://fldeploc.dep.state.fl.us/www\\_rcra/reports/handler\\_results\\_docs.asp?epaid=FLR000025833](https://fldeploc.dep.state.fl.us/www_rcra/reports/handler_results_docs.asp?epaid=FLR000025833)

Scotts Tire & Auto

1889 SW Biltmore Street, Port St. Lucie

USEPA ID #FLT 010 068 039

No information available in FDEP or EPA online databases

SGS Industrial Services

669 SW Sea Holly Terrace, Port St. Lucie

USEPA ID #FLT MP9 203 479

No information available in FDEP or EPA online databases

Thornhill Maintenance Facility

450 SW Thornhill Drive, Port St. Lucie

USEPA ID #FLT 960 054 930

No information available in FDEP or EPA online databases

Treasure Coast Marble & Granite

1749 SW South Macedo Blvd, Port St. Lucie 34984

USEPA ID #FLR 000 052 407

FDEP documents available online, [https://fldeploc.dep.state.fl.us/www\\_rcra/reports/handler\\_results\\_docs.asp?epaid=FLR000052407](https://fldeploc.dep.state.fl.us/www_rcra/reports/handler_results_docs.asp?epaid=FLR000052407)

Tropical Farms WTP & WWTP

8595 SW Kansas Avenue, Stuart

USEPA ID #FLT 140 084 971

No information available in FDEP or EPA online databases

Virginias Manufacturing & Distributing

1974 SW Biltmore Street, Suite 209, Port St. Lucie 34984

USEPA ID #FLD 984 219 584

FDEP documents available online, [https://fldeploc.dep.state.fl.us/www\\_rcra/reports/handler\\_results\\_docs.asp?epaid=FLD984219584](https://fldeploc.dep.state.fl.us/www_rcra/reports/handler_results_docs.asp?epaid=FLD984219584)

Based on information in the EST, there are no federal Superfund sites or Brownfields sites where federal grant monies have been expended within one mile of the project corridor.

**Additional Comments (optional):**

Contamination Screening Evaluation Report (PD&E Manual, Part 2, Chapter 22)

**CLC Commitments and Recommendations:**

**Degree of Effect:** 2 *Minimal* assigned 03/02/2017 by Mindy Parrott, South Florida Water Management District

**Coordination Document:** To Be Determined: Further Coordination Required

**Coordination Document Comments:**

If dewatering is necessary, a water use permit may be required. A general permit is available in rule 40E-2.061(2), FAC. Projects that do not qualify for the general permit will require a water use permit from SFWMD.



## Direct Effects

### Identified Resources and Level of Importance:

Ground and surface waters in the vicinity of the proposed project.

### Comments on Effects to Resources:

Construction methodologies, such as dewatering, must be designed to minimize movement of contaminant plumes.

### Additional Comments (optional):

If dewatering is necessary, a water use permit may be required. A general permit is available in rule 40E-2.061(2), FAC. Projects that do not qualify for the general permit will require a water use permit from SFWMD.

### CLC Commitments and Recommendations:

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## Infrastructure

### Project Effects

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 05/12/2017 by Florida's Turnpike Enterprise

### Comments:

There were no ETAT comments submitted for this issue. The Florida's Turnpike Enterprise has noted several infrastructure sites that may be impacted by the proposed improvements, including;

- 10 Hazardous Waste Facilities,
- 1 Grade Level Railroad Crossing,
- 2 Solid Waste Facilities,
- 2 Wastewater Facilities,
- 4 Federal Aviation Administration (FAA) Flight Towers,
- 1 School,
- 4 Wireless Antennae Structures,
- 1 Major Dam,

A utility evaluation will be conducted as part of the PD&E phase.

None found

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## Navigation

### Project Effects

**Coordinator Summary Degree of Effect:** 3 *Moderate* assigned 05/12/2017 by Florida's Turnpike Enterprise

### Comments:

This project will require, one, or all of the following actions: construction, replacement, or modification of bridge(s) which cross navigable waters of the United States that are protected under Section 10 of the Rivers and Harbors Act.

During the PD&E phase, Florida's Turnpike Enterprise will coordinate with the United States Coast Guard (USCG) and United States Army Corp of Engineers (USACE) to minimize impacts on navigation and obtain the appropriate permits required.

**Degree of Effect:** 3 *Moderate* assigned 02/24/2017 by Randall D Overton, US Coast Guard

**Coordination Document:** Permit Required

## Direct Effects

### Identified Resources and Level of Importance:

Navigable waters of the United States.

### Comments on Effects to Resources:

I assigned a projects effects level of moderate because the project asdescribed will requireone or all of the following actions:construction, replacement, or modification of bridge(s) which cross navigable waters of the United States and therefore require a Coast Guard permit. One of the waterways

crossed is the St. Lucie River at Lat/Long (27.117681, -80.274901), the St. Lucie river at this location is part of the Okeechobee Waterway which is a Congressional authorized Federal Navigation Project channel.

**Additional Comments (optional):**

**CLC Commitments and Recommendations:**

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**Degree of Effect:** 2 *Minimal* assigned 02/22/2017 by Tarrie L Ostrofsky, US Army Corps of Engineers

**Coordination Document:** Permit Required

**Coordination Document Comments:**

A Section 10 and Section 404 permit may be required if fill is proposed within the waterway for purposes other than bridges. A Section 404 permit would be necessary for fill associated with USCG bridges, as well. A Department of the Army Permit may be in the form of a Nationwide Permit, Regional General Permit, or a Standard Permit.

**Direct Effects**

**Identified Resources and Level of Importance:**

According to the information provided, analysis of navigation datashows that there is 1 potential navigable waterway, 1 public boat ramp, and 1 major dam within the 500-foot buffer. The St. Lucie River is a navigable waterway and intersects Florida's Turnpike (SR 91) and may be affected by proposed roadway improvements. If there are fill impacts proposed for construction of a USCG bridge, the Corps would regulate the fill under Section 404. Additionally, if fill is proposed in Section 10 waters which is not associated with a bridge, the Corps would regulate the fill under Section 404 and Section 10. Given the information provided, there is a chance that fill may be proposed for a non bridge activity in tidal waters. Therefore, the initial determination of direct effects on navigation is minimal until further information is provided.

**Comments on Effects to Resources:**

Direct effects may be reduced widths of tidal waters if fill is proposed at the shoreline locations. Direct effects may also include increased depths if dredging is proposed.

**Additional Comments (optional):**

A Section 10 and Section 404 permit may be required if fill is proposed within the waterway for purposes other than bridges. A Section 404 permit would be necessary for fill associated with USCG bridges, as well. A Department of the Army Permit may be in the form of a Nationwide Permit, Regional General Permit, or a Standard Permit.

**CLC Commitments and Recommendations:**

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**ETAT Reviews and Coordinator Summary: Special Designations**

**Special Designations**

**Project Effects**

**Coordinator Summary Degree of Effect:** 4 *Substantial* assigned 05/18/2017 by Florida's Turnpike Enterprise

**Comments:**

Within the project area there are two Outstanding Florida Waters: the Loxahatchee River Aquatic Preserve and Jonathan Dickinson State Park, one Aquatic Preserve: The Loxahatchee River-Lake Worth Creek Aquatic Preserve, and one Wild and Scenic River: The Loxahatchee River. There are no Scenic Highways in the project area. Florida's Turnpike Enterprise will continue to coordinate with the United States Environmental Protection Agency (USEPA), South Florida Water Management District (SFWMD), the National Parks Service (NPS) and the United States Fish and Wildlife Service (USFWS) to ensure that impacts to these resources are minimized and adequately mitigated.

**Degree of Effect:** 4 *Substantial* assigned 03/22/2017 by Kim Gates, US Environmental Protection Agency

**Coordination Document:** PD&E Support Document As Per PD&E Manual

**Coordination Document Comments:**

Water Quality Impact Evaluation (PD&E Manual, Part 2, Chapter 20)

## Direct Effects

### Identified Resources and Level of Importance:

The PD&E Manual (Part I, Chapter 3) defines the Special Designations category as comprised of Outstanding Florida Waters (Rule 62-302.700, F.A.C.), Aquatic Preserves (Rule 62-302.700(2)(f), F.A.C.), Scenic Highways (PD&E Manual, Part 2, Chapter 29), and Wild & Scenic Rivers (Rule 62-302.700(2)(d), F.A.C.).

The project corridor crosses the Northwest Fork of the Loxahatchee River, portions of which are designated as a National Wild & Scenic River (<https://www.rivers.gov/rivers/loxahatchee.php>), a Florida Scenic & Wild River, a State Aquatic Preserve (<http://www.dep.state.fl.us/coastal/sites/loxahatchee/>), and an Outstanding Florida Water (i.e., waters in Jonathan Dickinson State Park, <https://www.floridastateparks.org/park/Jonathan-Dickinson>). The National Wild & Scenic Rivers System was created by Congress in 1968 (Public Law 90-542, 16 U.S.C. 1271 et seq.) to preserve certain rivers with outstanding natural, cultural, and recreational values for the enjoyment of present and future generations (<https://www.rivers.gov/wsr-act.php>). Rivers may be designated by Congress or, if certain requirements are met, the Secretary of the Interior. The Northwest Fork of the Loxahatchee River was designated by the Secretary in May 1985 as the first National Wild & Scenic River in Florida.

The Northwest Fork of the Loxahatchee River is also the focus of the Loxahatchee River Watershed Restoration Project (<http://www.saj.usace.army.mil/Missions/Environmental/Ecosystem-Restoration/Loxahatchee-River-Watershed-Restoration-Project/>). The objectives of the project, which is a component of the Comprehensive Everglades Restoration Plan (CERP), include improving water distribution and timing to restore the natural system's ecological function and re-establishing connections among natural areas that have become spatially and/or hydrologically fragmented ([http://www.saj.usace.army.mil/Portals/44/docs/FactSheets/Loxahatchee\\_FS\\_January2015\\_revised.pdf](http://www.saj.usace.army.mil/Portals/44/docs/FactSheets/Loxahatchee_FS_January2015_revised.pdf)). CERP is the largest ecosystem restoration program in the history of Florida.

Additionally, in the *Agency Operating and Funding Agreement for Continuing Participation in the Efficient Transportation Decision Making and Transportation Project Development Processes between United States Environmental Protection Agency and Federal Highway Administration and Florida Department of Transportation*, January 23, 2015, FDOT requested the USEPA's focus on Sole Source Aquifer considerations for the Special Designations issue. The Sole Source Aquifer Protection Program is authorized by Section 1424(e) of the Safe Drinking Water Act of 1974.

The Biscayne aquifer, which underlies Miami-Dade, Broward, and part of Palm Beach counties, supplies virtually all of the potable water needs for almost 6 million residents in southeastern Florida, including the Florida Keys. Consistent with the Safe Drinking Water Act, which defines a Sole Source Aquifer as an underground water source that supplies at least 50% of the drinking water to the overlying area (<http://water.epa.gov/infrastructure/drinkingwater/sourcewater/protection/solesourceaquifer.cfm>), the USEPA designated the Biscayne aquifer as a Sole Source Aquifer (44 Federal Register 58797, October 11, 1979). The portion of the project corridor located in Palm Beach County is in the recharge zone for the Biscayne aquifer, which was included in the Sole Source Aquifer designation (<https://archive.epa.gov/pesticides/region4/water/groundwater/web/html/r4ssa.html>).

Once an area is designated as a Sole Source Aquifer (SSA), no commitments of federal financial assistance may be made to projects that the USEPA determines could contaminate the aquifer and create a significant hazard to public health. To ensure compliance with SDWA requirements, FDOT, FHWA, and the USEPA executed a Sole Source Aquifer Memorandum of Understanding (MOU) that identifies the types of proposed projects to be forwarded to the USEPA for evaluation and comment. The MOU, executed in January 1999, also memorializes FDOT's commitment to designing federal-aid projects in SSA-designated areas in a manner that will prevent the introduction of contaminants in quantities or concentrations that could create a significant hazard to public health, or which may require a public water system to install additional treatment to prevent such adverse effect ([http://www.fdot.gov/environment/pubs/SSA MOU.pdf](http://www.fdot.gov/environment/pubs/SSA%20MOU.pdf)). Pursuant to the MOU (see Attachment C, [http://www.fdot.gov/environment/pubs/SSA MOU Attachments.pdf](http://www.fdot.gov/environment/pubs/SSA%20MOU%20Attachments.pdf)), the following types of projects will be referred to the USEPA Region 4's Ground Water/Drinking Water Branch for review and comment prior to the commitment of federal funding:

1. All projects for which an EIS or EA/FONSI will be prepared.
2. Projects which have the potential to contaminate the aquifer, such as a new or stage construction involving extensive grading, widening or addition of lanes to an existing highway, replacement or rehabilitation/reconstruction of bridges over the Volusia-Floridan or Biscayne Aquifers or their designated recharge zones, and public rest areas which include domestic wastewater facilities which do not discharge to a central wastewater collection system.
3. All other projects which FHWA determines may be reasonably expected to contaminate the designated SSAs.

Federal responsibilities pursuant to the Safe Drinking Water Act also include wellhead protection (<http://www.epa.gov/sourcewaterprotection/source-water-protectionbasics>). The project is within Wellfield Protection Areas in Palm Beach County ([http://discover.pbcgov.org/pzb/planning/PDF/Projects/LoxGroves/LU\\_4\\_1.pdf](http://discover.pbcgov.org/pzb/planning/PDF/Projects/LoxGroves/LU_4_1.pdf)) and St. Lucie County ([http://www.cityofpsl.com/planning-zoning/pdf/comprehensive\\_plan/comprehensive\\_plan\\_maps.pdf](http://www.cityofpsl.com/planning-zoning/pdf/comprehensive_plan/comprehensive_plan_maps.pdf)). The USEPA was not able to determine definitively if wellfields in Martin County could be impacted by the project. We recommend confirming the location of the Martin Downs Wellfield shown in the USGS/SFWM Water Resources Investigations Report 99-4214 ([https://fl.water.usgs.gov/PDF\\_files/wri99\\_4214\\_hittle.pdf](https://fl.water.usgs.gov/PDF_files/wri99_4214_hittle.pdf)) and any other wellfields that may be located in the project

vicinity (e.g., wells supplying the Tropical Farms Water Treatment Plant).

**Comments on Effects to Resources:**

Loxahatchee River

The FDEP and SFWMD are the lead agencies responsible for implementing the Loxahatchee National Wild & Scenic River Management Plan (<https://www.rivers.gov/documents/plans/loxahatchee-plan.pdf>). The SFWMD has also been involved with the Loxahatchee River Watershed Restoration Project ([http://141.232.10.32/pm/projects/proj\\_17\\_lox\\_river.aspx](http://141.232.10.32/pm/projects/proj_17_lox_river.aspx)). Please coordinate with these agencies regarding the potential impacts on the river of project construction and stormwater runoff (<https://www.dep.state.fl.us/water/wqssp/docs/ofw-factsheet.pdf>).

Biscayne Sole Source Aquifer

It is not clear if federal funding will be used for the project. Even though the Preliminary Environmental Discussion Comments Report (PED) implied, in the Section 4(f) Potential comments, that state funds will be used, no funding information was provided in the Purpose and Need.

If federal funds are used, the project needs to be reviewed by the USEPA Region 4's Ground Water/Drinking Water Branch prior to commitment of the funds. We recommend including the following information in the review request:

1. Location of project and name of Sole Source Aquifer.
2. Project description and federal funding source.
3. Is there any increase in impervious surface? If so, what is the area?
4. Describe how stormwater is currently treated along the project corridor.
5. How will stormwater be treated during construction and throughout the life of the project?
6. Are there any underground storage tanks present or to be installed? Include details of such tanks.
7. Will there be any liquid or solid waste generated? If so, how will it be disposed of?
8. What is the depth of excavation?
9. Are there any wells in the area that may provide contaminants with direct access to the aquifer and how close are they to the project?
10. Are there any hazardous waste sites in the project area? In particular, are there any sites with groundwater plumes and monitoring wells that may be disturbed? Include details.
11. Are there any deep pilings that may provide access to the aquifer?
12. Are Best Management Practices planned to address any possible risks or concerns? Include details.
13. Does the project include improvements that may be beneficial to the aquifer?
14. Any other information that could be helpful in determining if this project could impact the aquifer.

Wellfield Protection Areas

The USEPA does not understand why wellfield protection areas in the project corridor were not addressed in the PED. Consistent with the PD&E Manual (Part 2, Chapter 20), the PED should have discussed the potential project involvement with groundwater resources, which include wellfield protection areas. Please explain this oversight in the ETDM Summary Report and confirm that the various municipalities with wellfield protection areas in the project corridor will be contacted to determine if regulations restricting development and/or regulating land uses may apply to the project.

**Additional Comments (optional):**

Water Quality Impact Evaluation (PD&E Manual, Part 2, Chapter 20)

**CLC Commitments and Recommendations:**

---

**Degree of Effect:** 3 *Moderate* assigned 03/02/2017 by Mindy Parrott, South Florida Water Management District

**Coordination Document:** To Be Determined: Further Coordination Required

**Coordination Document Comments:**

For these issues additional coordination is required, especially if new on ramps/ exits or expanded intersections are proposed, or if the project components will occur outside of the current FDOT right of way. Coordination with CERP project managers, SFWMD land management staff, SFWMD Right of Way permitting staff and FDEP state park staff is highly recommended.

Also, for the area of the project within the Loxahatchee River, review by the Loxahatchee River Management Coordinating Council is needed.

## Direct Effects

### Identified Resources and Level of Importance:

The project is within or adjacent to substantial public lands that serve as important habitat and are also used for recreation:

- Jonathan Dickinson State Park,
- the Wild and Scenic section of the Loxahatchee River
- the Loxahatchee River Aquatic Preserve.
- SFWMD Lands: Cypress Creek and Loxahatchee River Management Area and the Ten Mile Creek parcels.

The Ten Mile Creek area and the Loxahatchee River area are part of a CERP project.

The C-23, C-24 and C-25 canals are SFWMD Right of Way.

There are several existing conservation easements dedicated to the SFWMD adjacent to the Turnpike Right of Way.

The Loxahatchee River, Cypress Creek, South Fork of the St. Lucie River, Ten Mile Creek, possibly Bessey Creek are likely state owned lands that require public easements from the Board of Trustees, or modification of existing easements.

### Comments on Effects to Resources:

To protect the Loxahatchee River, there are additional criteria that apply for aquatic preserves. Use of submerged lands also has additional criteria, as do use of SFWMD Right of Way.

The Loxahatchee River and Ten Mile Creek area are both CERP -related projects. While designing the project, efforts should be taken to avoid construction outside of the existing right of ways in these areas.

### Additional Comments (optional):

For these issues additional coordination is required, especially if new on ramps/ exits or expanded intersections are proposed, or if the project components will occur outside of the current FDOT right of way. Coordination with CERP project managers, SFWMD land management staff, SFWMD Right of Way permitting staff and FDEP state park staff is highly recommended.

Also, for the area of the project within the Loxahatchee River, review by the Loxahatchee River Management Coordinating Council is needed.

### CLC Commitments and Recommendations:

---

**Degree of Effect:** 4 *Substantial* assigned 01/25/2017 by John Wrublik, US Fish and Wildlife Service

**Coordination Document:** To Be Determined: Further Coordination Required

## Direct Effects

### Identified Resources and Level of Importance:

Public Conservation Lands

### Comments on Effects to Resources:

The project corridor is located within, or immediately adjacent to, valuable public conservation lands that provide important habitat for fish and wildlife (Palm Beach County's Cypress Creek Natural Area, and South Florida Water Management District's Cypress Creek and Loxahatchee River Management Area). The project corridor is also located within lands that may be targeted for acquisition for conservation purposes (the Pal Mar Florida Forever Board of Trustees Project and the Atlantic Ridge Ecosystem). Impacts to these areas should be avoided to the greatest extent practicable through widening within the existing center median. If impacts to currently protected public conservation lands are unavoidable, the Service requests that additional public lands be acquired by the FDOT to offset the conservation lands lost due to the project.

### Additional Comments (optional):

### CLC Commitments and Recommendations:

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## ETAT Reviews and Coordinator Summary: Emergency Response

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## Eliminated Alternatives

There are no eliminated alternatives for this project.

## Project Scope

### General Project Recommendations

There are no general project recommendations identified for this project in the EST.

### Anticipated Permits

Permit	Type	Conditions	Review Org	Review Date
NPDES General Permit	FDEP		Florida's Turnpike Enterprise	11/29/16
Bridge Permit	USCG		Florida's Turnpike Enterprise	09/02/16
SFWMD Environmental Resource Permit	Water		Florida's Turnpike Enterprise	11/29/16
Section 404 Individual Permit	USACE		Florida's Turnpike Enterprise	11/29/16

### Anticipated Technical Studies

Technical Study Name	Type	Conditions	Review Org	Review Date
Location Hydraulics Report	ENGINEERING		Florida's Turnpike Enterprise	11/29/2016
Drainage/Pond Siting Report	ENGINEERING		Florida's Turnpike Enterprise	11/29/2016
Geotechnical Report	ENGINEERING		Florida's Turnpike Enterprise	11/29/2016
Bridge Hydraulic Report	ENGINEERING		Florida's Turnpike Enterprise	11/29/2016
Noise Study Report	ENVIRONMENTAL		Florida's Turnpike Enterprise	11/29/2016
Air Quality Report	ENVIRONMENTAL		Florida's Turnpike Enterprise	11/29/2016
Contamination Screening Evaluation Report	ENVIRONMENTAL		Florida's Turnpike Enterprise	11/29/2016
Conceptual Stage Relocation Plan	ENVIRONMENTAL		Florida's Turnpike Enterprise	11/29/2016
Sociocultural Effects Evaluation	Other		Florida's Turnpike Enterprise	11/29/2016
Preliminary Engineering Report	ENGINEERING		Florida's Turnpike Enterprise	11/29/2016
Endangered Species Biological Assessment Technical Memorandum	ENVIRONMENTAL		Florida's Turnpike Enterprise	11/29/2016
Water Quality Impact Evaluation (WQIE)	ENVIRONMENTAL		Florida's Turnpike Enterprise	11/29/2016
Cultural Resource Assessment Survey	ENVIRONMENTAL		Florida's Turnpike Enterprise	11/29/2016
Utility Assessment Technical Memorandum	ENGINEERING		Florida's Turnpike Enterprise	11/29/2016
Bridge Analysis Report	ENGINEERING		Florida's Turnpike Enterprise	11/29/2016
Natural Resources Evaluation (NRE)	ENVIRONMENTAL		Florida's Turnpike Enterprise	11/29/2016

### Dispute Resolution Activity Log

There are no dispute actions identified for this project in the EST.

# Hardcopy Maps: Alternative #1

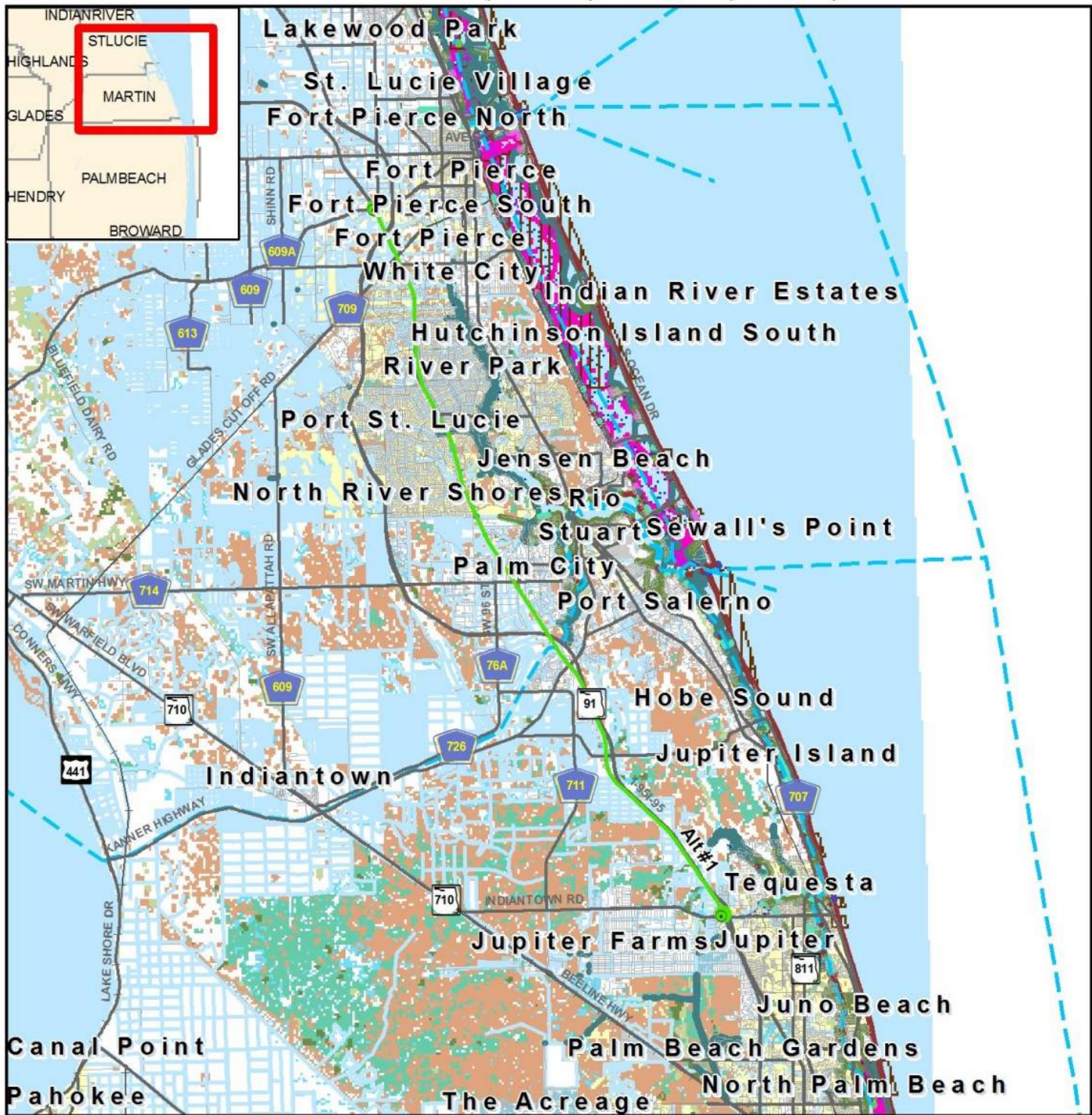




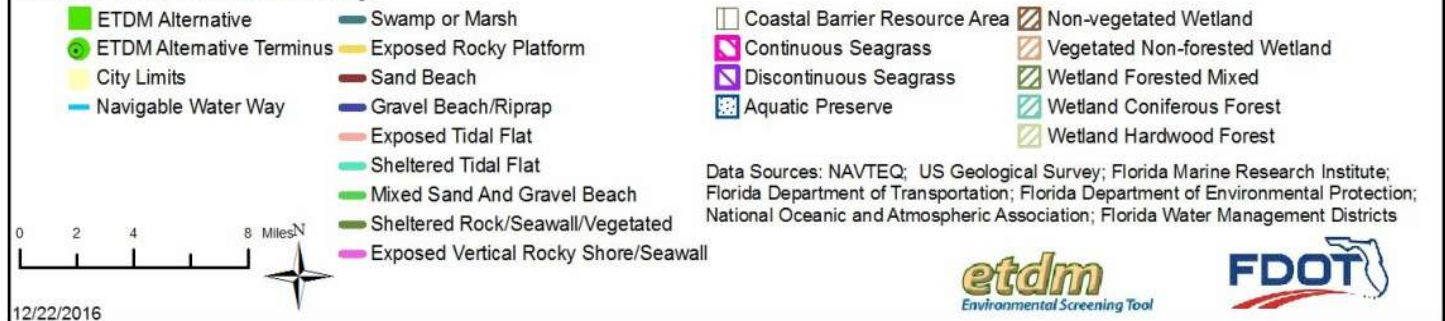


# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

Indiantown Road (MP 116) to SR 70 (MP 152)



## Coastal and Marine Map



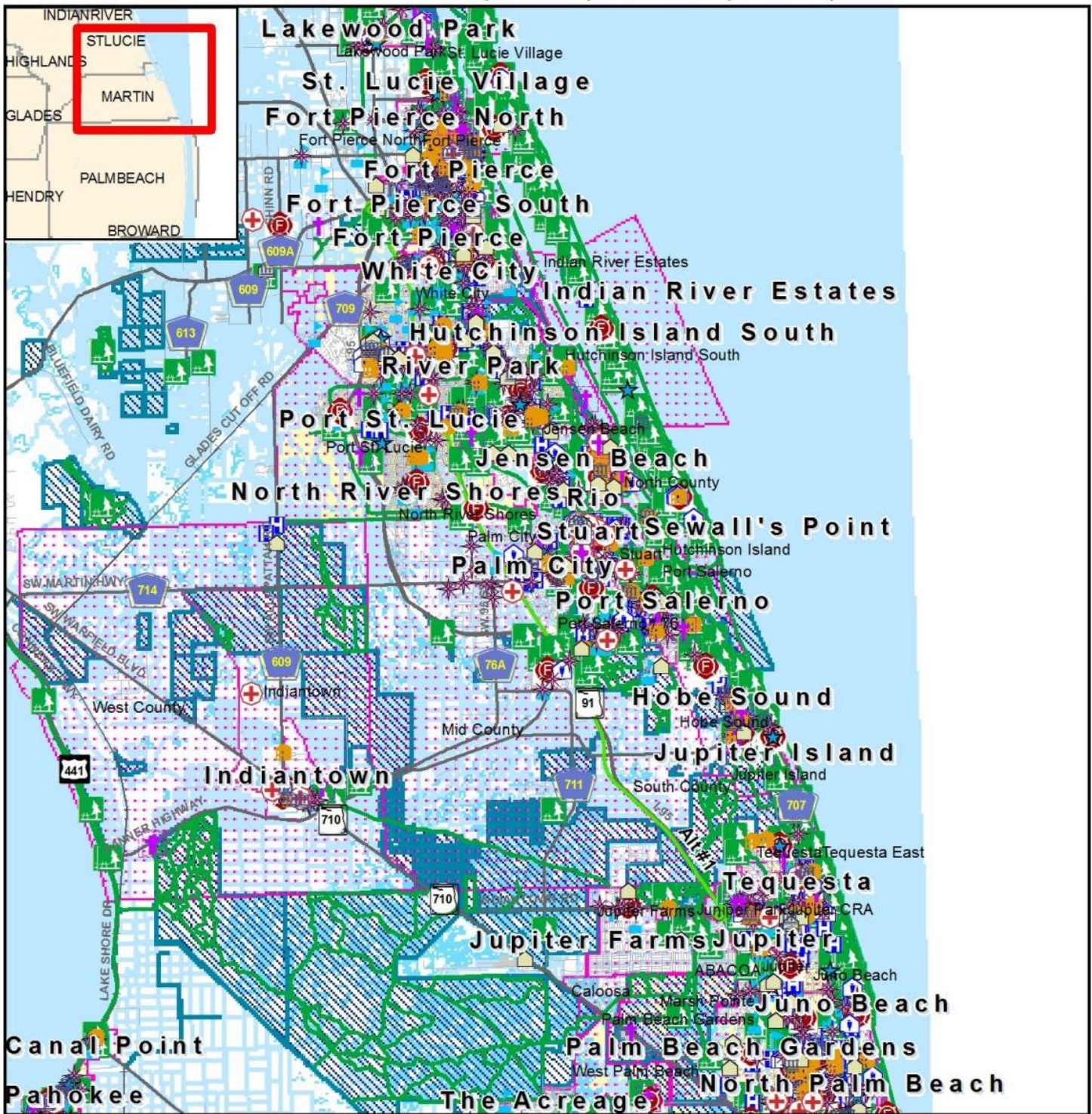
12/22/2016

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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

## Indiantown Road (MP 116) to SR 70 (MP 152)



### Community Services Map

ETDM Alternative	Government	Law Enforcement	Health Care	Recreational Trail
ETDM Alternative Terminus	Civic Center	Place of Worship	School	Community Boundary
Major Road	Cemetery	Cultural Center	Park	Conservation or Recreation Area
Local Road or Trail	Social Service	Fire Station		
City Limits	Community Center			

Data Sources:  
 US Geological Survey; FL Department of Transportation; NAVTEQ; FL Property Appraisers; FL Natural Areas Inventory

0 1.5 3 6 Miles

N

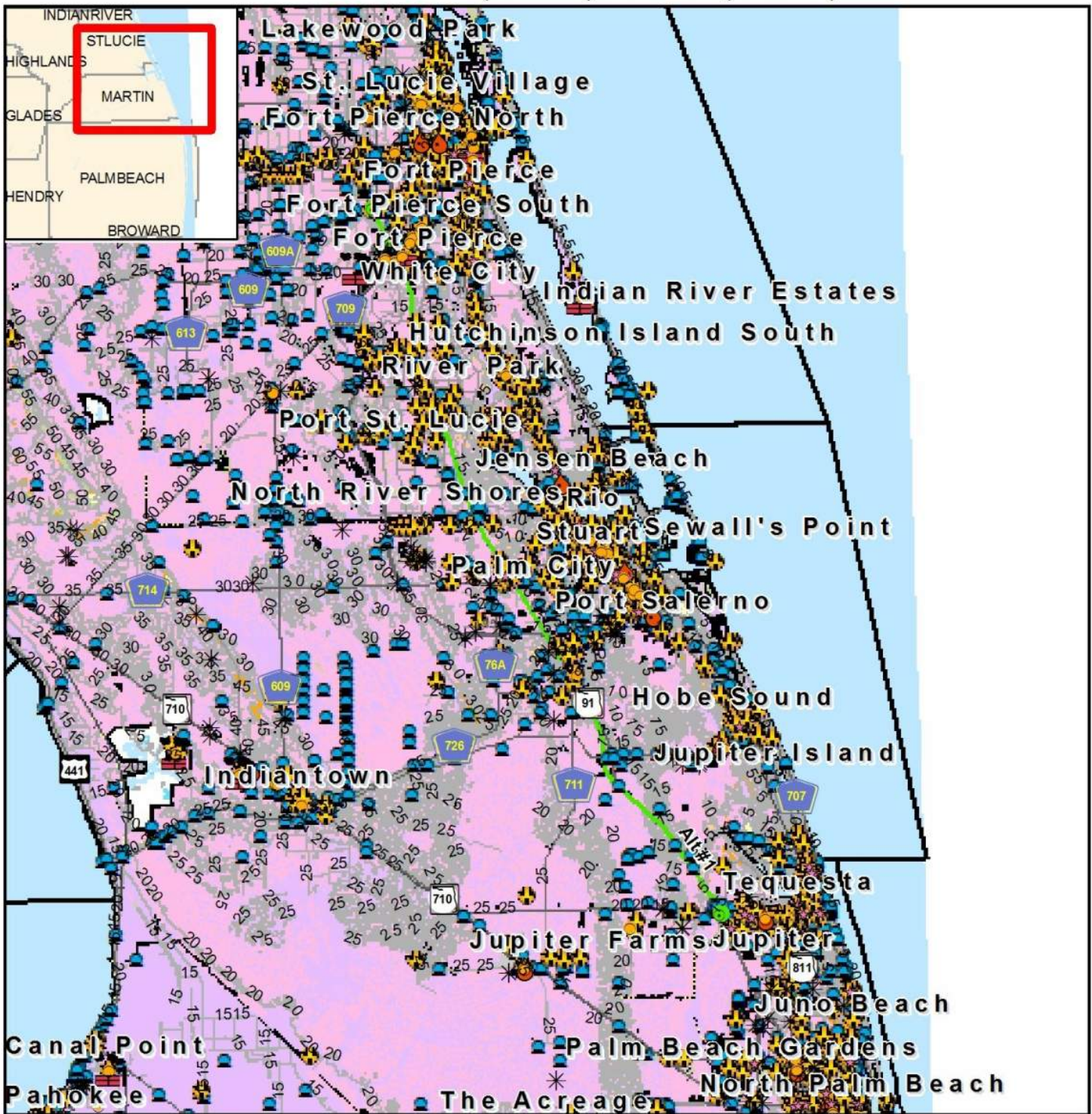
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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

## Indiantown Road (MP 116) to SR 70 (MP 152)



### Contamination Map

ETDM Alternative	Solid Waste Facility	FDEP Tanks	<b>Soil Drainage</b>	Somewhat Poorly Drained
ETDM Alternative Terminus	Hazardous Material Site	5 FT Contour	Excessively Drained	Poorly Drained
Major Road	Power Plant	NPL Remediation Site	Well Drained	Very Poorly Drained
Local Road or Trail	Superfund Site	Brownfield Area	Somewhat Excessively Drained	Unclassified
Toxic Release Inventory	Nuclear Site		Moderately Well Drained	
Dry Cleaning Facility				

Data Sources: NAVTEQ, US Geological Survey, FL Department of Transportation, FL Department of Environmental Protection, FL Water Management Districts, US Environmental Protection Agency, Natural Resource Conservation Service

0 2 4 8 Miles

12/22/2016

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# Cultural Resources Data Map

- ETDM Alternative
- Major Road
- Local Road or Trail
- ★ Historic Structure
- Historic Bridge
- State Historic Highway
- ⊠ Historic Cemetery
- ⊠ Historic Resource Group
- Cultural Resource Field Survey Area
- ETDM Alternative

## Year Built

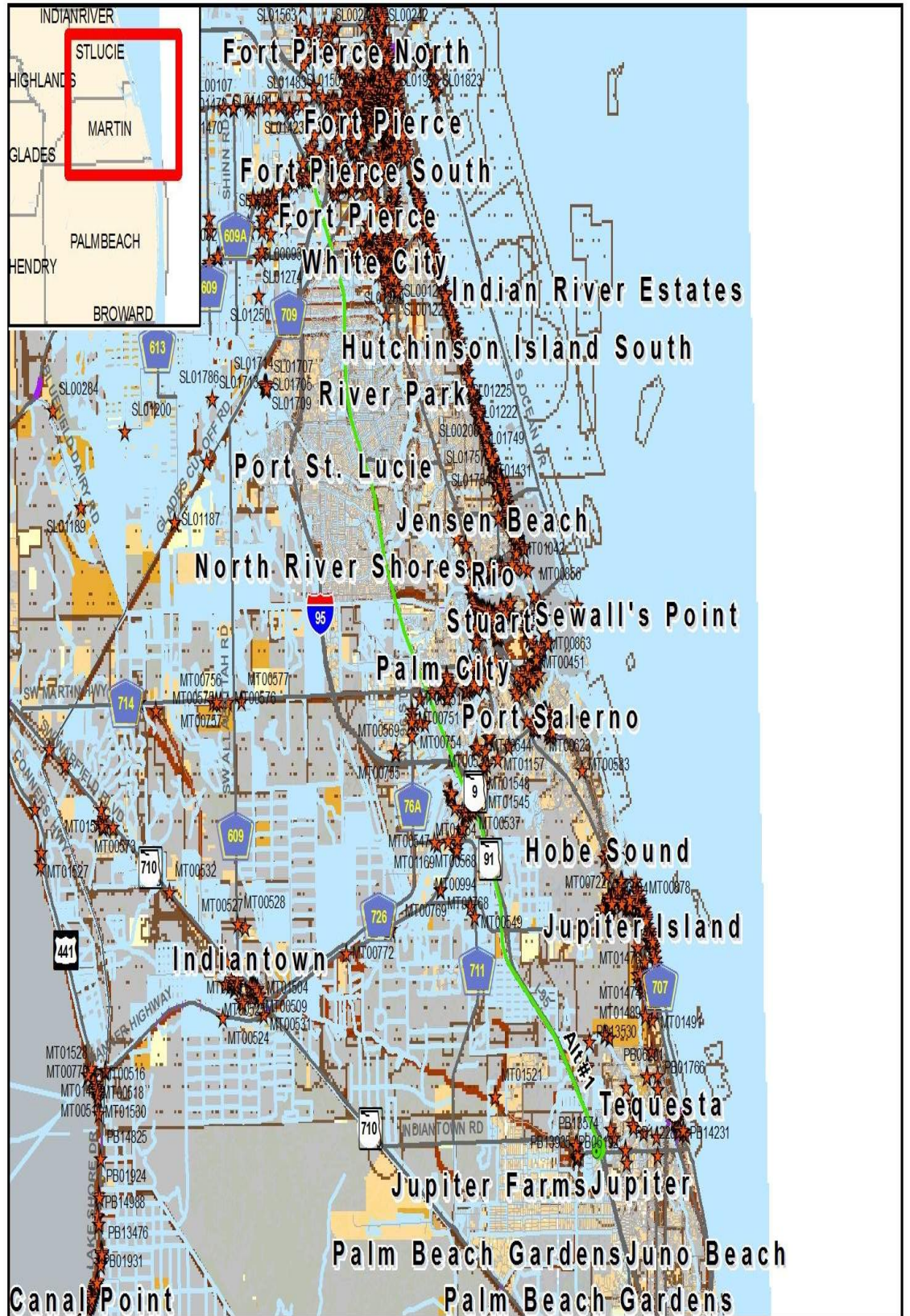
- Pre 1970
- Post 1980
- 1970 - 1979
- Parcels w/ no values



0 1.25 2.5 5 Miles



Data Sources:  
 NAVTEQ  
 US Geological Survey  
 Florida Department of Transportation  
 Florida Department of State,  
 Bureau of Archaeological Research

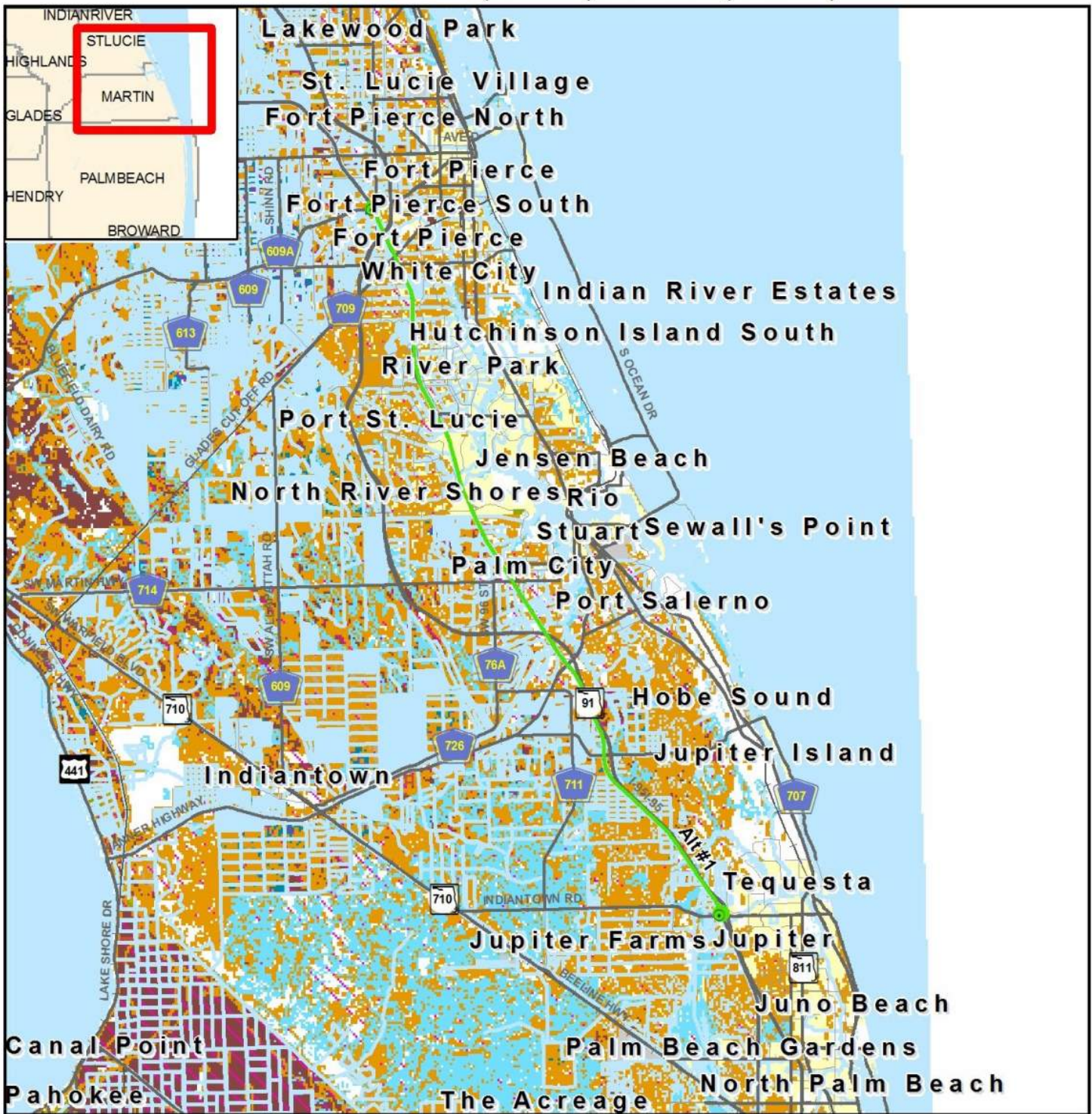


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 Note: Historic properties depicted on this map represent resources listed in the Florida Master Site File excluding archeological site locations, which, pursuant to Chapter 267.135, Florida Statutes, may be exempt from public record (Chapter 119.07, Florida Statutes). Absence of features on the map does not necessarily indicate an absence of resources in the project vicinity.



# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

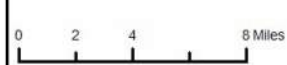
Indiantown Road (MP 116) to SR 70 (MP 152)



## Farmlands Map

- ETDM Alternative
- ETDM Alternative Terminus
- Major Road
- Local Road or Trail
- City Limits
- Cropland/Pastureland
- Nurseries/Vineyards
- Specialty Farms
- Tree Crops
- Rural Open Lands
- Prime Farmland Soils

Data Sources:  
 NAVTEQ  
 Florida Water Management Districts  
 US Geological Survey  
 Natural Resources Conservation Services



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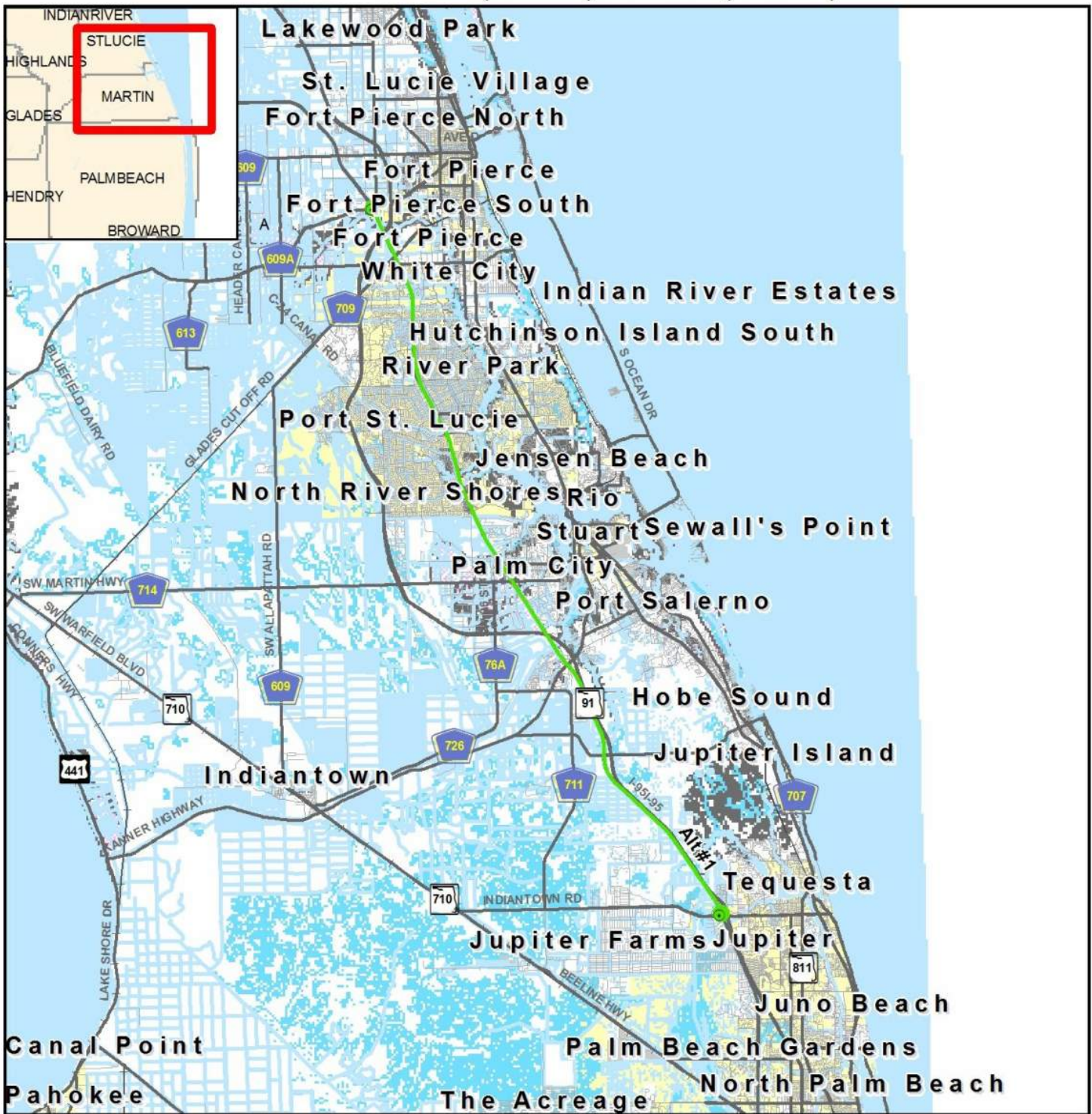
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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

Indiantown Road (MP 116) to SR 70 (MP 152)



## Floodplains Map

- ETDM Alternative
- ETDM Alternative Terminus
- Major Road
- Local Road or Trail
- City Limits
- Special Flood Hazard Area

Data Sources:  
 NAVTEQ  
 US Geological Survey  
 Federal Emergency Management Agency



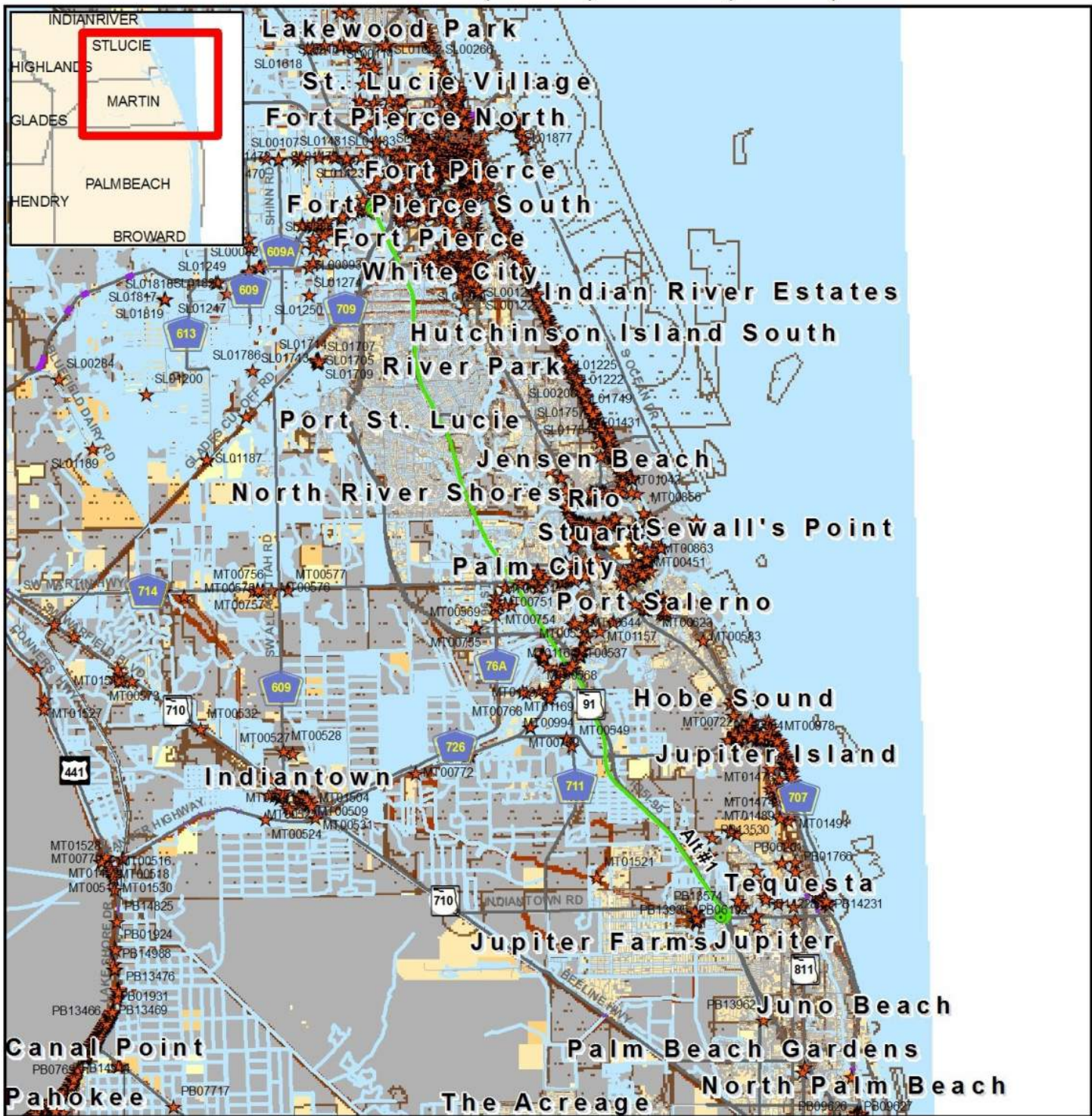
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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

## Indiantown Road (MP 116) to SR 70 (MP 152)

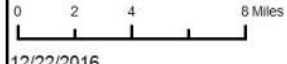


### Historic Resource Map

- ETDM Alternative
- ETDM Alternative Terminus
- Major Road
- Local Road or Trail
- Year Built
  - Pre 1970
  - Post 1980
  - 1970 - 1979
  - Parcels w/ no values
- ★ Historic Structure
- Historic Bridge
- State Historic Highway
- Historic Cemetery
- Historic Resource Group
- Cultural Resource Field Survey Area

Data Sources:  
 NAVTEQ  
 US Geological Survey  
 Florida Department of Transportation  
 Florida Department of State,  
 Bureau of Archaeological Research

Note: Historic properties depicted on this map represent resources listed in the Florida Master Site File excluding archeological site locations, which, pursuant to Chapter 267.135, Florida Statutes, may be exempt from public record (Chapter 119.07, Florida Statutes). Absence of features on the map does not necessarily indicate an absence of resources in the project vicinity.



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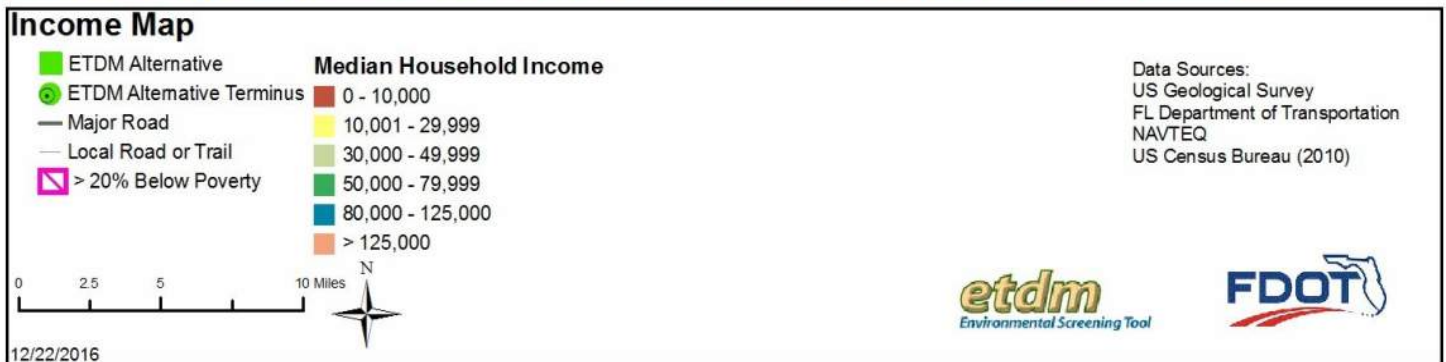
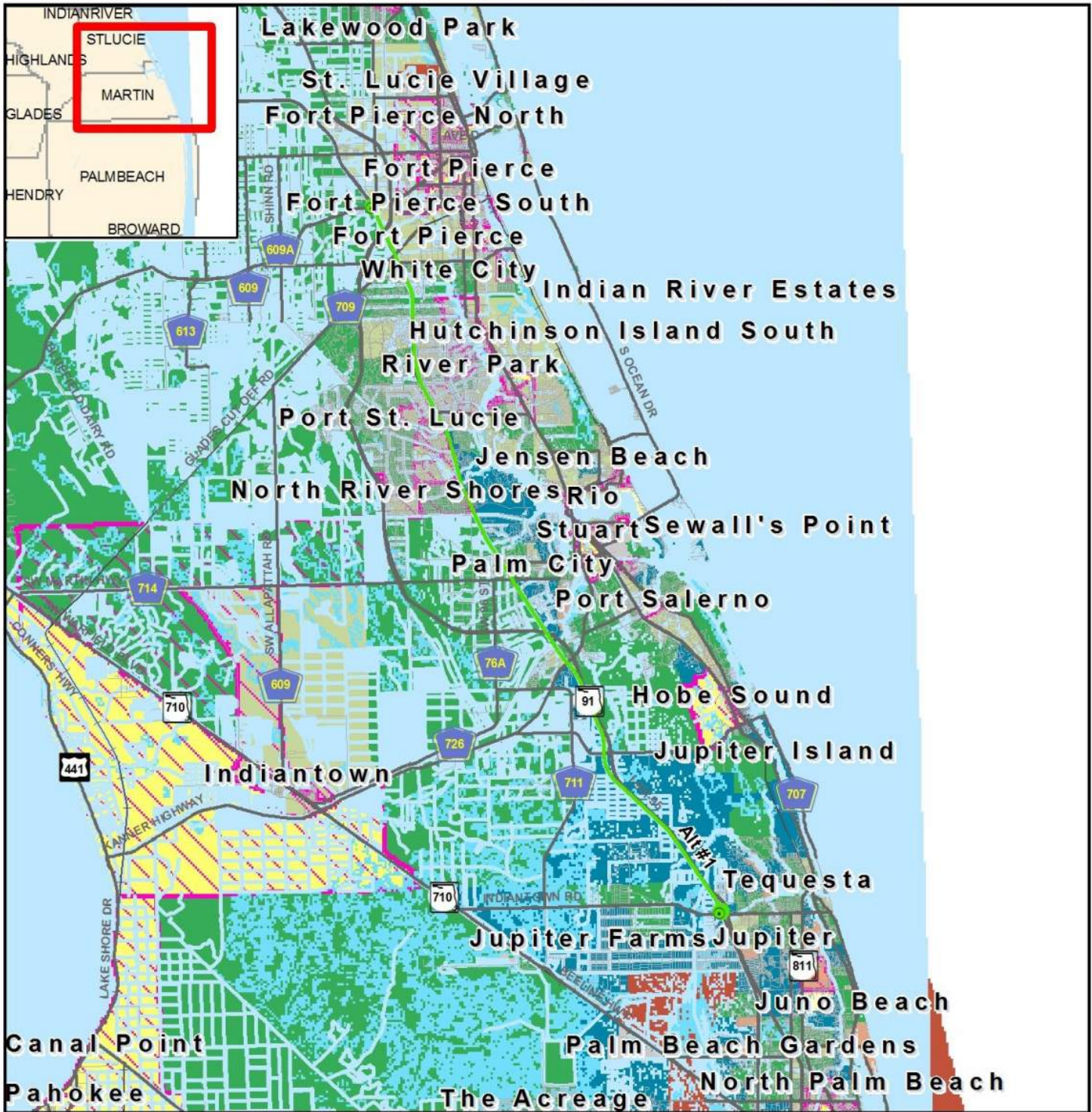






# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

## Indiantown Road (MP 116) to SR 70 (MP 152)

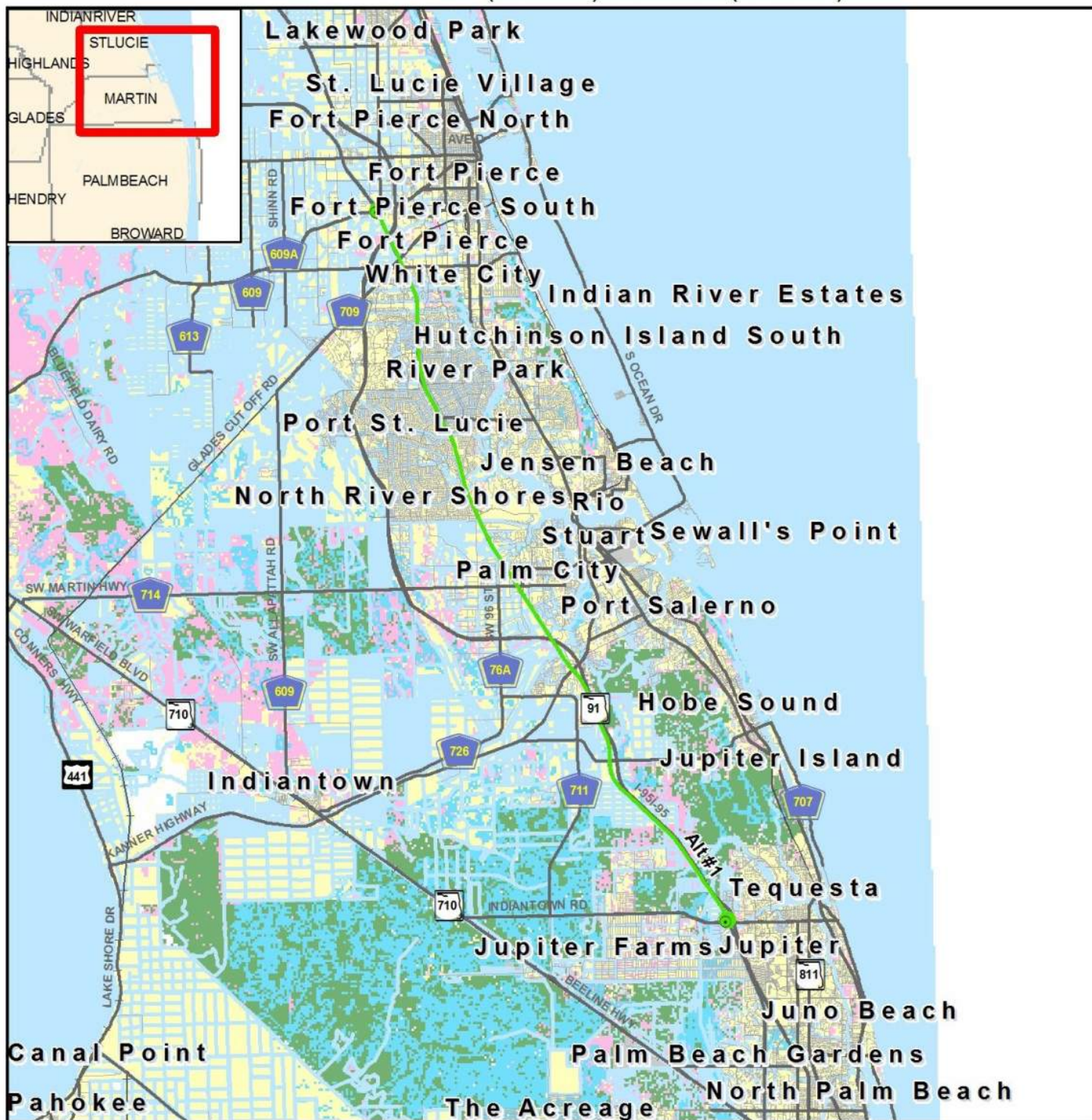


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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

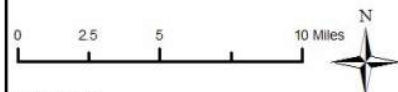
## Indiantown Road (MP 116) to SR 70 (MP 152)



### Integrated Wildlife Model Map

- ETDM Alternative
- ETDM Alternative Terminus
- Major Road
- Local Road or Trail
- Low Habitat Quality
- Medium Habitat Quality
- High Habitat Quality

Data Sources:  
NAVTEQ  
US Geological Survey  
Florida Department of Transportation  
Florida Fish & Wildlife Conservation Commission



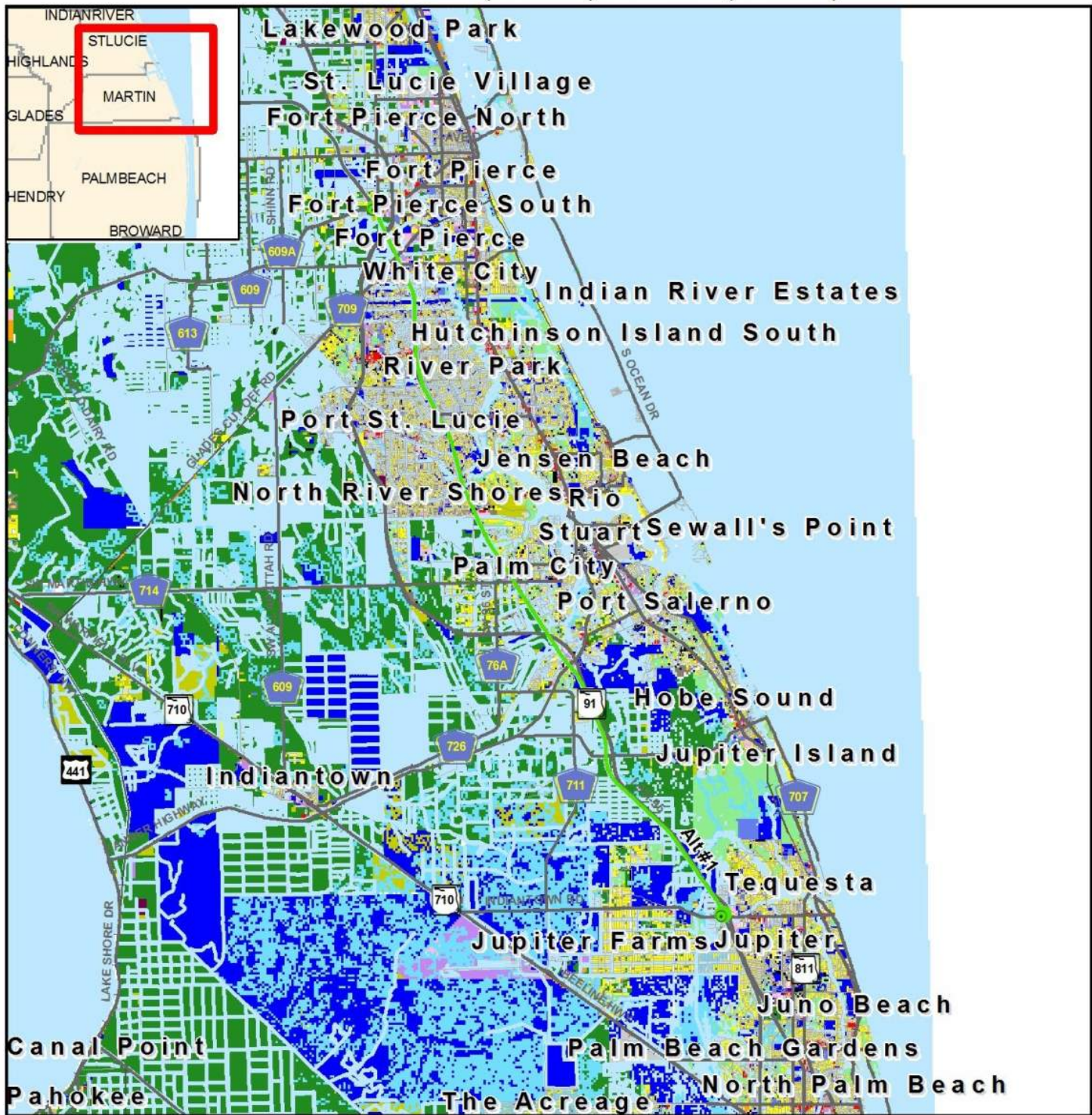
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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

## Indiantown Road (MP 116) to SR 70 (MP 152)



**Land Use Map**

<span style="color: green;">■</span> ETDM Alternative	<span style="color: green;">■</span> Agricultural	<span style="color: orange;">■</span> Other	<span style="color: red;">■</span> Retail/Office
<span style="color: green;">●</span> ETDM Alternative Terminus	<span style="color: purple;">■</span> Industrial	<span style="color: blue;">■</span> Public	<span style="color: brown;">■</span> Vacant (Residential)
<span style="color: black;">—</span> Major Road	<span style="color: blue;">■</span> Institutional	<span style="color: black;">■</span> Right-of-Way	<span style="color: pink;">■</span> Vacant (Nonresidential)
<span style="color: grey;">—</span> Local Road or Trail	<span style="color: purple;">■</span> Mining	<span style="color: lightgreen;">■</span> Recreational	<span style="color: lightblue;">■</span> Water
	<span style="color: yellow;">■</span> Open (Not Agricultural)	<span style="color: yellow;">■</span> Residential	<span style="border: 1px solid black; display: inline-block; width: 10px; height: 10px;"></span> No Data

Data Sources:  
 NAVTEQ  
 US Geological Survey  
 Florida Department of Revenue  
 Florida Department of Transportation  
 Florida County Property Appraiser Offices

0 2.5 5 10 Miles

N

12/22/2016

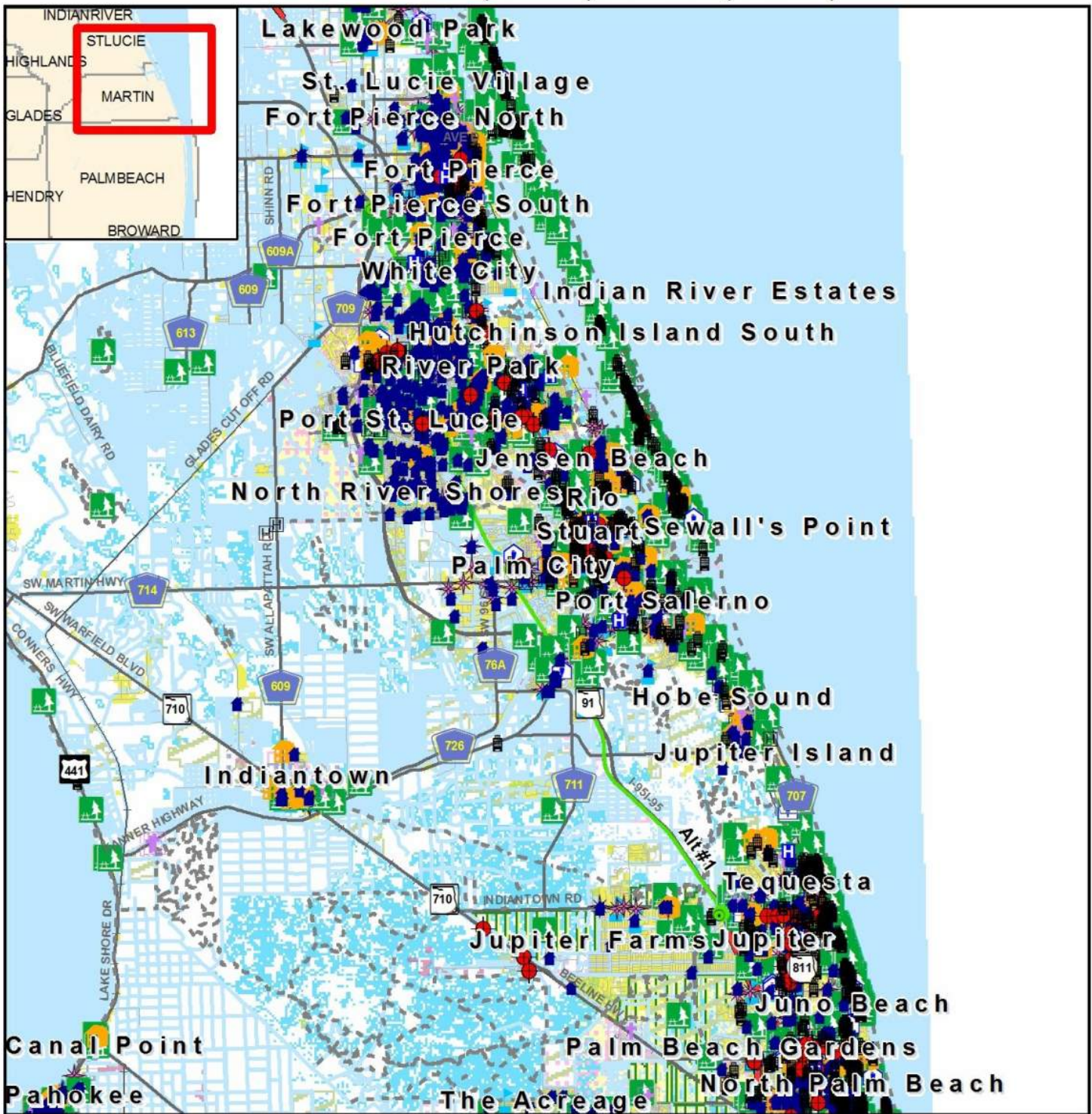
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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce Fort Pierce, Alternative #1 Indiantown Road (MP 116) to SR 70 (MP 152)



### Noise Map

ETDM Alternative	Condo Owners Associations	Cultural Center	Historic Cemetery	ETAT.LU_NW_INDUSTRIAL_13
ETDM Alternative Terminus	Hospitals	Health Care	Planned Unit Developments	ETAT.LU_NW_RESIDENT_13
City Limits	Laser On-site	Park	Wildlife Refuges	HUD Renewal
Noise Barriers	Group Care Facilities	National Parks	National Park Projects	Nat'l Estuarine Reserves
Existing Trails	Cemetery	Place of Worship	Marine Sanctuaries	Enterprise Zones
	Community Center	School	Military Installations	DRI

0 2 4 8 Miles

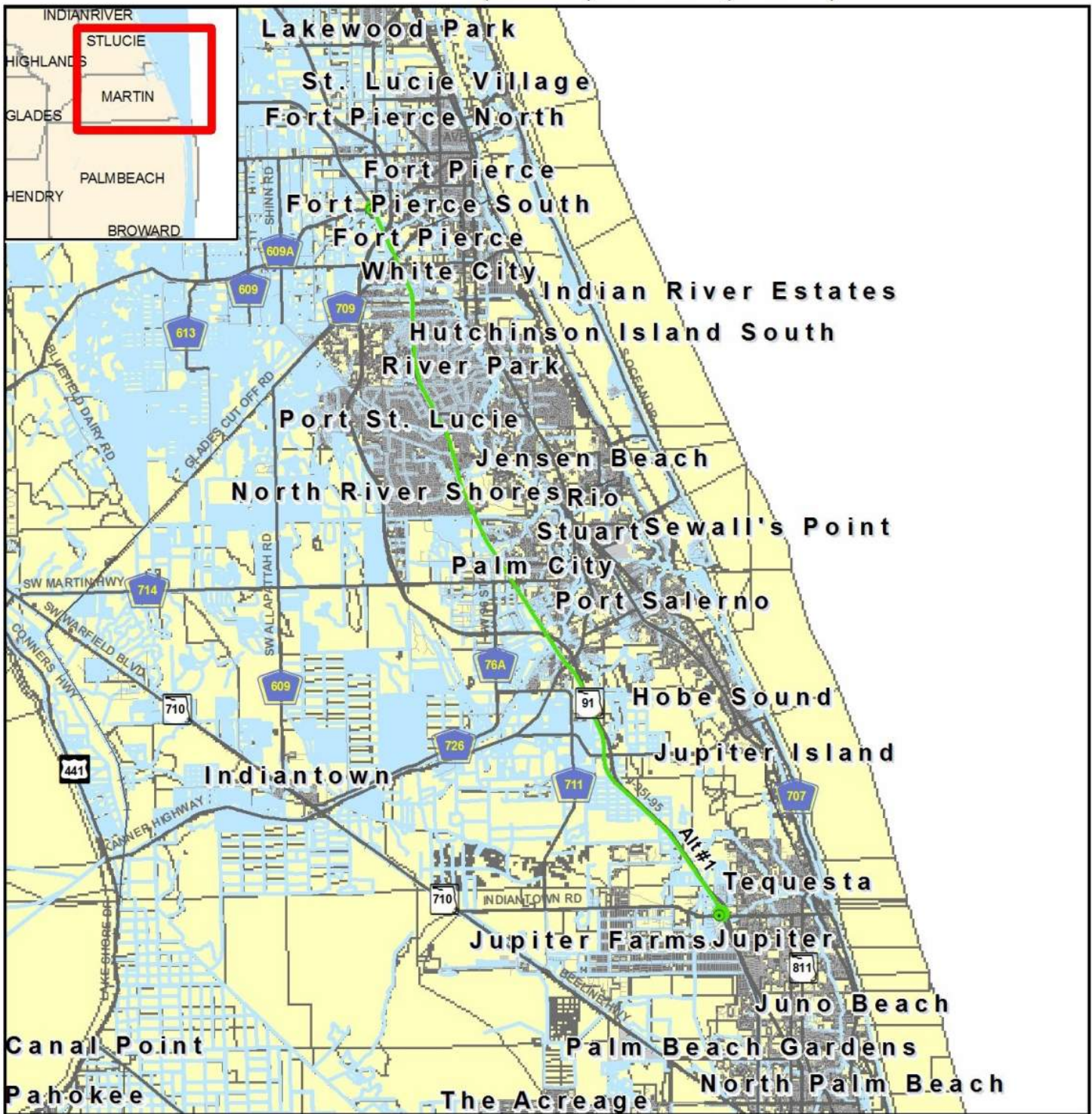
Data Sources: GeoPlan Center, US Geological Survey, US Census Bureau, HUD, Florida DOT, US Fish and Wildlife, National Park Service, NOAA, National Estuarine Research, Enterprise Florida

12/22/2016

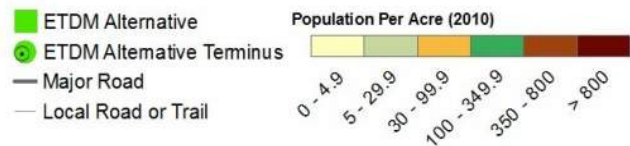
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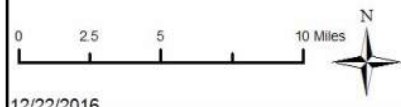
**14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1**  
**Indiantown Road (MP 116) to SR 70 (MP 152)**



**Population Density Map**



Data Sources:  
 US Geological Survey  
 FL Department of Transportation  
 NAVTEQ  
 US Census Bureau (2010)



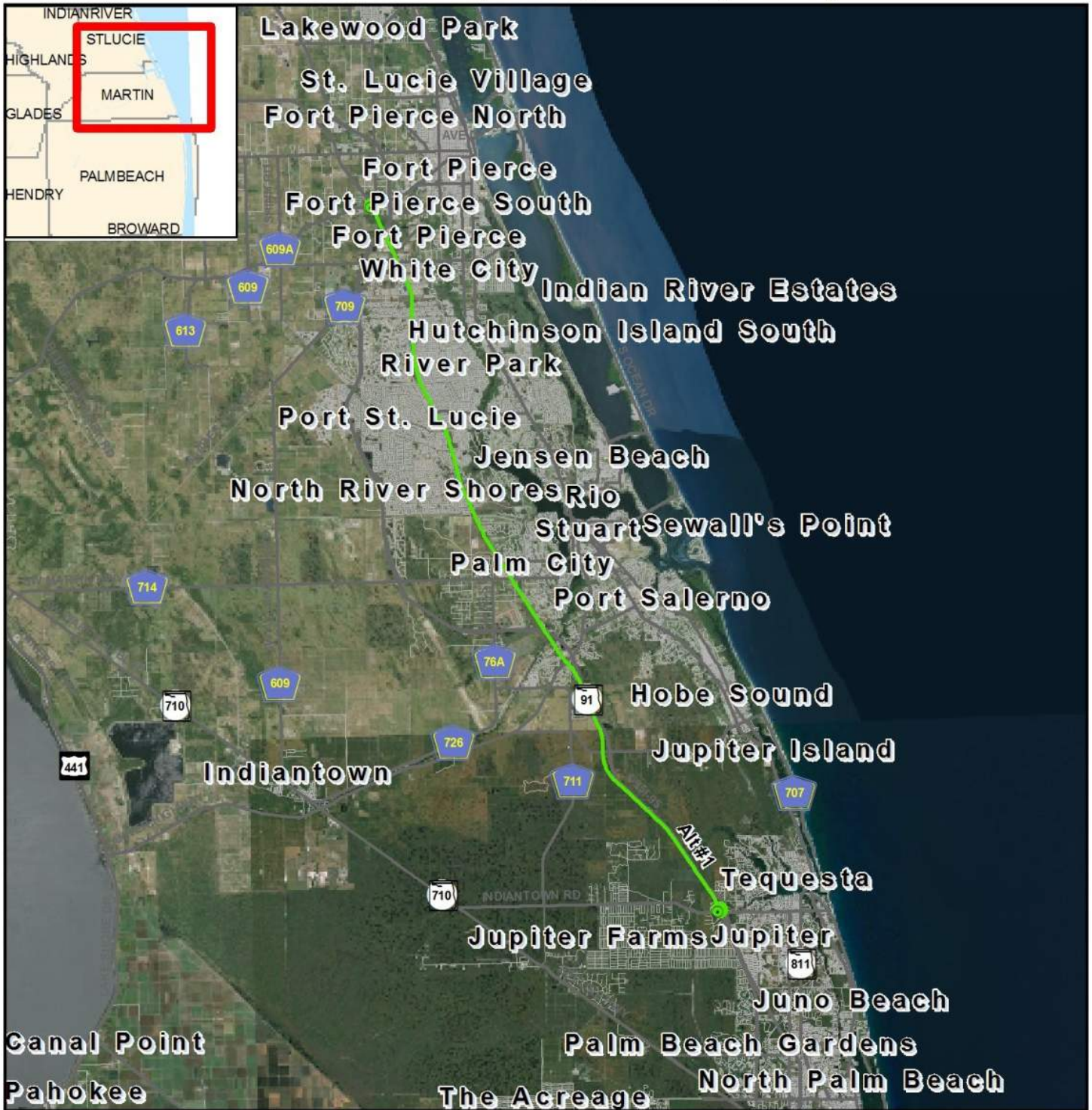
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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce to Fort Pierce, Alternative #1

Indiantown Road (MP 116) to SR 70 (MP 152)



## Project Aerial Map

- ETDM Alternative
- ETDM Alternative Terminus
- Major Road
- Local Road or Trail

Data Sources:  
 Highways - NAVTEQ  
 Digital Orthophotograph - ArcGIS Online



12/22/2016

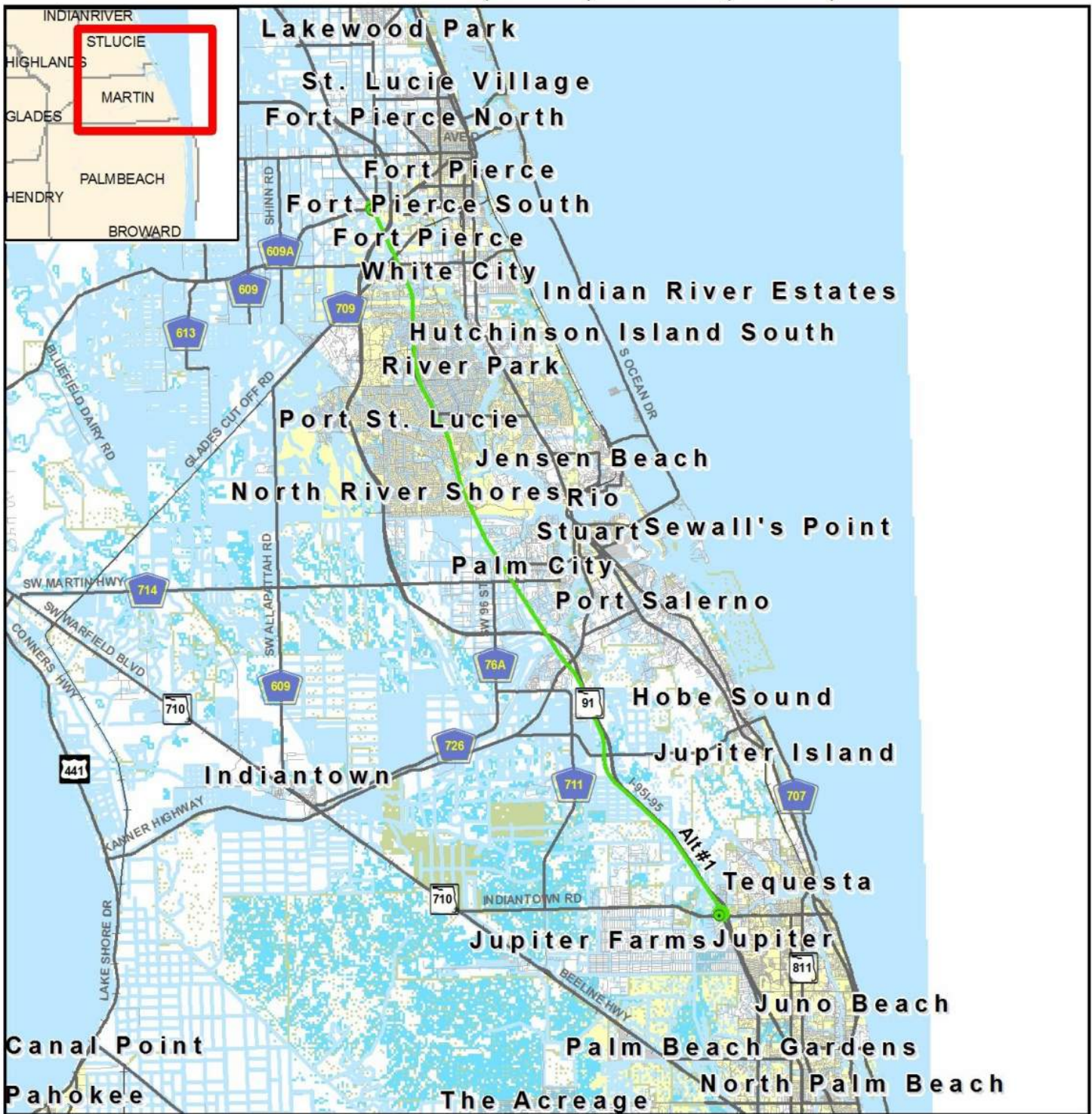
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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

Indiantown Road (MP 116) to SR 70 (MP 152)



## Project Base Map

- ETDM Alternative
- ETDM Alternative Terminus
- Local Road or Trail
- Major Road
- City Limits
- Managed Conservation Lands

Data Sources:  
 NAVTEQ  
 US Geological Survey  
 US Census Bureau  
 County Property Appraisers  
 Florida Natural Areas Inventory



12/22/2016

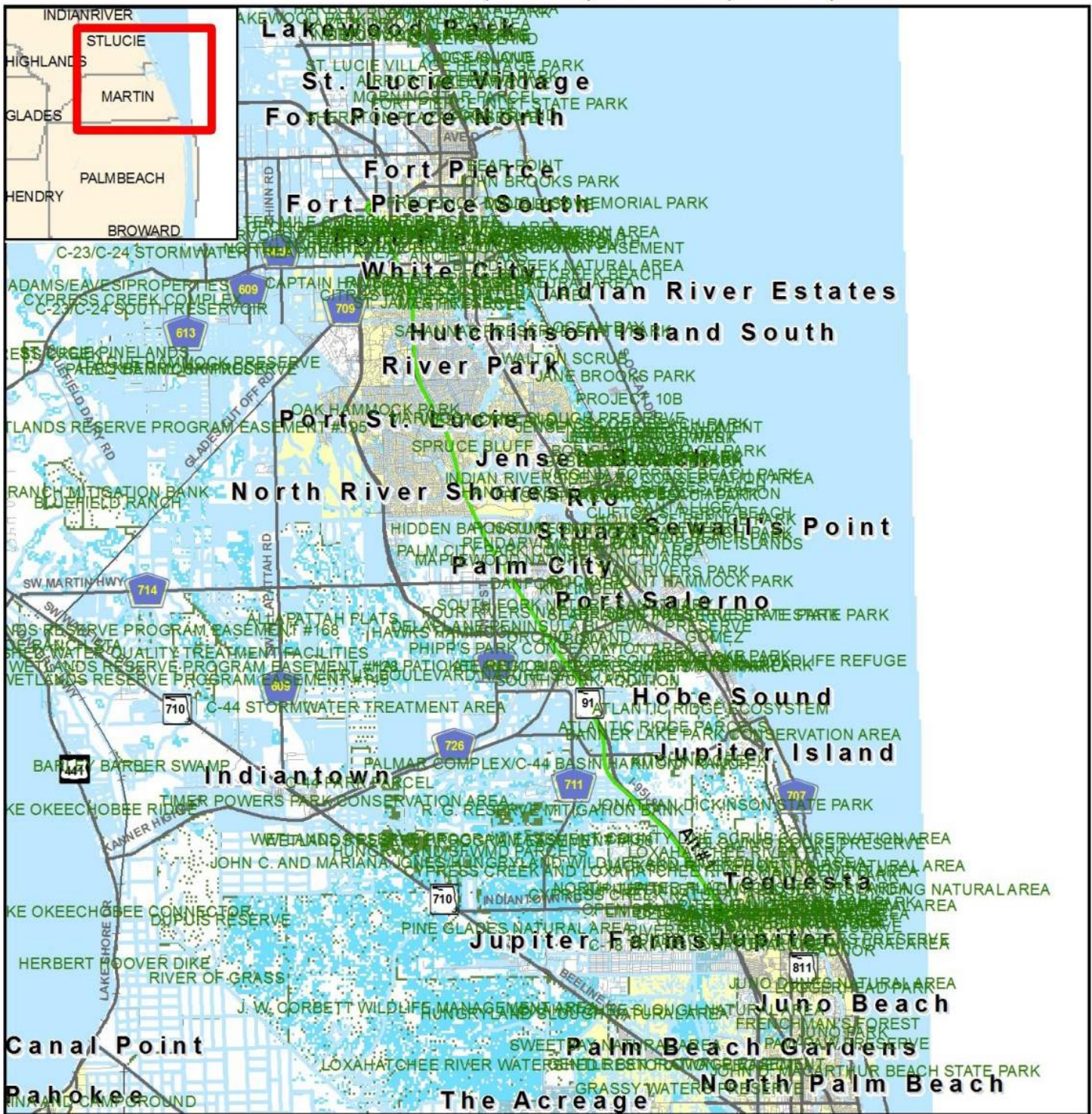
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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

## Indiantown Road (MP 116) to SR 70 (MP 152)



### Recreational Areas Map

<span style="color: green;">■</span> ETDM Alternative	<span style="border: 1px dashed gray; padding: 2px;"> </span> Conservation or Recreation Area	Data Sources: NAVTEQ US Geological Survey Florida Natural Areas Inventory
<span style="color: green;">●</span> ETDM Alternative Terminus		
— Major Road		
— Local Road or Trail		
■ City Limits		

0 2.5 5 10 Miles

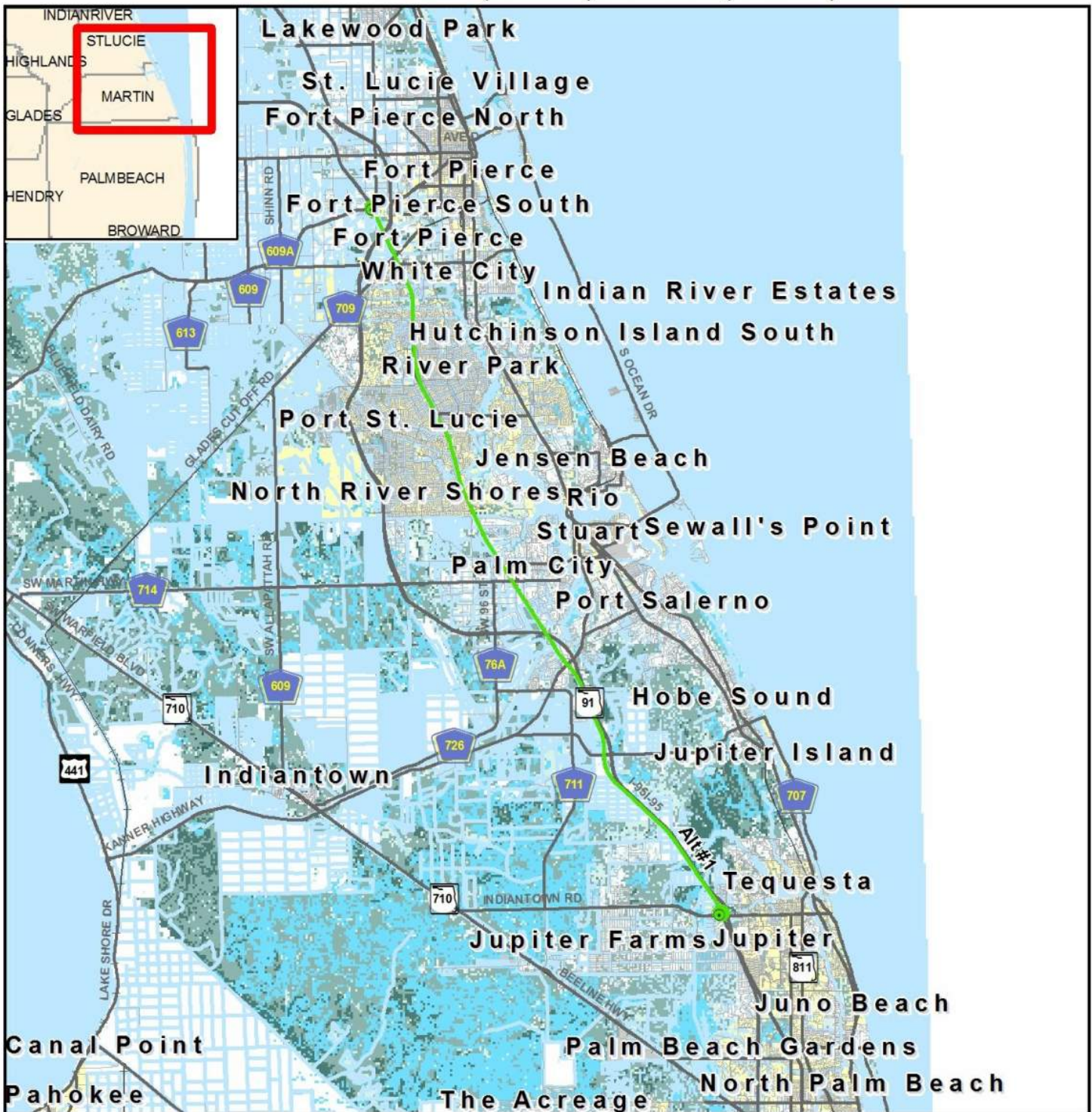
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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

Indiantown Road (MP 116) to SR 70 (MP 152)



### Species Potential Map

- ETDM Alternative
- ETDM Alternative Terminus
- Major Road
- Local Road or Trail
- City Limits

**Potential Habitat Richness**

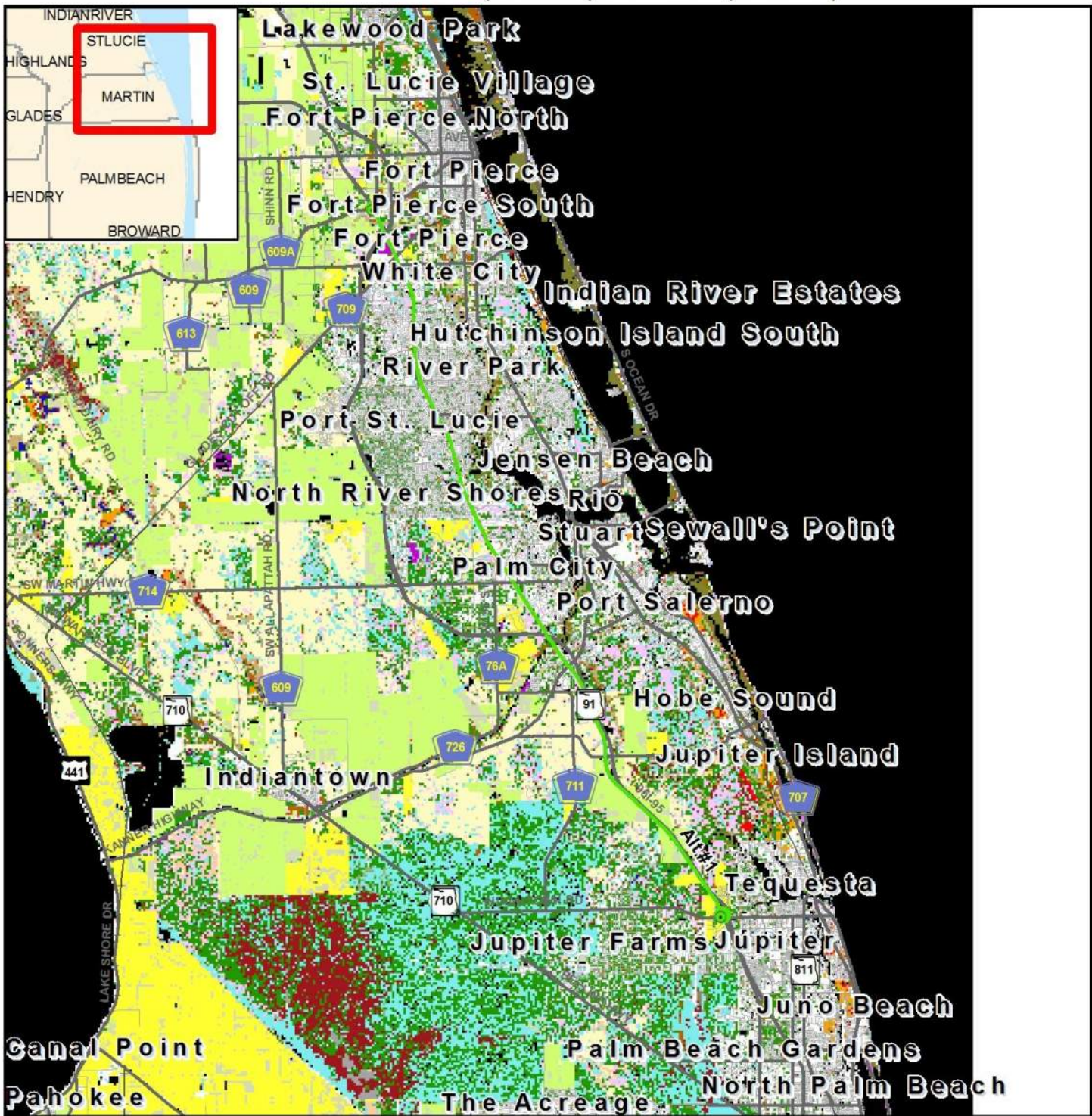
Data Sources:  
 NAVTEQ  
 US Geological Survey  
 Florida Department of Transportation  
 Florida Fish & Wildlife Conservation Commission

12/22/2016

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**14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1**  
**Indiantown Road (MP 116) to SR 70 (MP 152)**



**Vegetation Map**

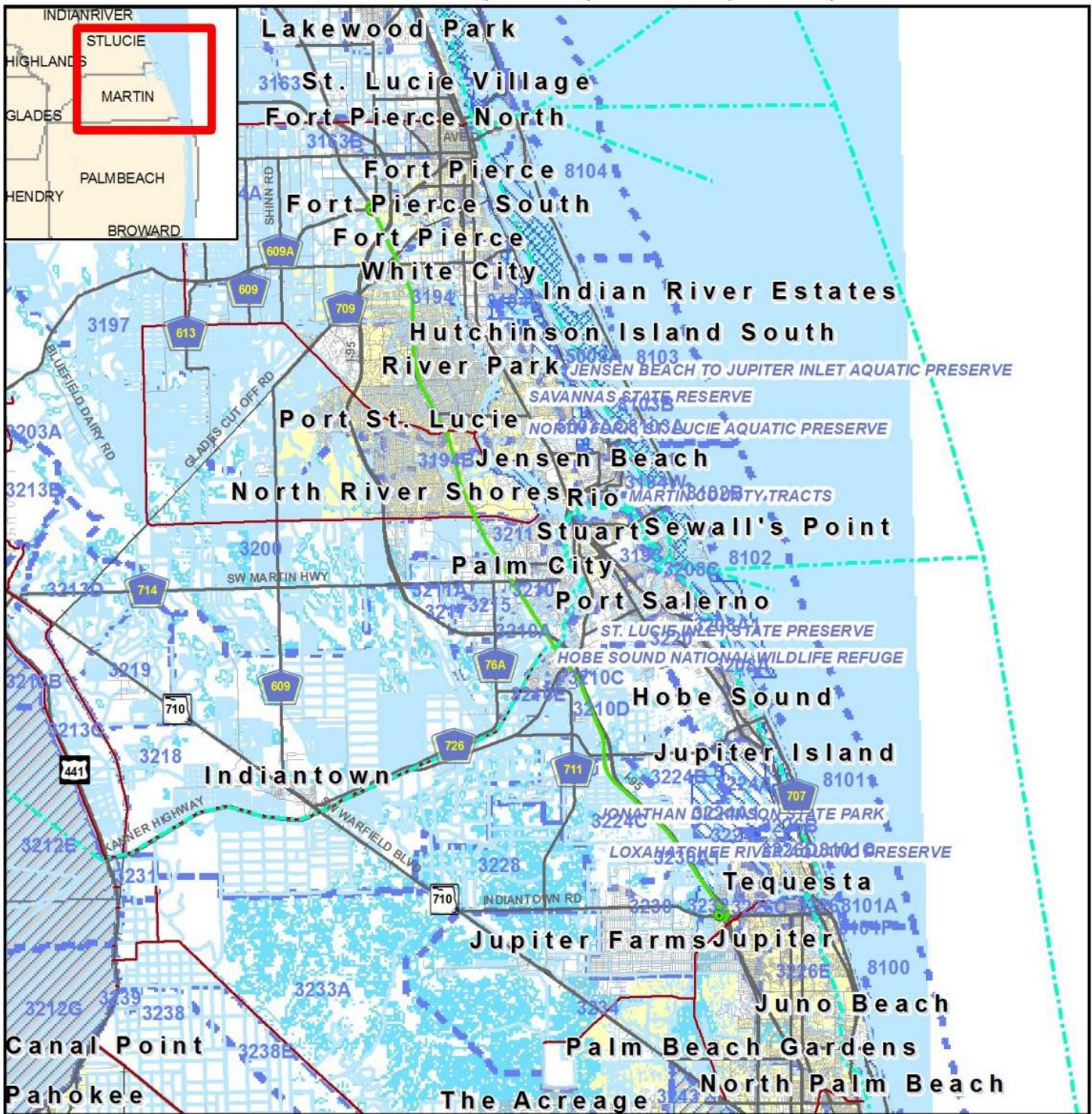


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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

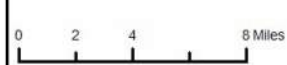
Indiantown Road (MP 116) to SR 70 (MP 152)



## Water Resource Map

- ETDM Alternative
- ETDM Alternative Terminus
- Major Road
- Local Road or Trail
- City Limits
- 1st Magnitude Spring
- River, Stream or Canal
- Navigable Water Way
- SFWMD Canals
- Drainage Basin
- Outstanding Florida Water
- Surface Water Class I
- Surface Water Class II
- Water Body
- Swamp/Marsh

Data Sources:  
 NAVTEQ  
 US Geological Survey  
 Florida Department of Transportation  
 Florida Department of Environmental Protection  
 Florida Geological Survey  
 US Bureau of Transportation Statistics



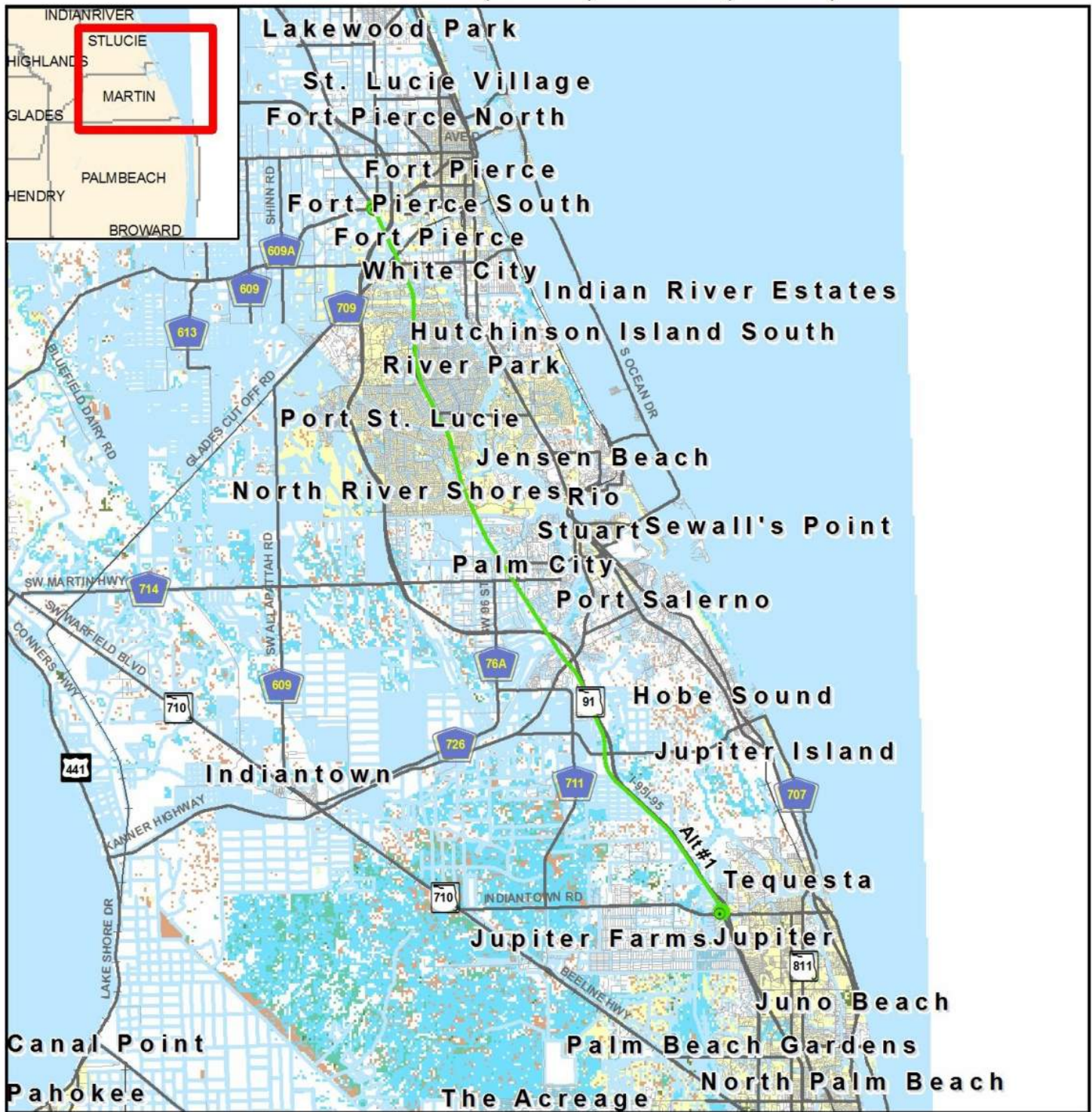
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# 14295 Turnpike Mainline Widening from Jupiter to Fort Pierce, Alternative #1

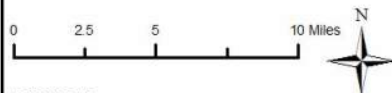
Indiantown Road (MP 116) to SR 70 (MP 152)



## Wetlands and Surface Waters Map

- ETDM Alternative
- ETDM Alternative Terminus
- Major Road
- Local Road or Trail
- City Limits
- River, Stream or Canal
- Water Body
- Swamp/Marsh
- Non-vegetated Wetland
- Vegetated Non-forested Wetland
- Wetland Forested Mixed
- Wetland Coniferous Forest
- Wetland Hardwood Forest

Data Sources:  
 NAVTEQ  
 Florida Water Management Districts  
 US Geological Survey



12/22/2016

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## Appendices

### PED Comments

#### Advance Notification Comments

##### US Army Corps of Engineers Comment --

The Corps has reviewed the information and has provided comments in the Wetlands and Navigation sections under the Project Effects.

--Tarrie L Ostrofsky, 2/22/2017

##### Response --

--, \$tools.date.format("M/d/yyyy"),\$comment.responseTimestamp)

### GIS Analyses

Since there are so many GIS Analyses available for Project #14295 - Turnpike Mainline Widening from Jupiter to Fort Pierce, they have not been included in this ETDM Summary Report. GIS Analyses, however, are always available for this project on the Public ETDM Website. Please click on the link below (or copy this link into your Web Browser) in order to view detailed GIS tabular information for this project:

<http://etdmpub.fla-etat.org/est/index.jsp?tpID=14295&startPageName=GIS%20Analysis%20Results>

**Special Note:** Please be sure that when the GIS Analysis Results page loads, the **Project Published 5/19/2017 Milestone** is selected. GIS Analyses snapshots have been taken for Project #14295 at various points throughout the project's life-cycle, so it is important that you view the correct snapshot.

### Project Attachments

There are no attachments for this project.

### Degree of Effect Legend

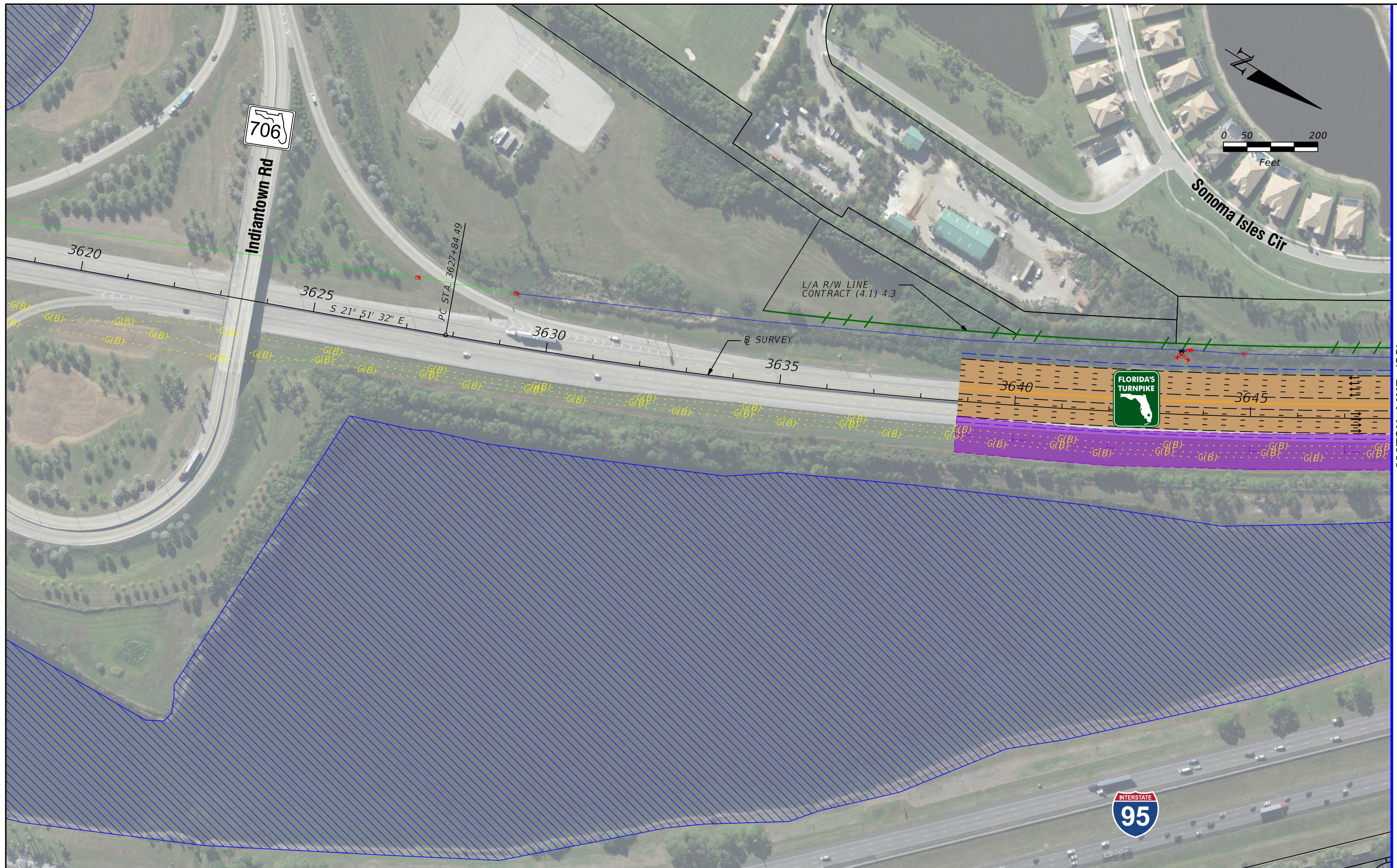
Color Code	Meaning	ETAT	Public Involvement
N/A	Not Applicable / No Involvement	There is no presence of the issue in relationship to the project, or the issue is irrelevant in relationship to the proposed transportation action.	
0	None (after 12/5/2005)	The issue is present, but the project will have no impact on the issue; project has no adverse effect on ETAT resources; permit issuance or consultation involves routine interaction with the agency. The <i>None</i> degree of effect is new as of 12/5/2005.	No community opposition to the planned project. No adverse effect on the community.
1	Enhanced	Project has positive effect on the ETAT resource or can reverse a previous adverse effect leading to environmental improvement.	Affected community supports the proposed project. Project has positive effect.
2	Minimal	Project has little adverse effect on ETAT resources. Permit issuance or consultation involves routine interaction with the agency. Low cost options are available to address concerns.	Minimum community opposition to the planned project. Minimum adverse effect on the community.
2	Minimal to None (assigned prior to 12/5/2005)	Project has little adverse effect on ETAT resources. Permit issuance or consultation involves routine interaction with the agency. Low cost options are available to address concerns.	Minimum community opposition to the planned project. Minimum adverse effect on the community.
3	Moderate	Agency resources are affected by the proposed project, but avoidance and minimization options are available and can be addressed during development with a moderated amount of agency involvement and moderate cost impact.	Project has adverse effect on elements of the affected community. Public Involvement is needed to seek alternatives more acceptable to the community. Moderate community interaction will be required during project development.
4	Substantial	The project has substantial adverse effects but ETAT understands the project need and will be able to seek avoidance and minimization or mitigation options during project development. Substantial interaction will be required during project development and permitting.	Project has substantial adverse effects on the community and faces substantial community opposition. Intensive community interaction with focused Public Involvement will be required during project development to address community concerns.
5	Potential Dispute (Planning Screen)	Project may not conform to agency statutory requirements and may not be permitted. Project modification or evaluation of alternatives is required before advancing to the LRTP Programming Screen.	Community strongly opposes the project. Project is not in conformity with local comprehensive plan and has severe negative impact on the affected community.
5	Dispute Resolution (Programming Screen)	Project does not conform to agency statutory requirements and will not be permitted. Dispute resolution is required before the project proceeds to programming.	Community strongly opposes the project. Project is not in conformity with local comprehensive plan and has severe negative impact on the affected community.

	No ETAT Consensus	ETAT members from different agencies assigned a different degree of effect to this project, and the ETDM coordinator has not assigned a summary degree of effect.
	No ETAT Reviews	No ETAT members have reviewed the corresponding issue for this project, and the ETDM coordinator has not assigned a summary degree of effect.



**APPENDIX C**  
*Preferred Alternative Conceptual Plans*





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
	PROPERTY LINES		PROPOSED ITS
	TRAFFIC BARRIER WALL		POTENTIAL NEW TRAFFIC SIGNAL
	EXISTING ROW LINE		EXISTING TRAFFIC SIGNAL
	EXISTING LIMITED ACCESS ROW LINE		TOLL GANTRY
	POTENTIAL ROW LINE		POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
	POTENTIAL LIMITED ACCESS ROW LINE		
	RAILROAD		
	PROPOSED ROW NOISE BARRIER		EXISTING BRIDGE
	PROPOSED SHOULDER NOISE BARRIER		OTHER PROJECTS (BRIDGE)
	PAVEMENT REMOVAL		OTHER PROJECTS (ROADS)
	WETLANDS/OTHER SURFACE WATERS		FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
	PROPOSED ROADWAY		POTENTIAL BUSINESS RELOCATION
	PROPOSED BRIDGE		POTENTIAL RESIDENTIAL RELOCATION

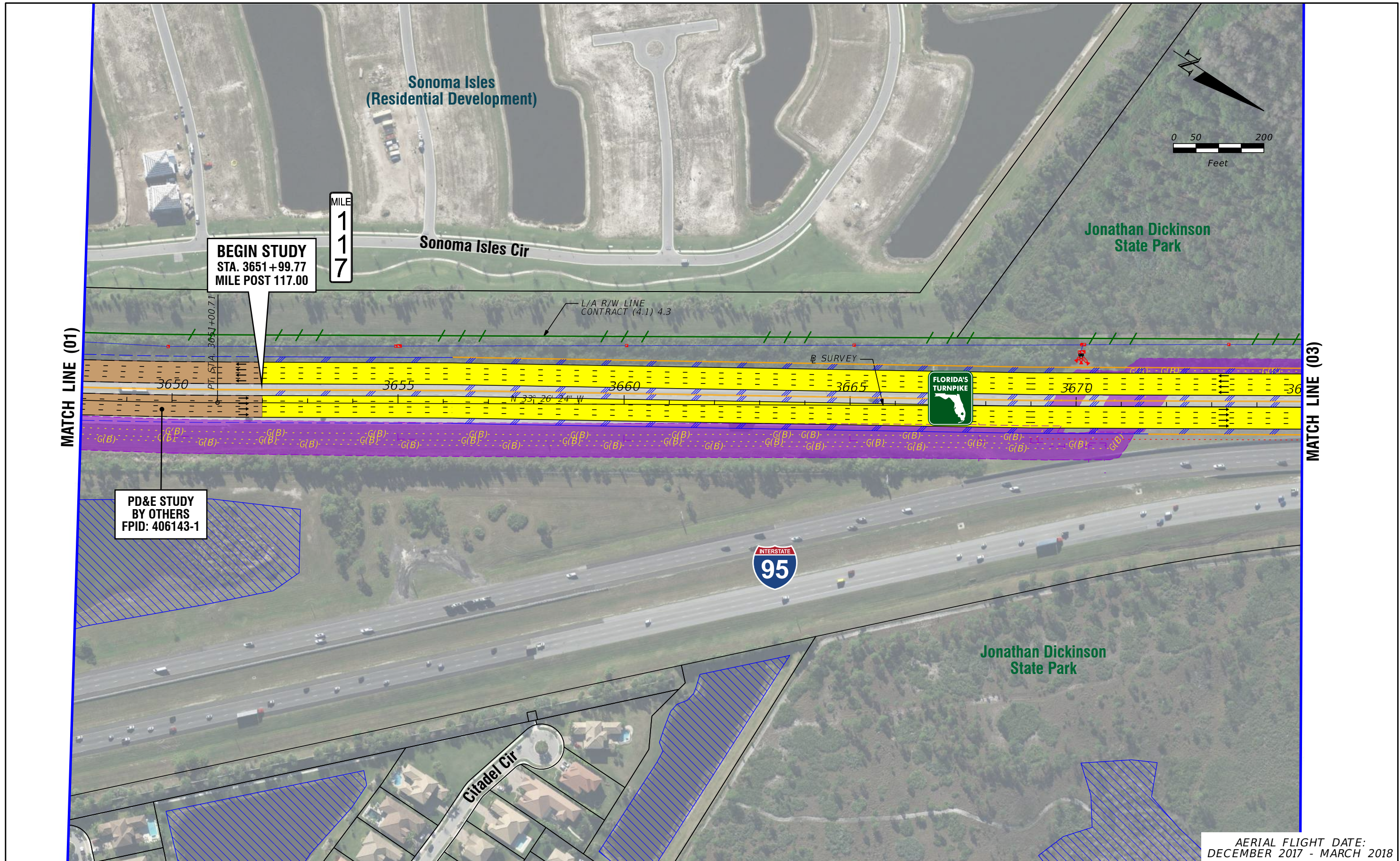
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TO FT. PIERCE (OKEECHOBEE ROAD/SR 70)  
PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY  
FINANCIAL PROJECT IDENTIFICATION (FPID) NUMBER: 423374-1

**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**1**





**BEGIN STUDY**  
 STA. 3651+99.77  
 MILE POST 117.00

MILE  
 117



**PD&E STUDY**  
 BY OTHERS  
 FPID: 406143-1

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

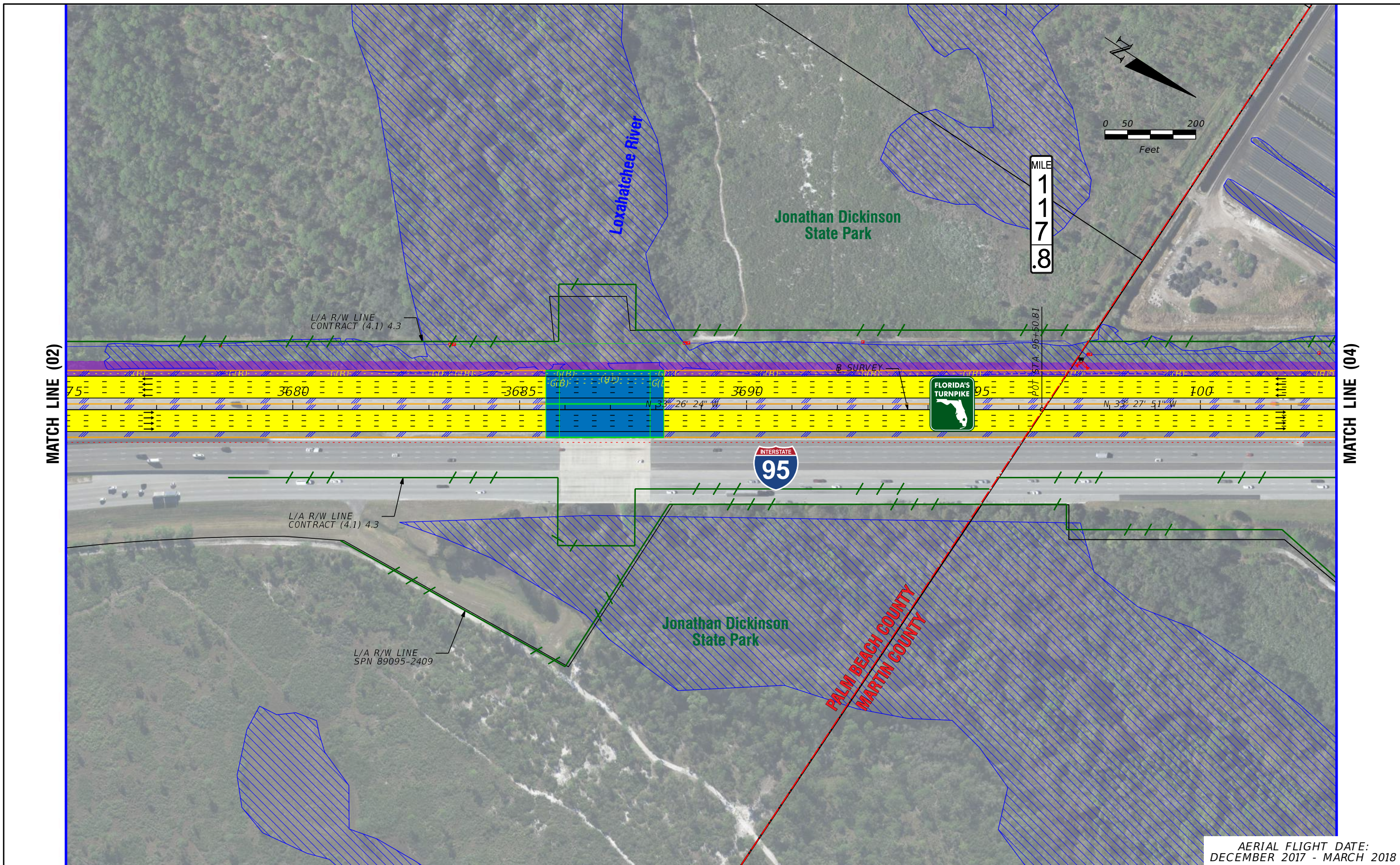
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 2





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

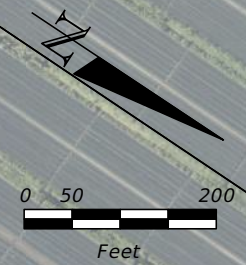
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PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
3





MILE  
1  
1  
8

MATCH LINE (03)

MATCH LINE (05)



Jonathan Dickinson  
State Park

AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

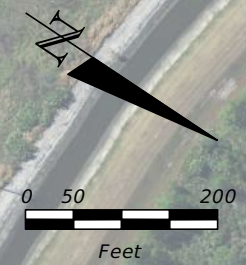
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CONCEPT PLAN**

SHEET NO.  
4





MILE  
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MILE  
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MATCH LINE (04)

MATCH LINE (06)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

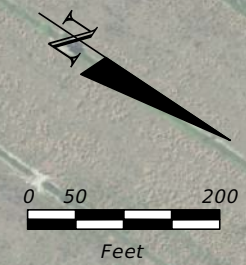
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CONCEPT PLAN**

SHEET NO.  
5





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MATCH LINE (05)

MATCH LINE (07)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	NOTE: ROW = RIGHT OF WAY	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)		—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)		—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH		—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION		—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION				
—	RAILROAD								

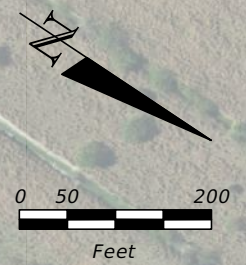
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SHEET NO.  
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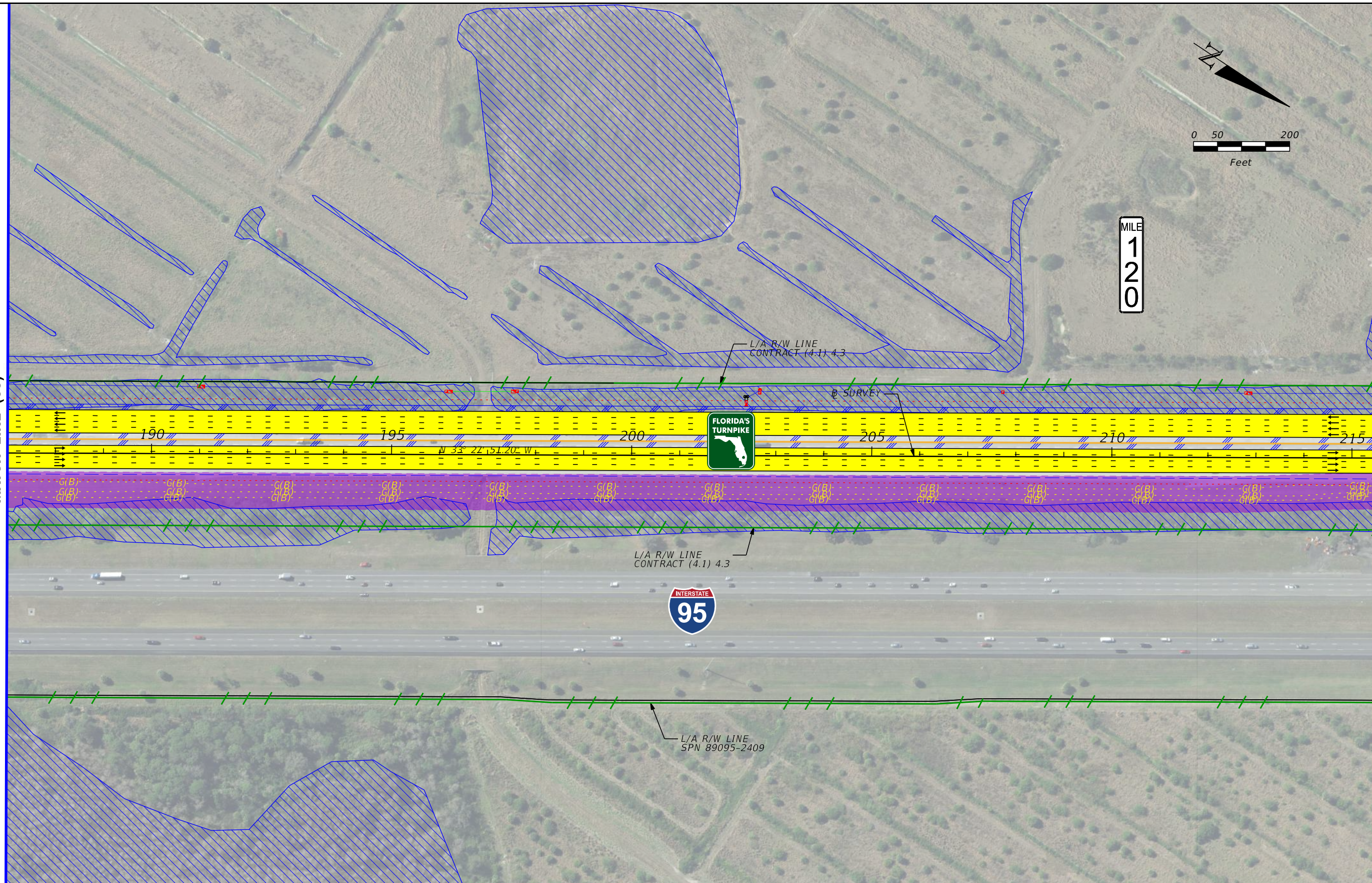




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2  
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MATCH LINE (06)

MATCH LINE (08)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

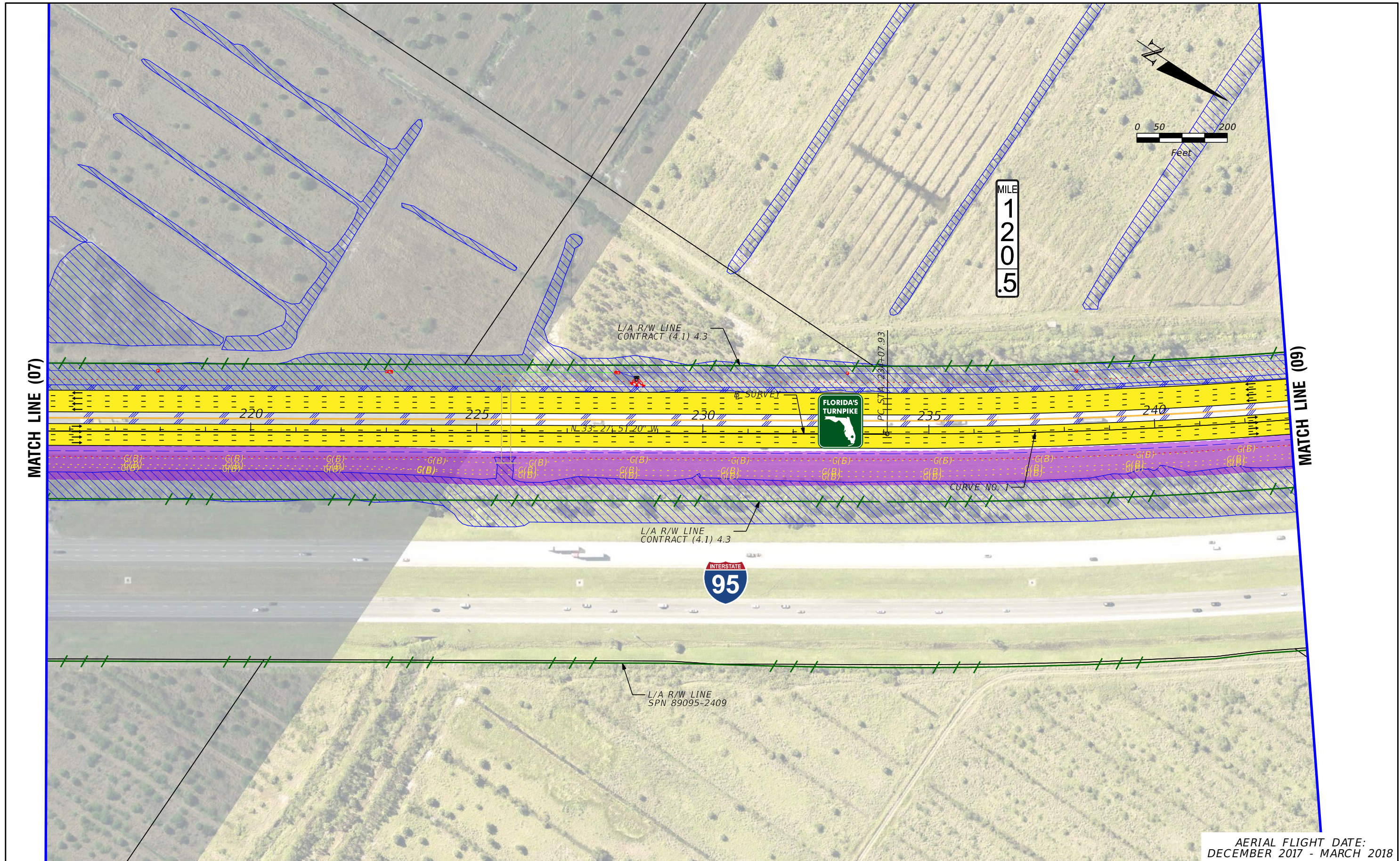
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PREFERRED ALTERNATIVE  
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SHEET NO.  
7





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

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SHEET NO.  
**8**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

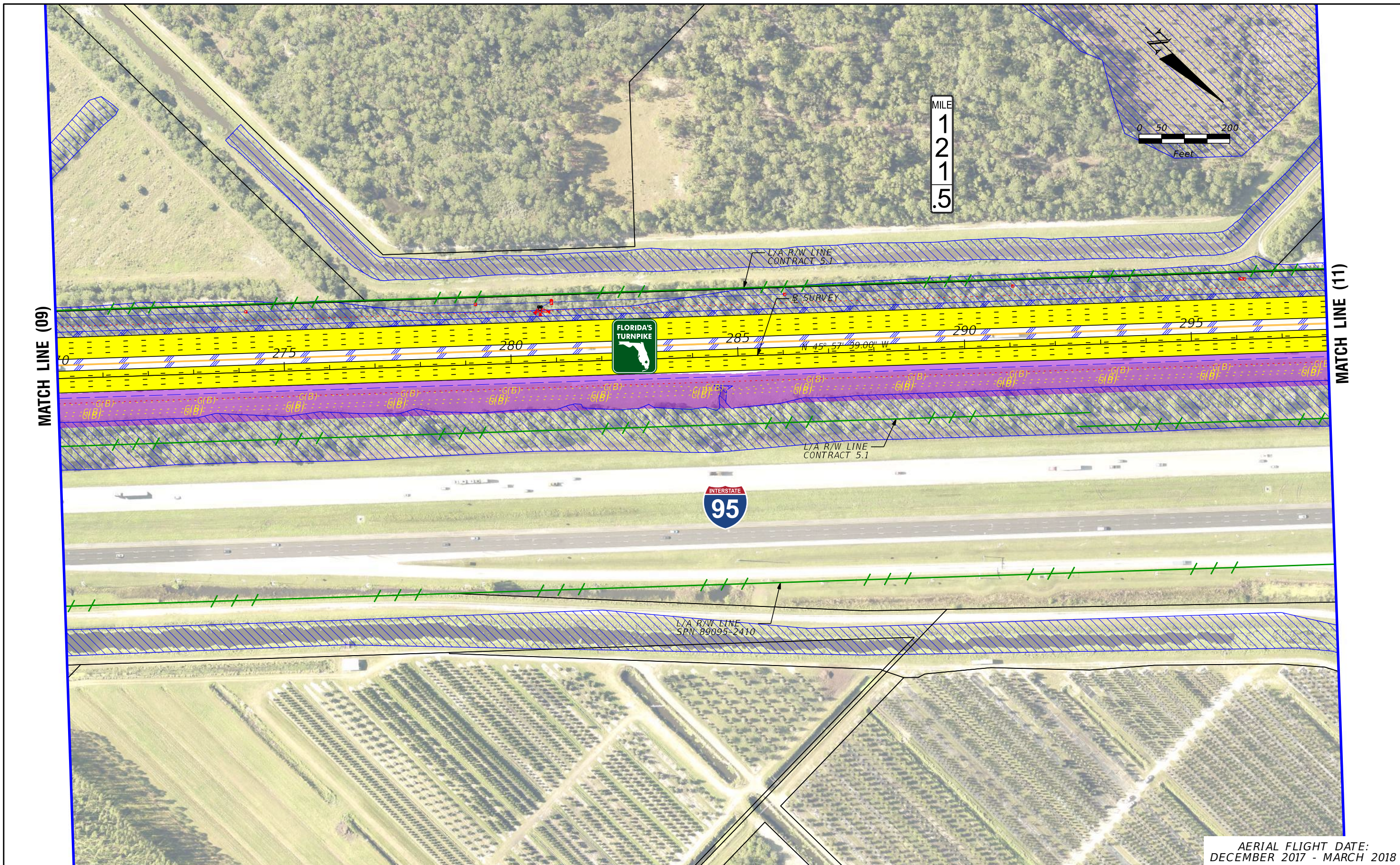
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SHEET NO.  
**9**





<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

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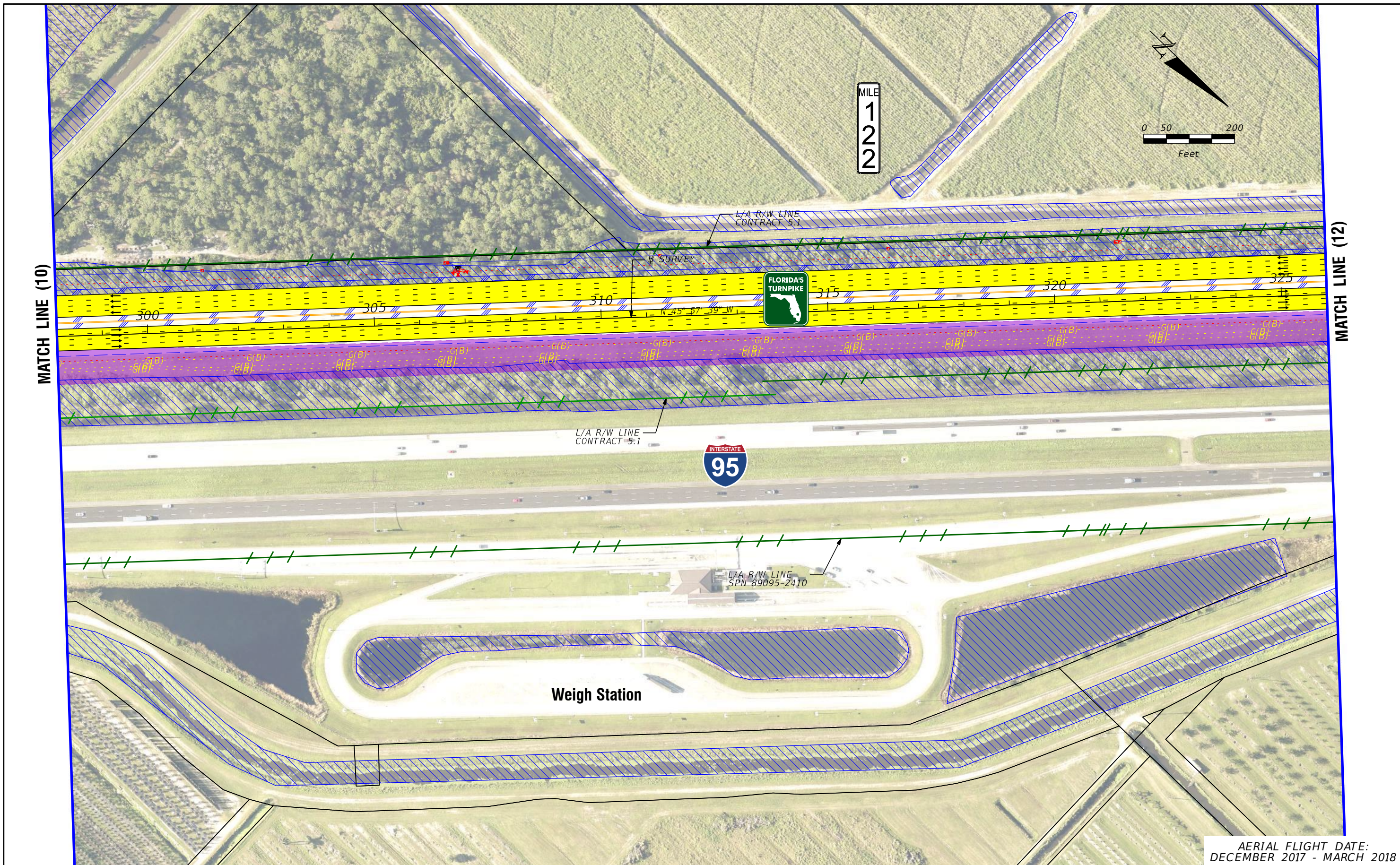
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 CONCEPT PLAN**

SHEET NO.  
 10

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

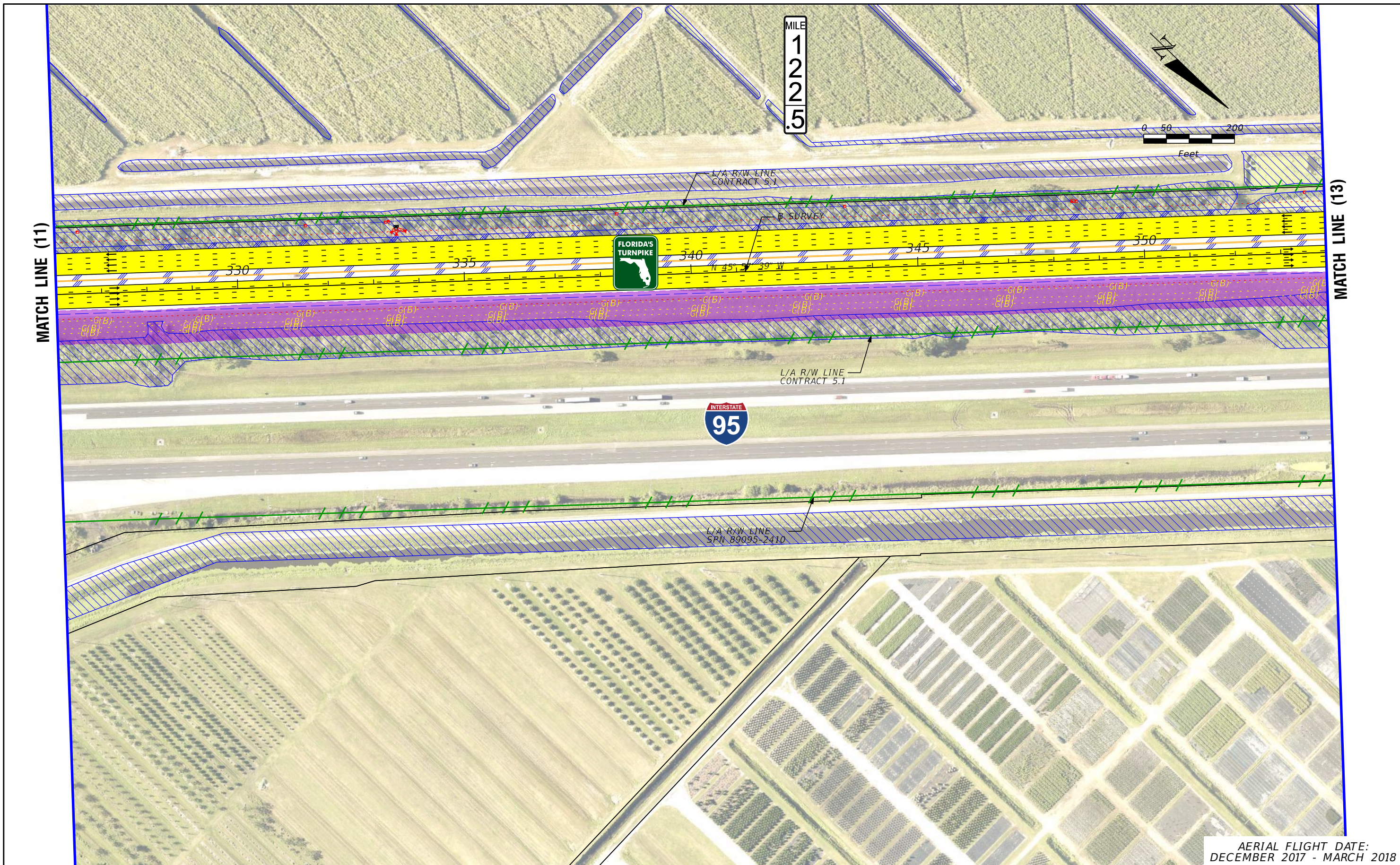
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AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

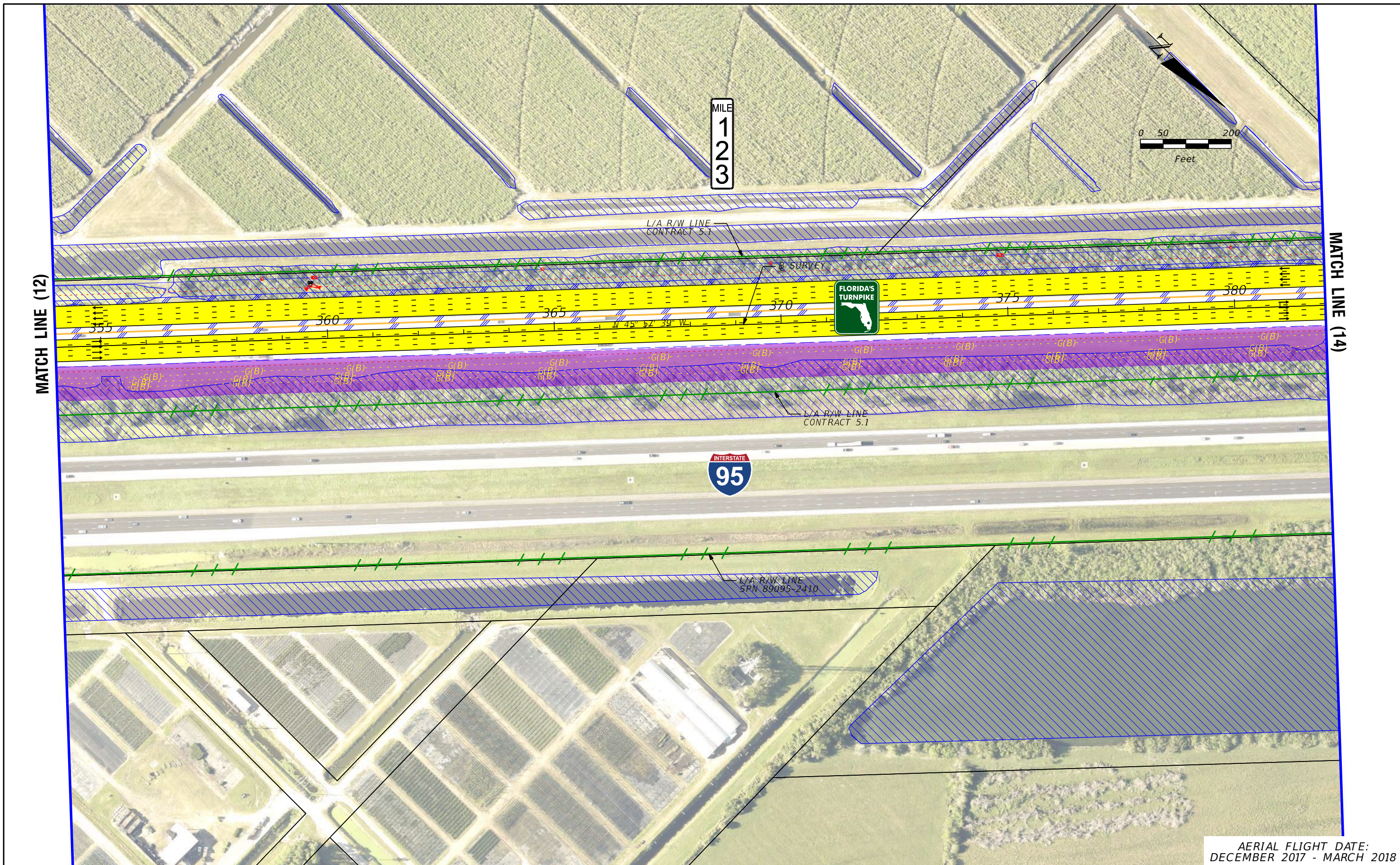
H. W. LOCHNER, INC.  
4350 W. CYPRESS ST., SUITE 800  
TAMPA, FLORIDA 33607  
FBPR CERTIFICATE OF AUTH. #894

FLORIDA'S TURNPIKE (SR 91) WIDENING  
FROM JUPITER (INDIANTOWN ROAD/SR 706)  
TO FT. PIERCE (OKEECHOBEE ROAD/SR 70)  
PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY  
FINANCIAL PROJECT IDENTIFICATION (FPID) NUMBER: 423374-1

**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**12**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD		
—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE
—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)
—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)
—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION
—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION

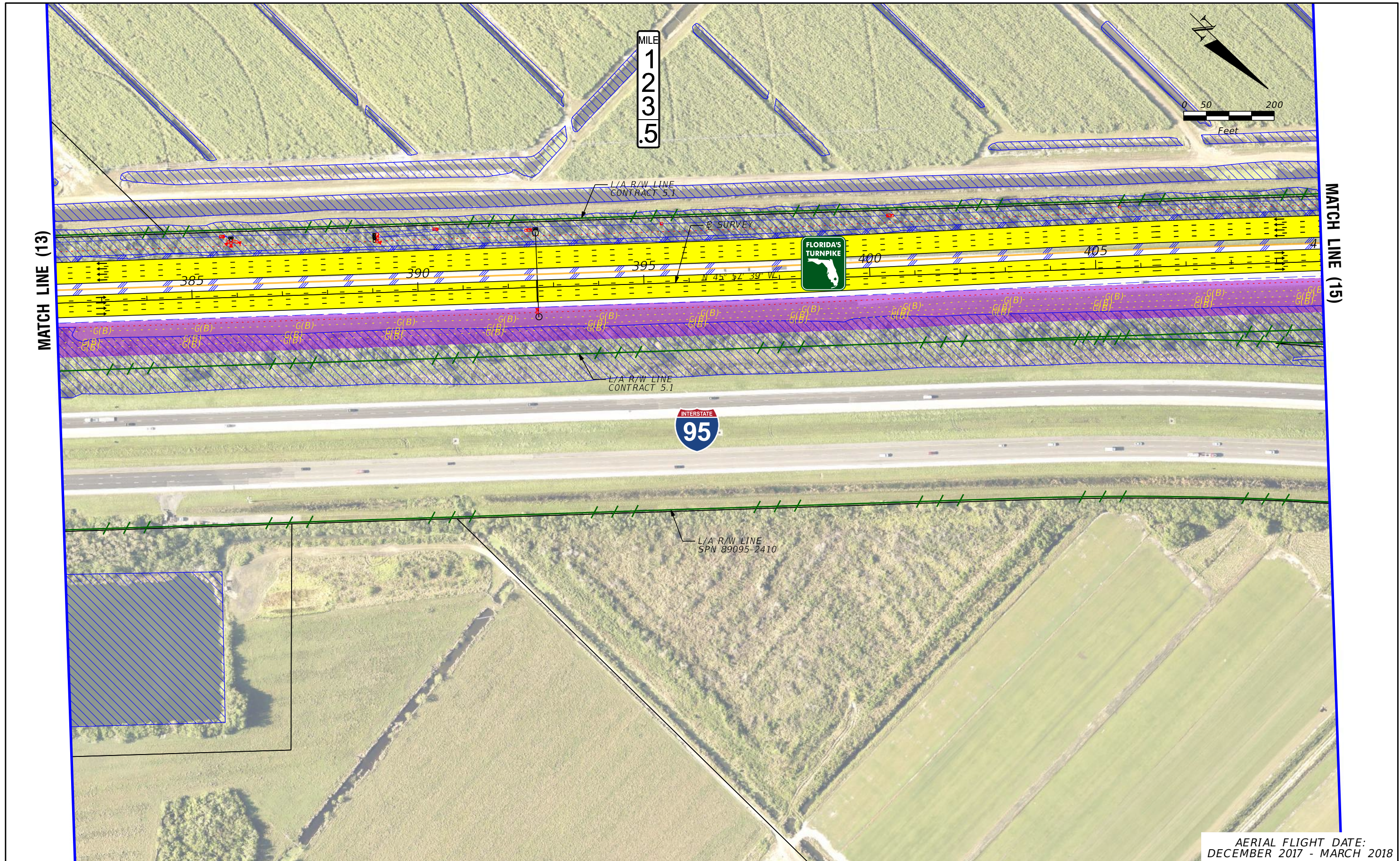
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**13**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

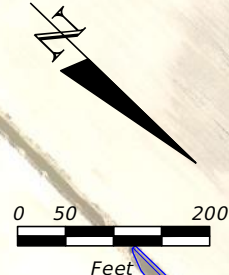
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**14**

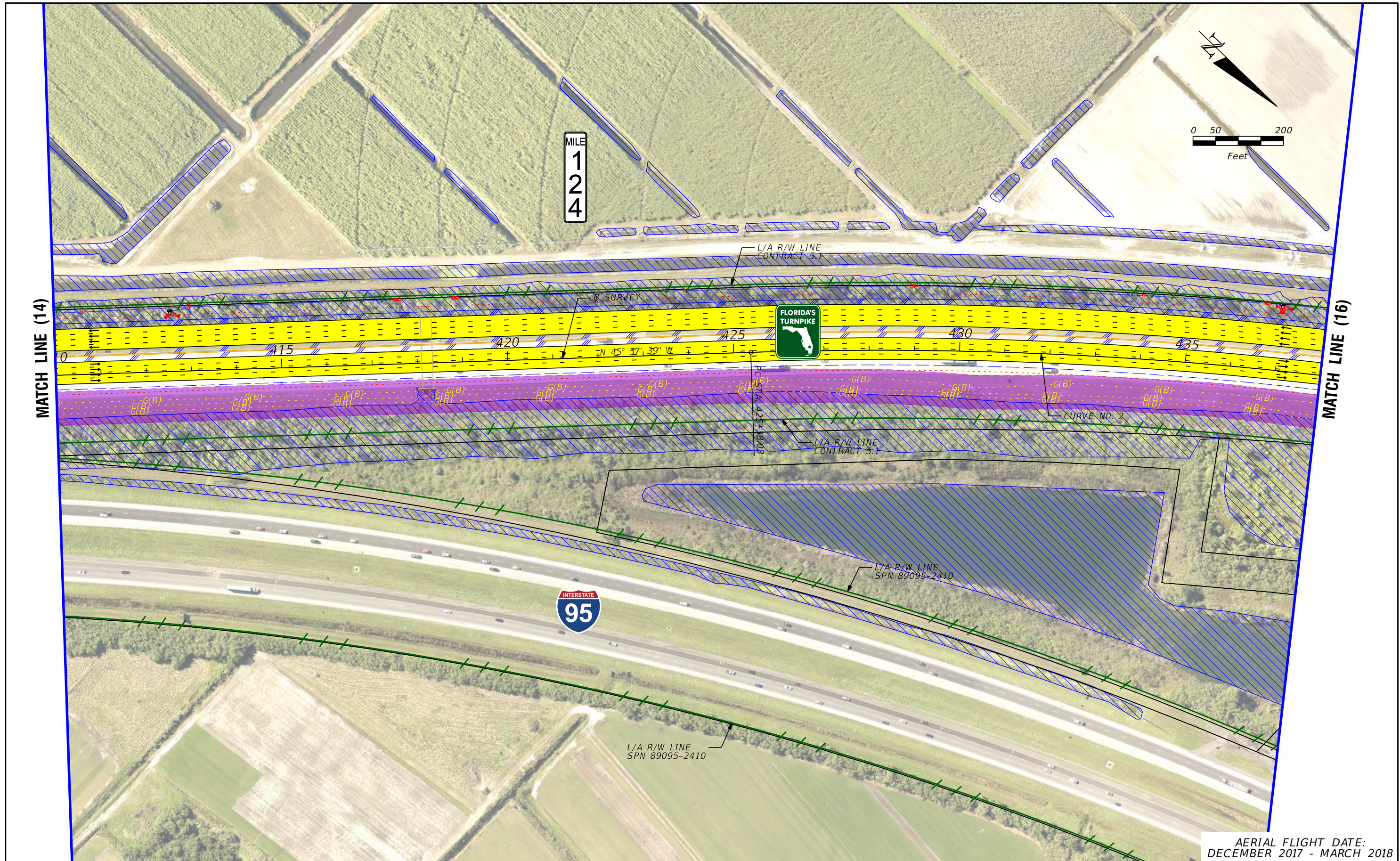




MILE  
1  
2  
4

MATCH LINE (14)

MATCH LINE (16)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

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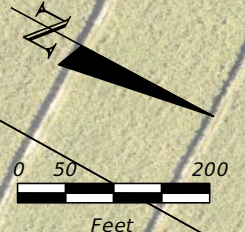
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**15**



MILE  
1  
2  
4  
5



MATCH LINE (15)

MATCH LINE (17)



—	PROPERTY LINES	▨	PROPOSED ROW NOISE BARRIER	▨	EXISTING BRIDGE	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	▨	PROPOSED SHOULDER NOISE BARRIER	▨	OTHER PROJECTS (BRIDGE)	●	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	▨	PAVEMENT REMOVAL	▨	OTHER PROJECTS (ROADS)	●	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	▨	WETLANDS/OTHER SURFACE WATERS	▨	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	●	TOLL GANTRY
—	POTENTIAL ROW LINE	▨	PROPOSED ROADWAY	Ⓟ	POTENTIAL BUSINESS RELOCATION	▲	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	▨	PROPOSED BRIDGE	Ⓡ	POTENTIAL RESIDENTIAL RELOCATION		
—	RAILROAD						

NOTE: ROW = RIGHT OF WAY

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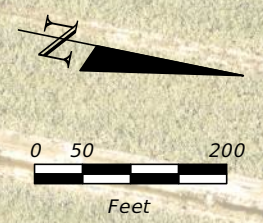
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**16**

AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

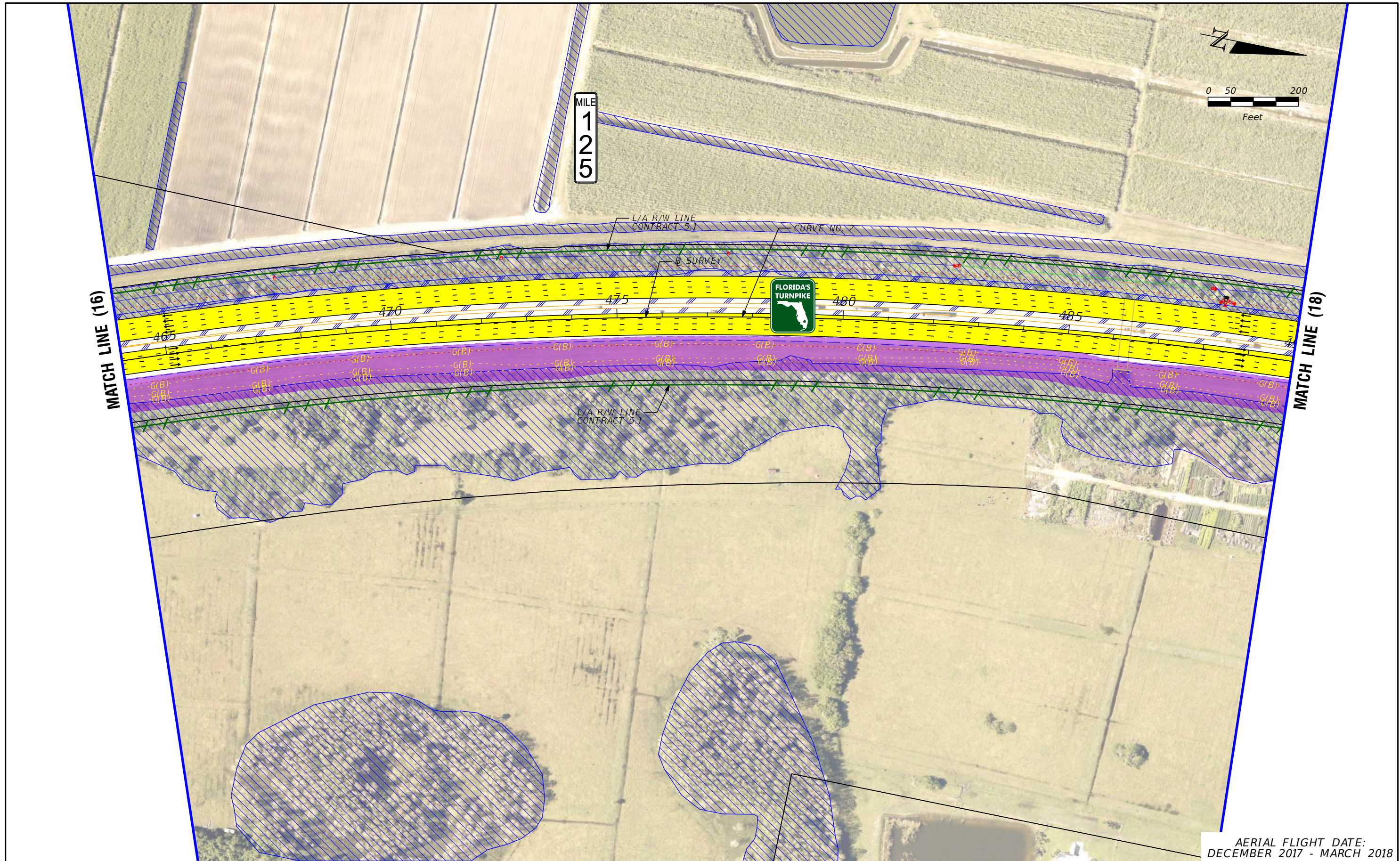




MILE  
1  
2  
5

MATCH LINE (16)

MATCH LINE (18)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	—	
—	RAILROAD			

NOTE: ROW = RIGHT OF WAY

—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE
—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)
—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)
—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION
—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION

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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
17

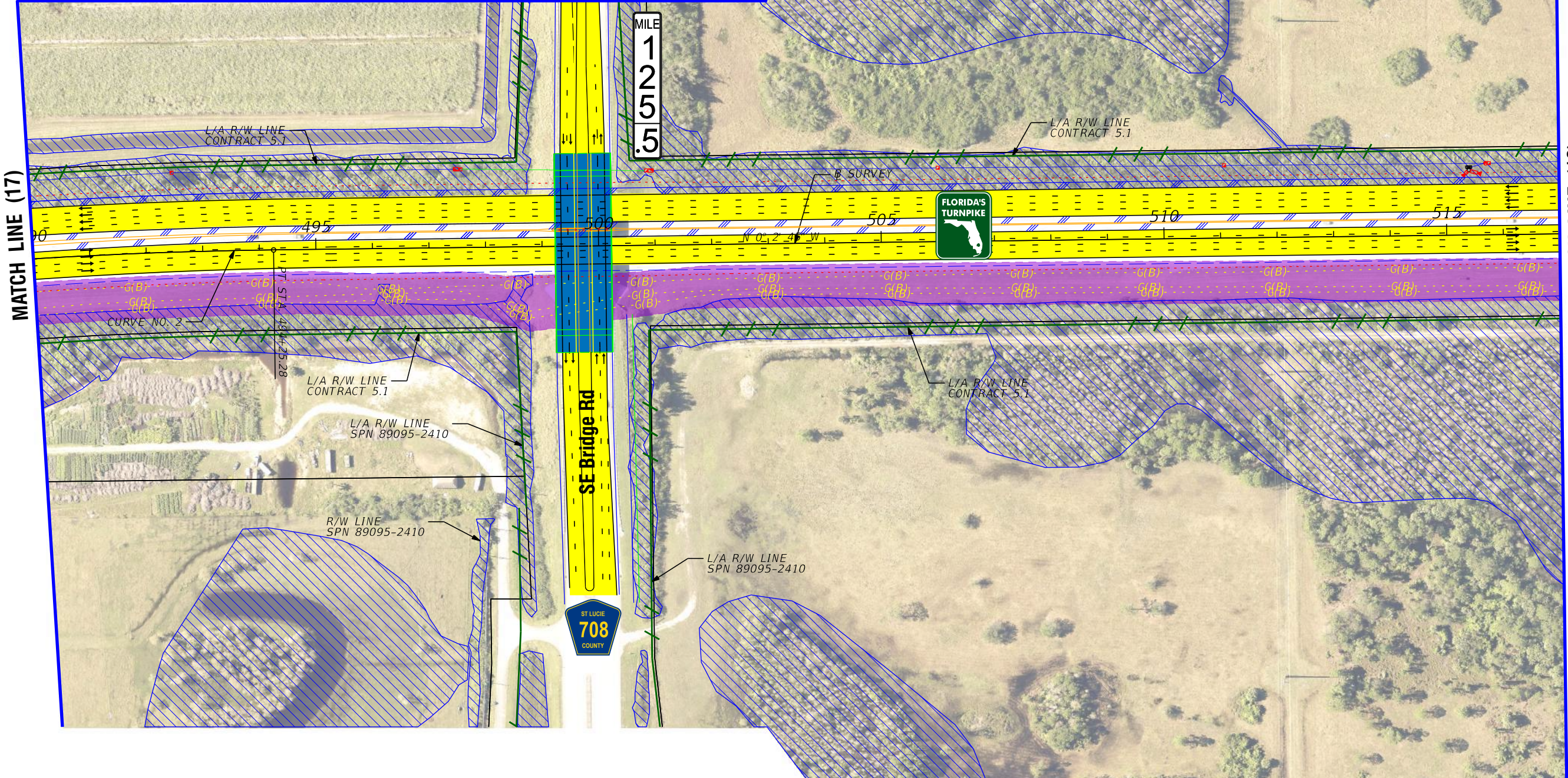




MATCH LINE (18A)

MATCH LINE (17)

MATCH LINE (19)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

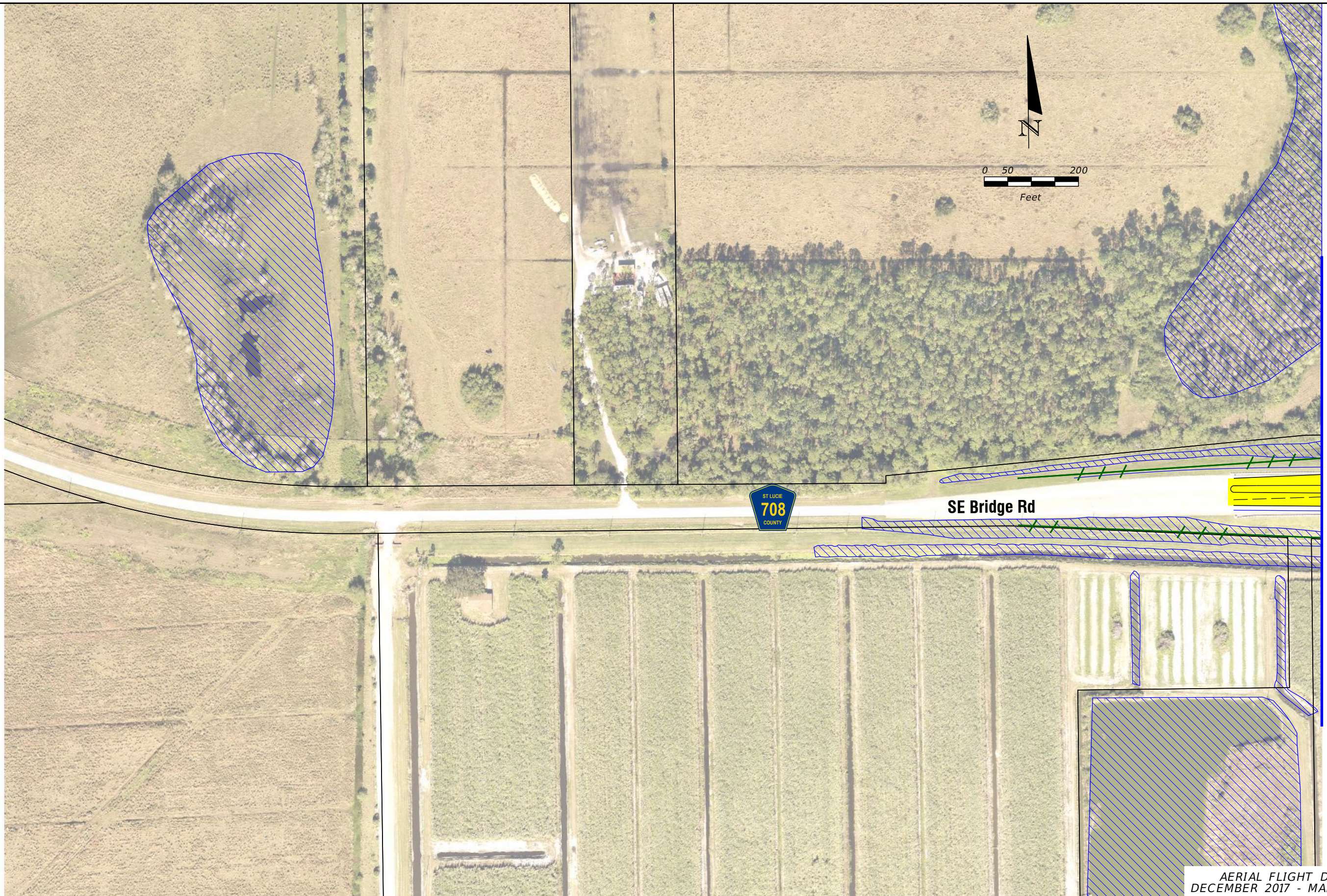
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**18**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
	—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
	—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—	
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

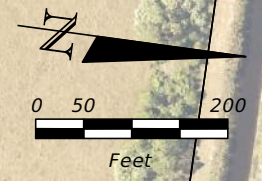
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**18A**

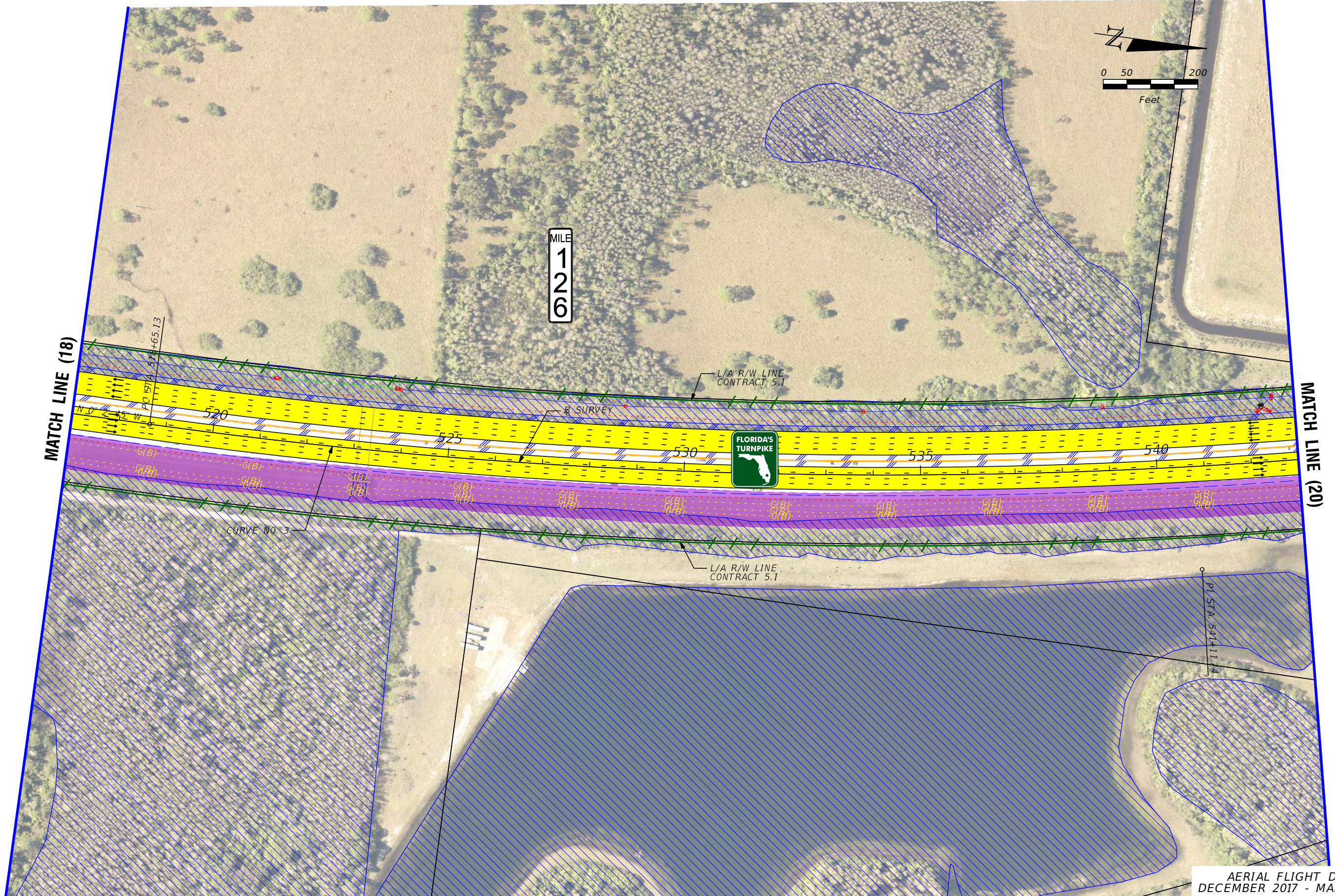




MILE  
1  
2  
6

MATCH LINE (18)

MATCH LINE (20)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

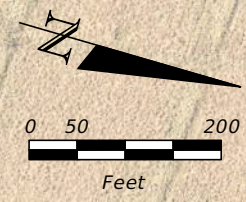
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
19

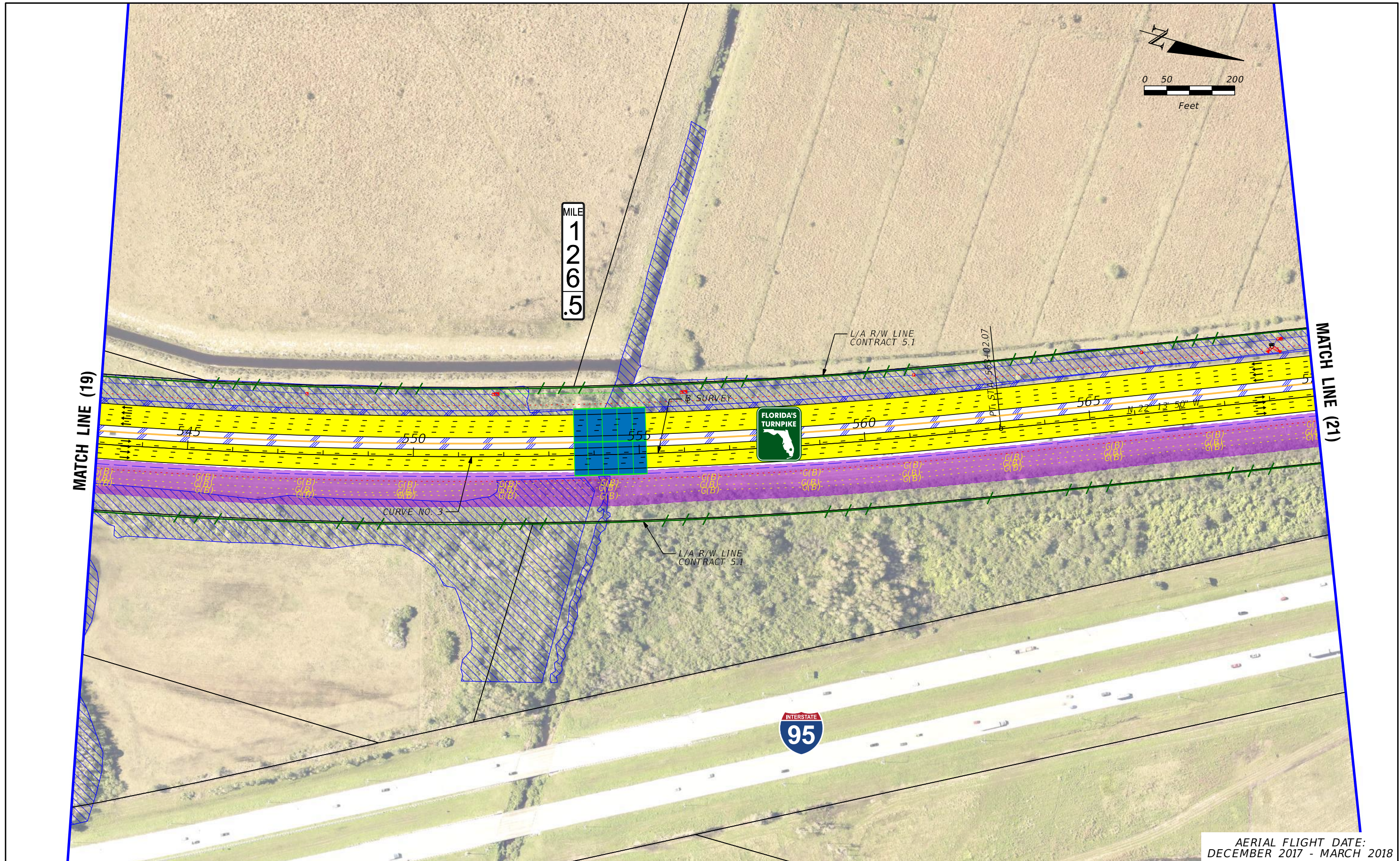




MILE  
1  
2  
6  
.5

MATCH LINE (19)

MATCH LINE (21)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

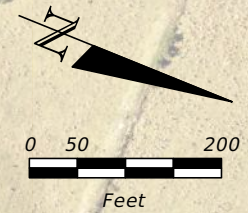
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
20





MILE  
1  
2  
7

L/A R/W LINE  
CONTRACT 5.1

① SURVEY



MATCH LINE (20)

MATCH LINE (22)

AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

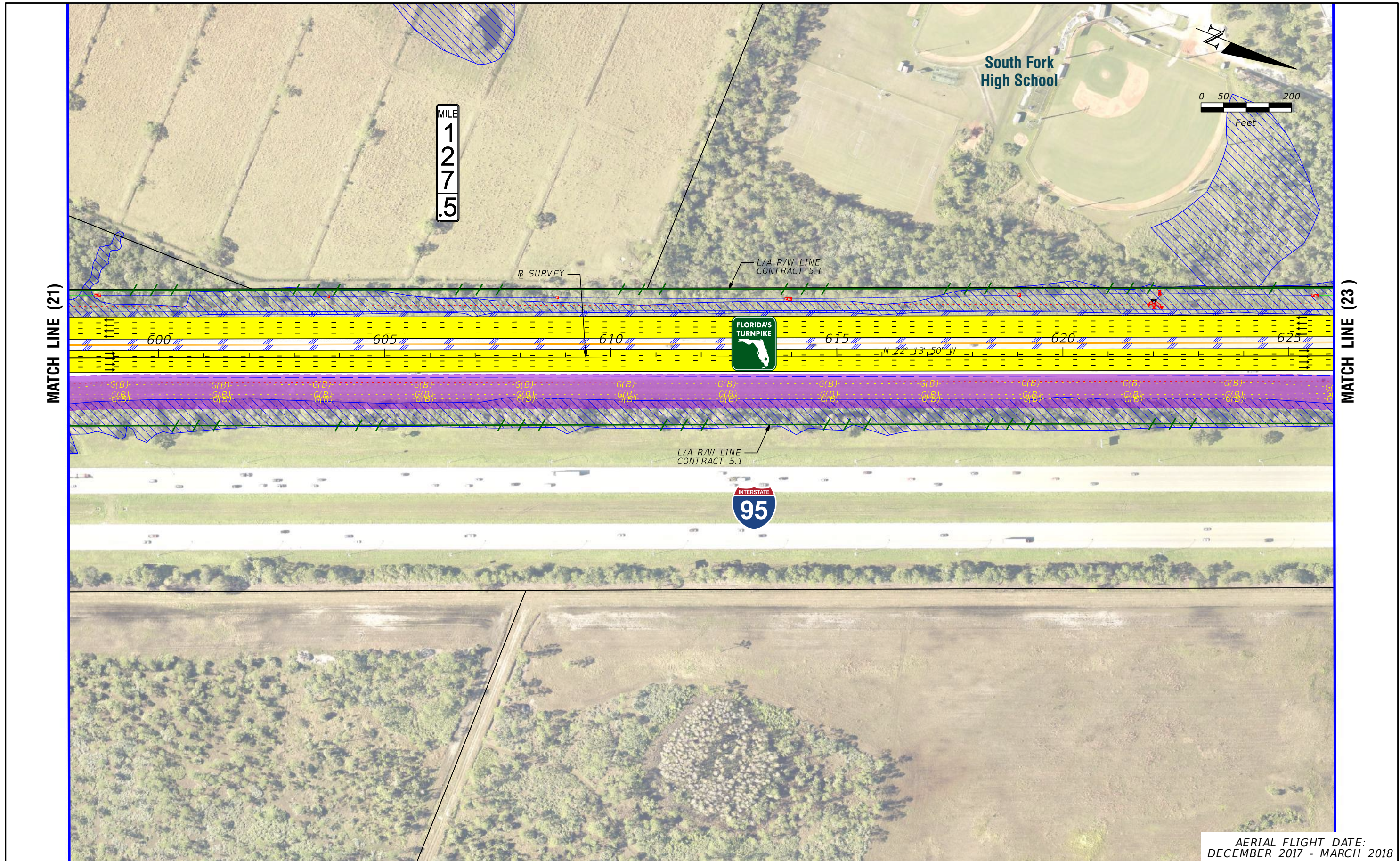
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
21





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

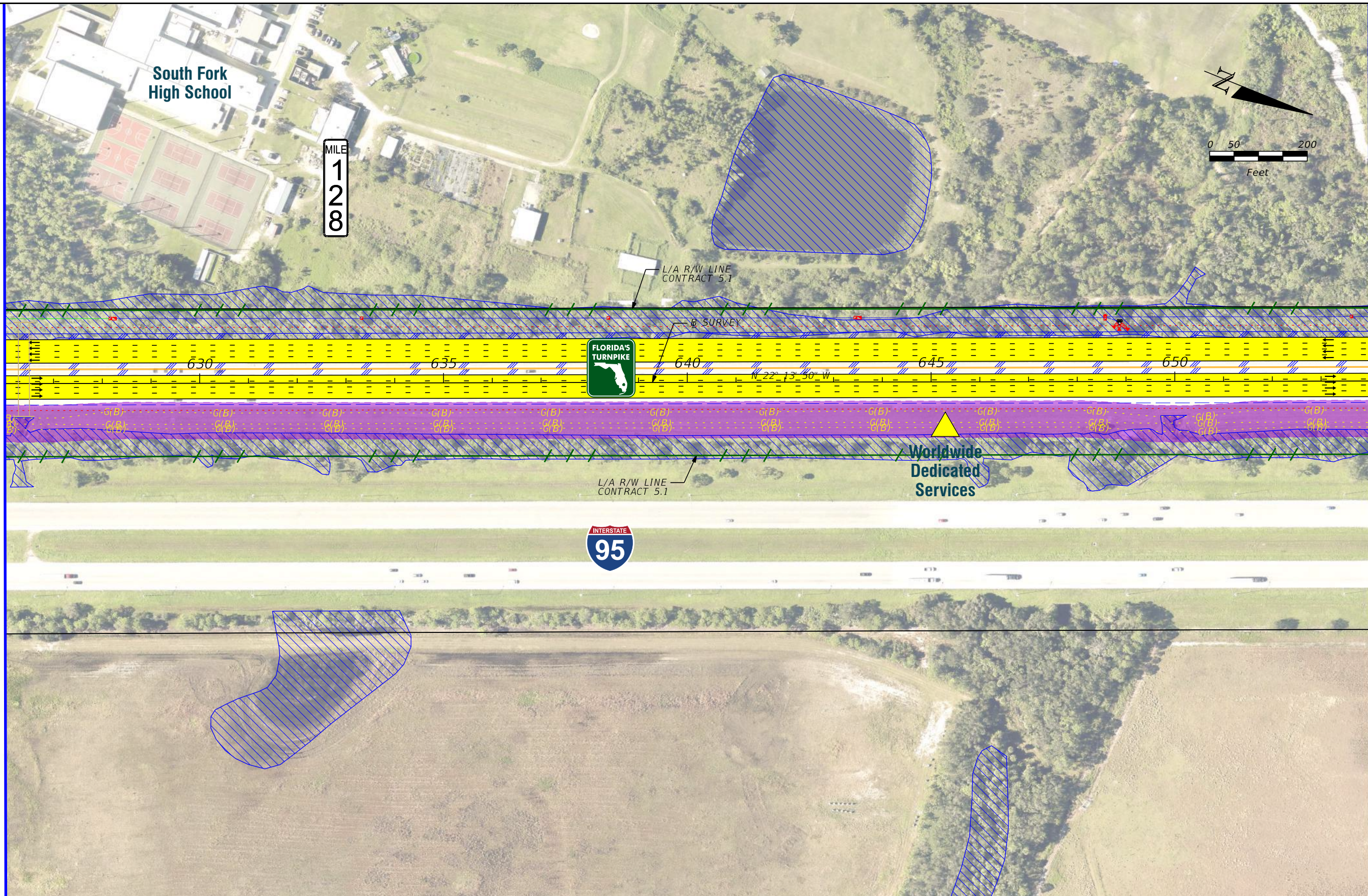
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
22





MATCH LINE (22)

MATCH LINE (24)

South Fork High School

MILE 128



Worldwide Dedicated Services

AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

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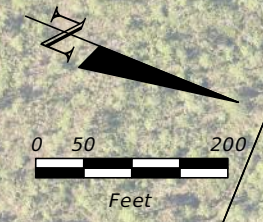
FLORIDA'S TURNPIKE (SR 91) WIDENING  
FROM JUPITER (INDIANTOWN ROAD/SR 706)  
TO FT. PIERCE (OKEECHOBEE ROAD/SR 70)  
PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY  
FINANCIAL PROJECT IDENTIFICATION (FPID) NUMBER: 423374-1

**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**23**

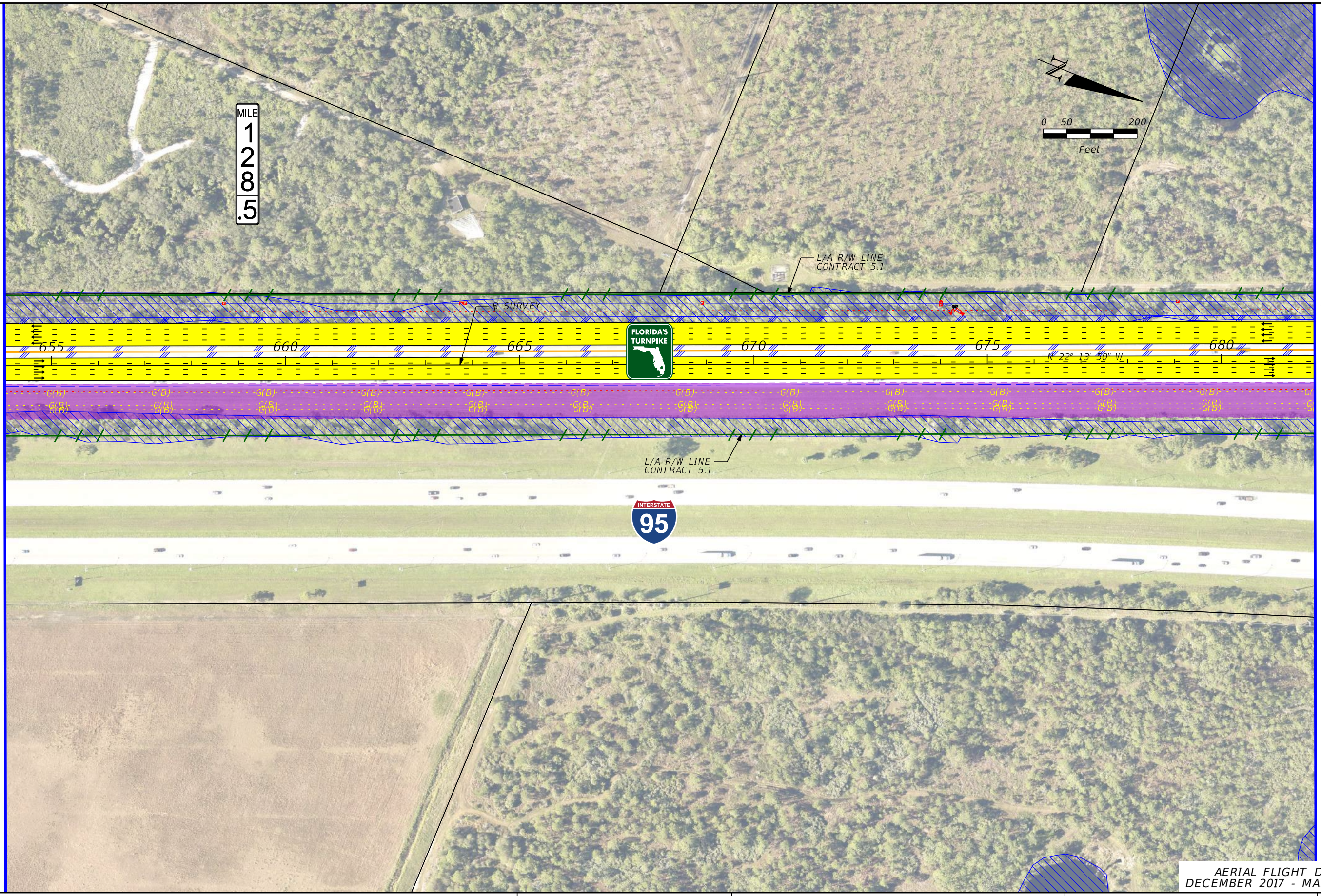


MILE  
1  
2  
8  
.5



MATCH LINE (23)

MATCH LINE (25)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

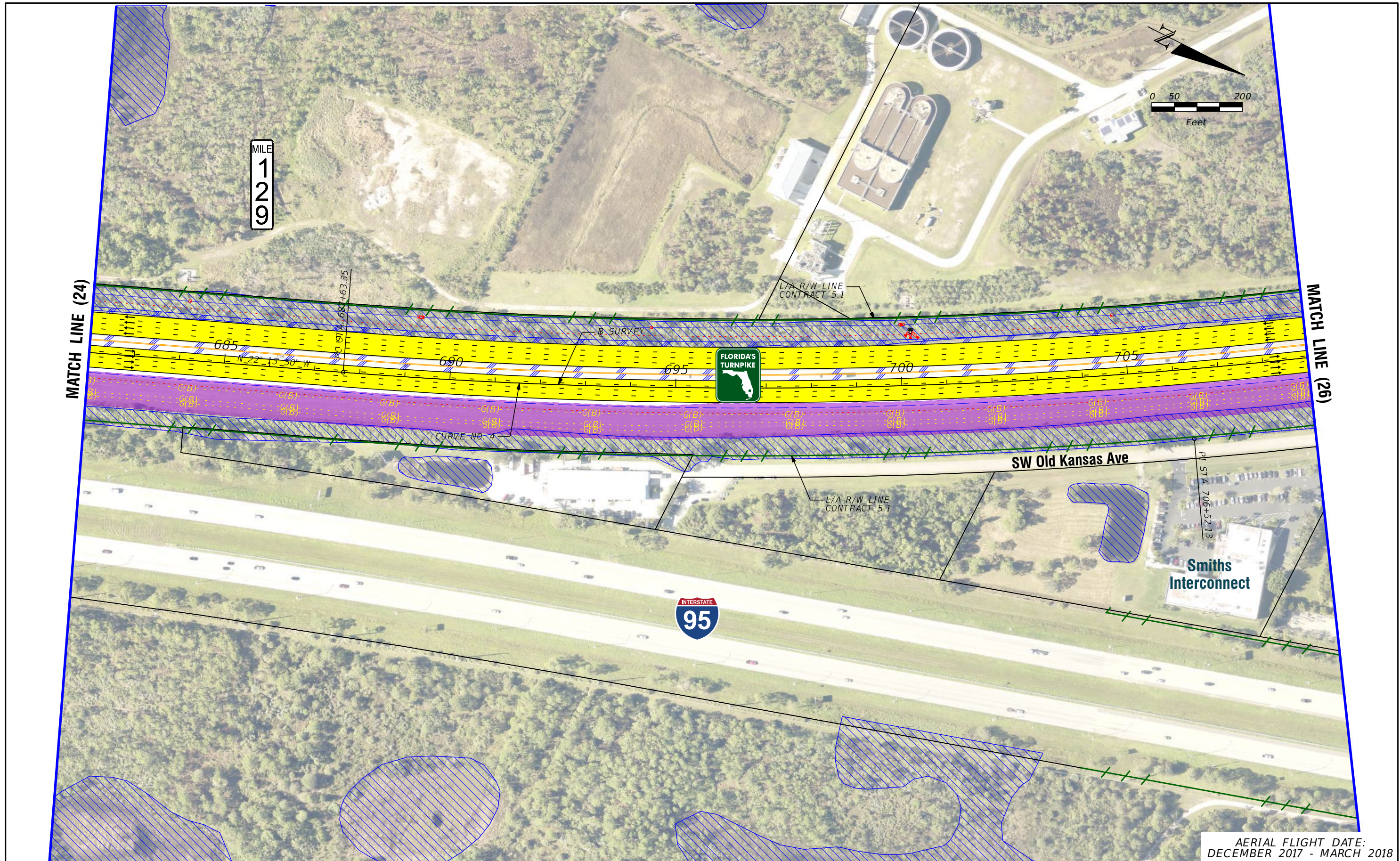
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
24





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

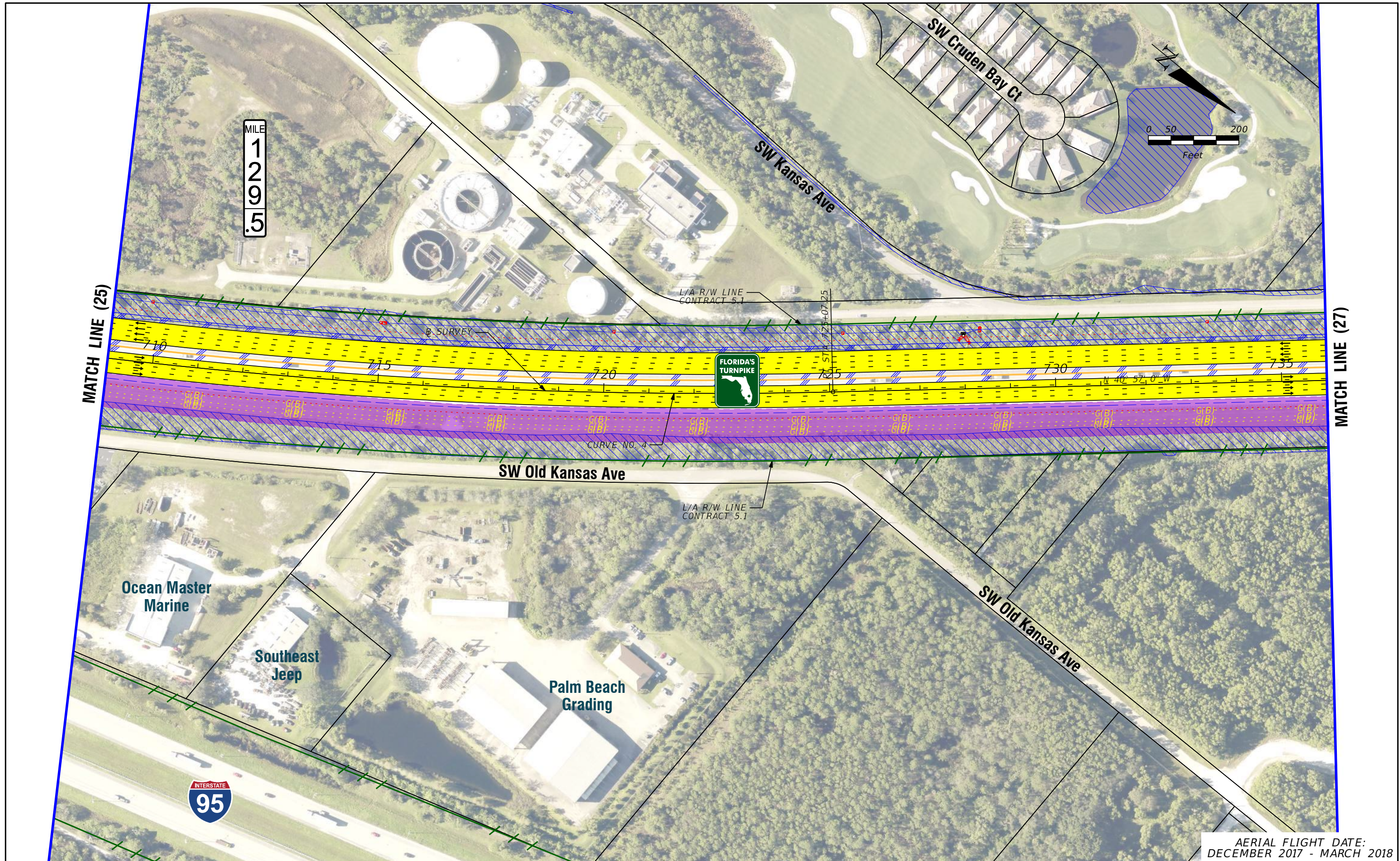
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**25**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	NOTE: ROW = RIGHT OF WAY	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)		—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)		—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH		—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	ⓑ	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	ⓓ			
—	RAILROAD								

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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

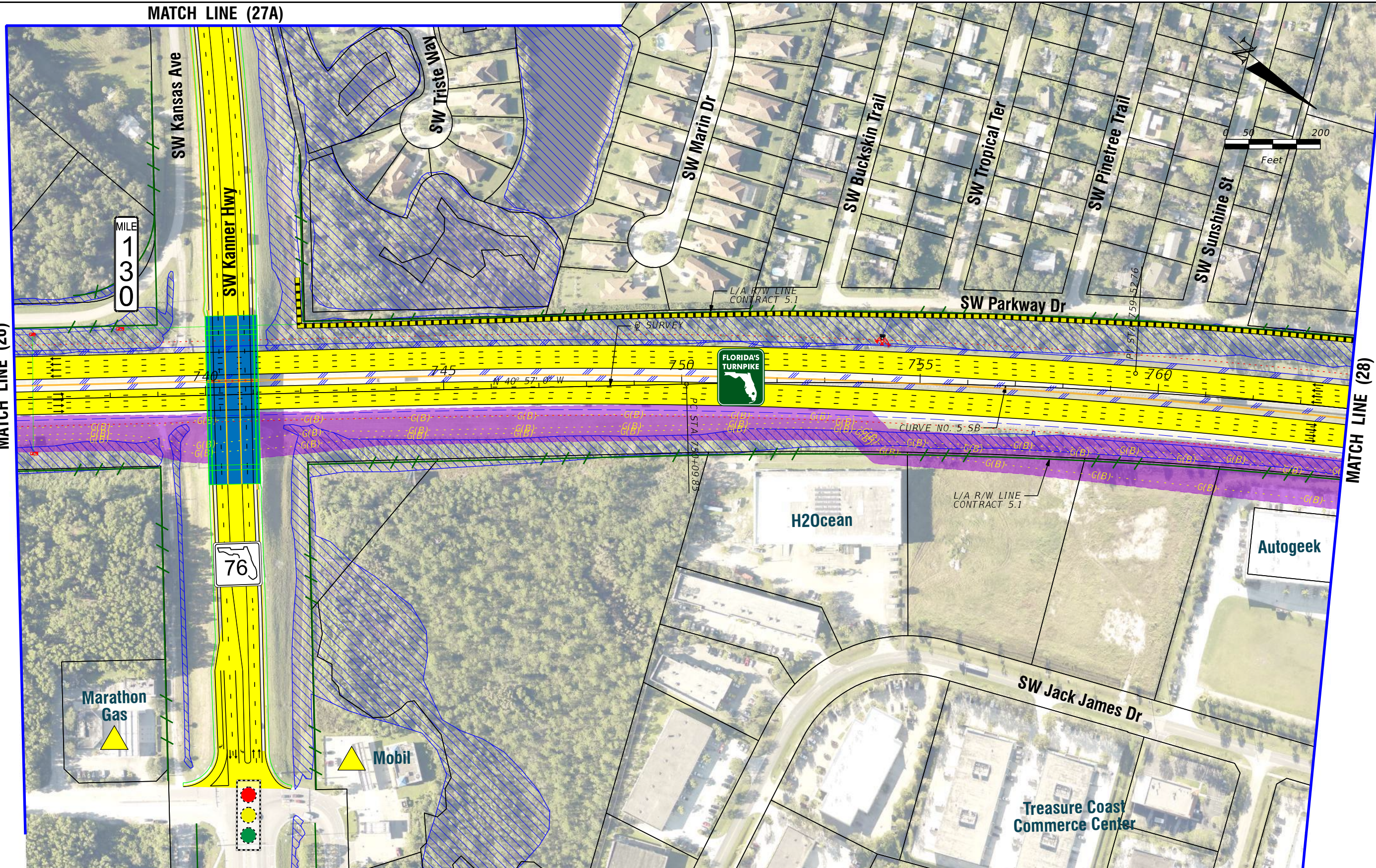
SHEET NO.  
26



MATCH LINE (27A)

MATCH LINE (26)

MATCH LINE (28)



MILE  
1  
3  
0

76

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

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PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
27

AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

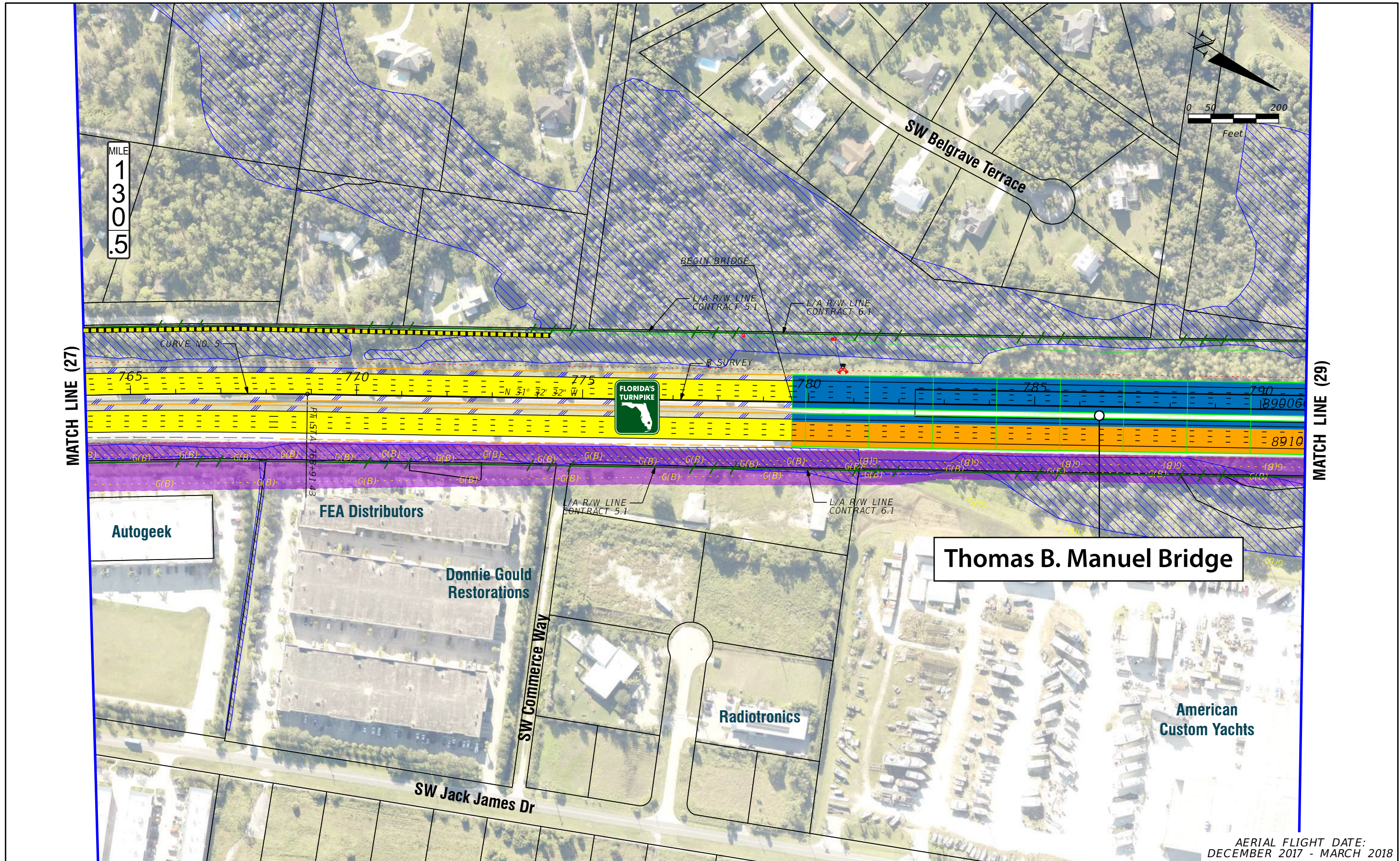
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**27A**





LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

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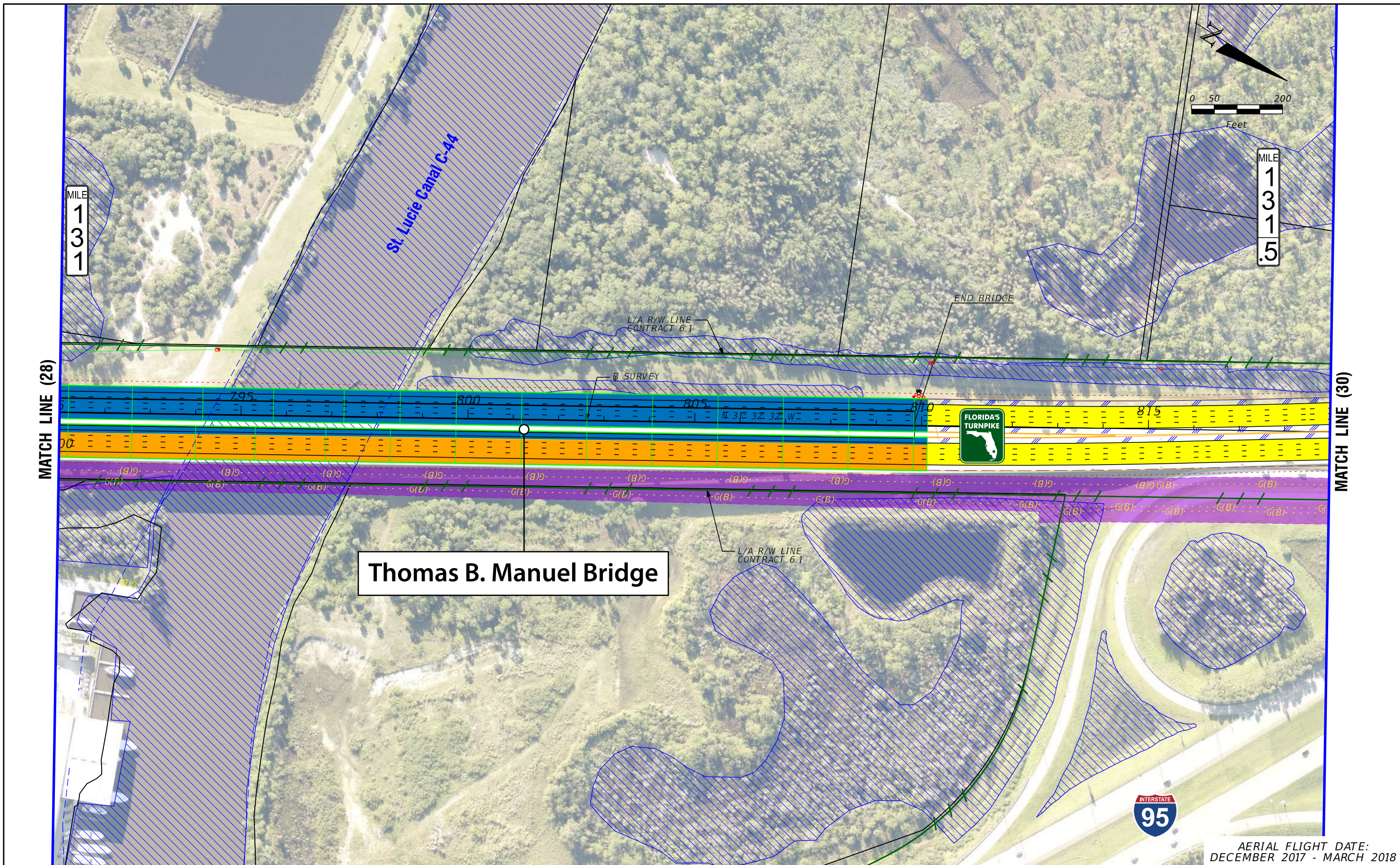
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**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

SHEET NO.  
 28

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018





**Thomas B. Manuel Bridge**

LEGEND

—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION		
—	RAILROAD						

NOTE: ROW = RIGHT OF WAY

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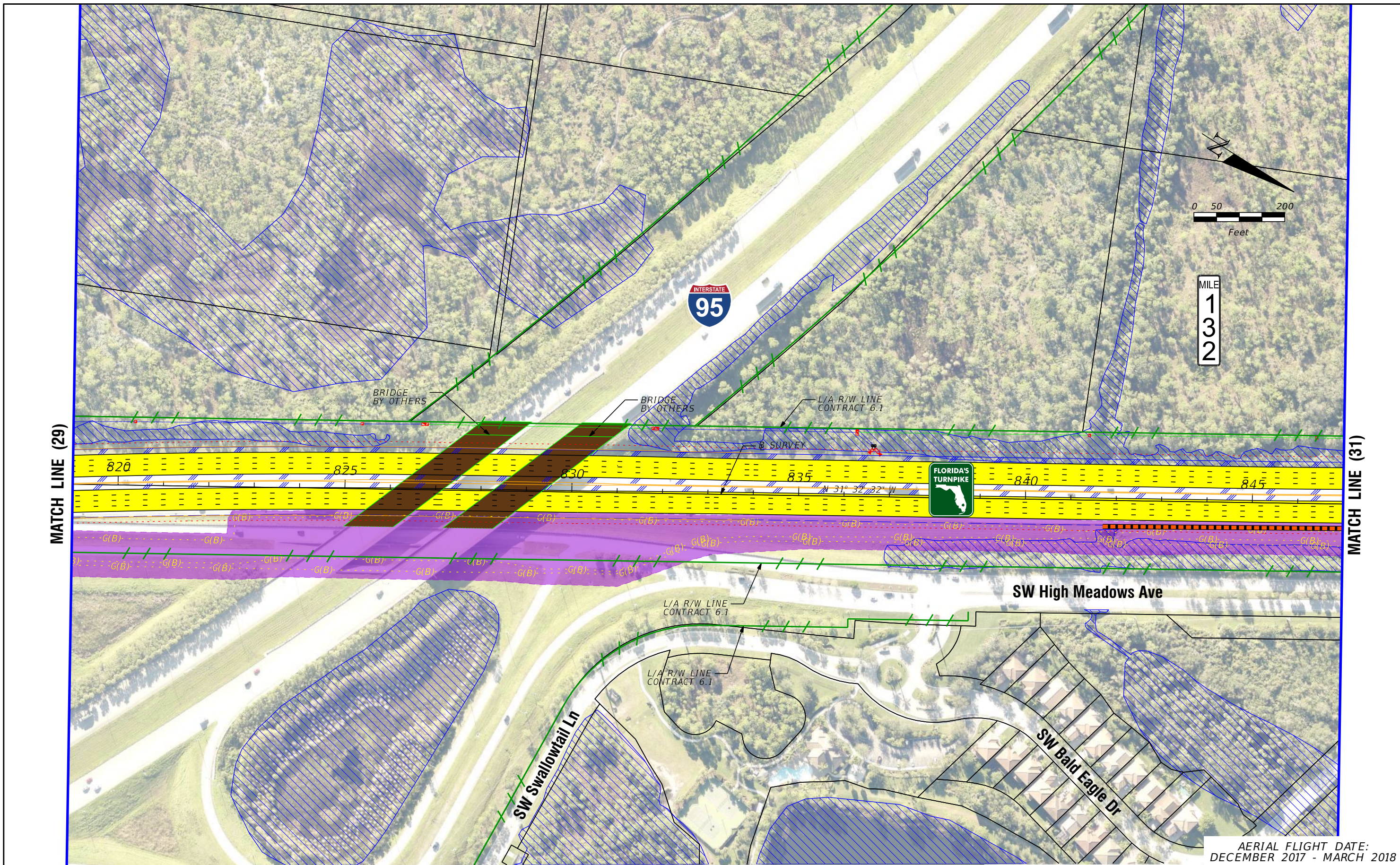
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**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

SHEET NO.  
 29

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

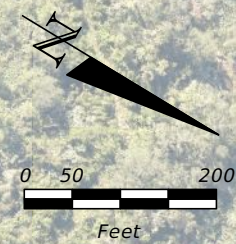
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**30**





MILE  
1  
3  
2  
.5

Mapps Creek

EXISTING  
TOLL  
GANTRY

MATCH LINE (30)

MATCH LINE (32)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	—	
—	RAILROAD	—	—	

NOTE: ROW = RIGHT OF WAY

—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE
—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)
—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)
—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION
—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION

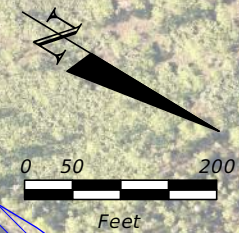
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET  
NO.  
**31**

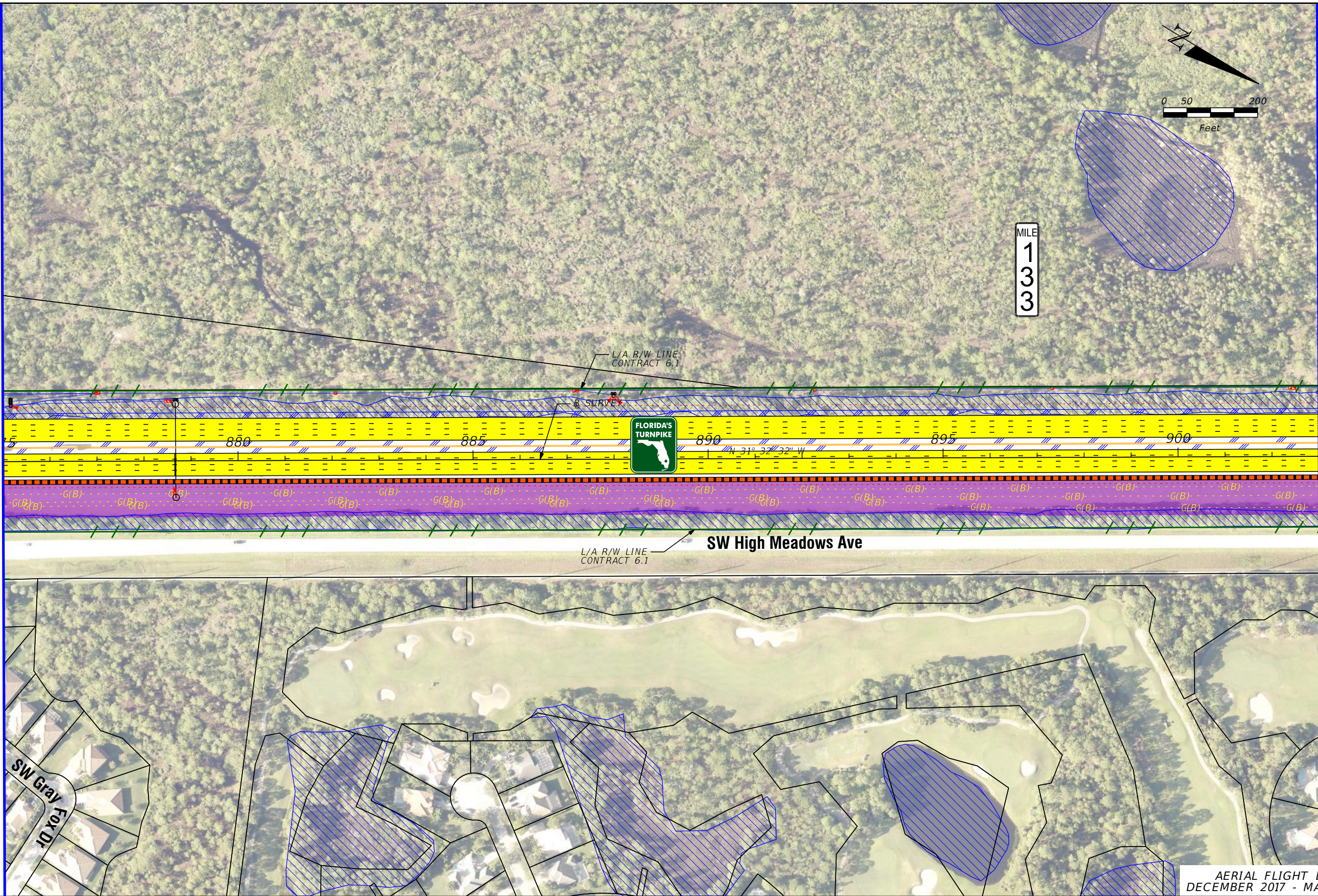




MILE  
1  
3  
3

MATCH LINE (31)

MATCH LINE (33)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD		
—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE
—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)
—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)
—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION
—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION

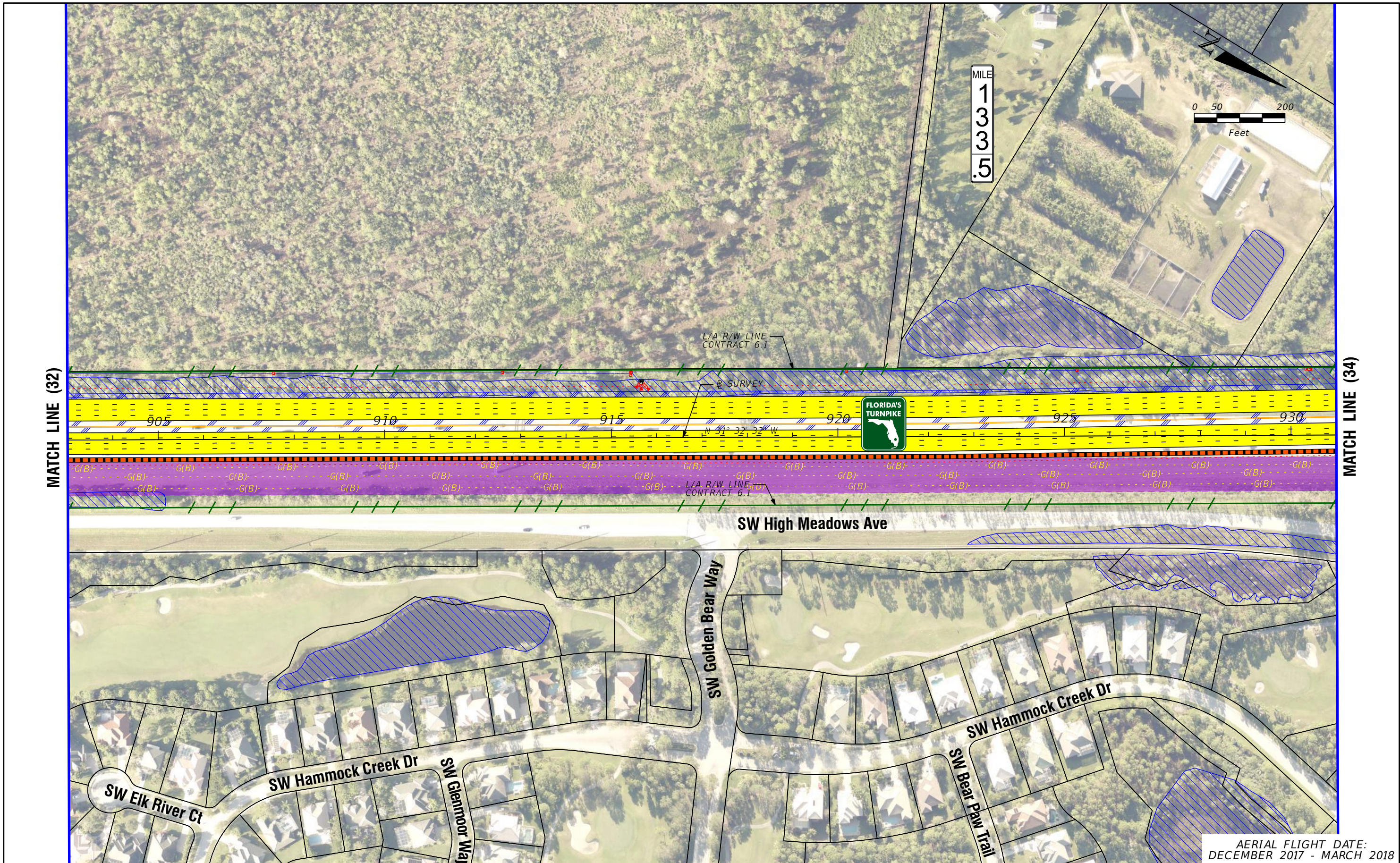
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET  
NO.  
32





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

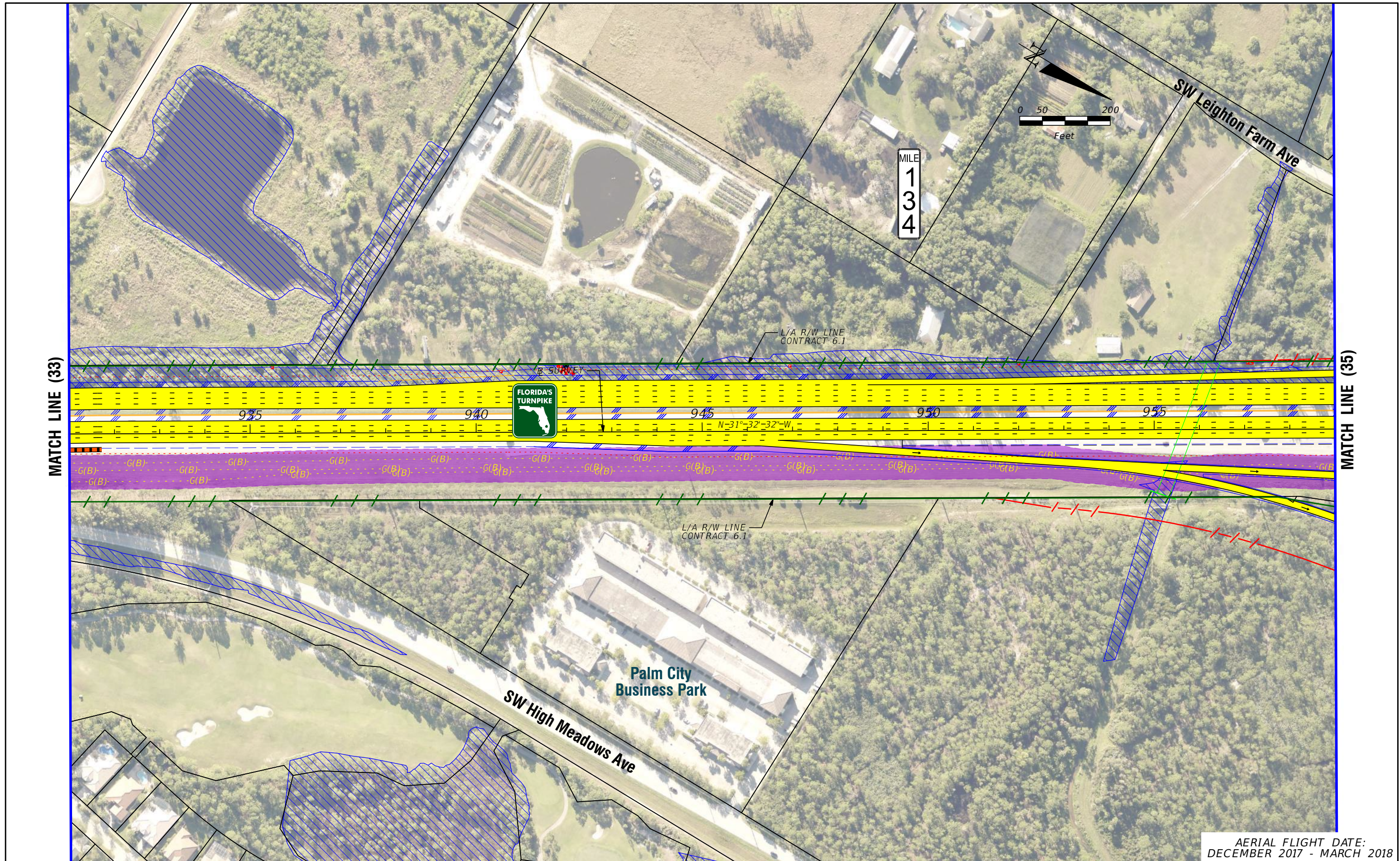
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**33**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

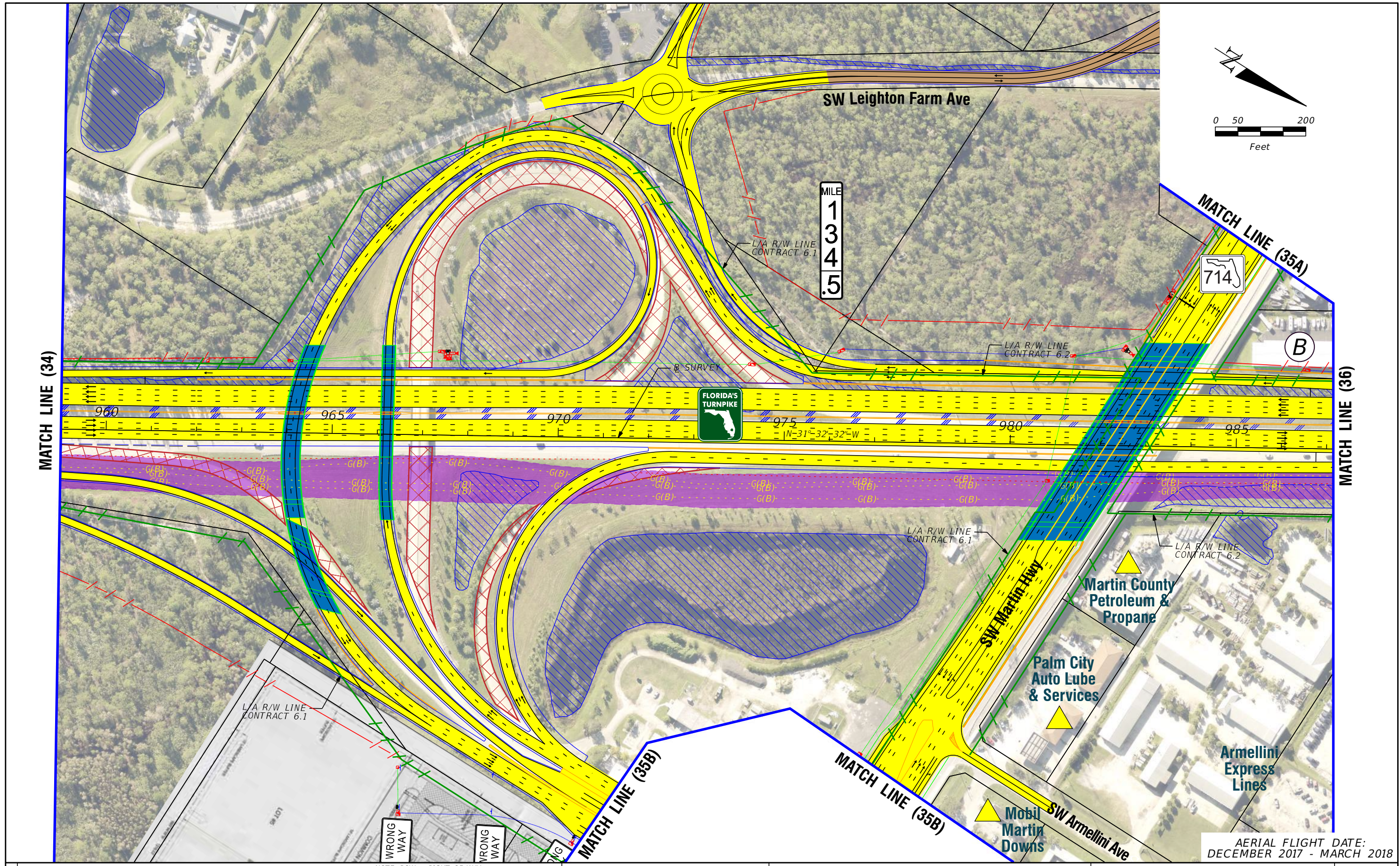
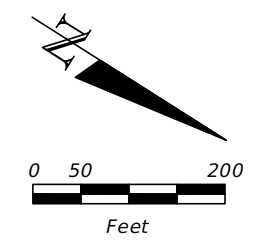
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**34**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

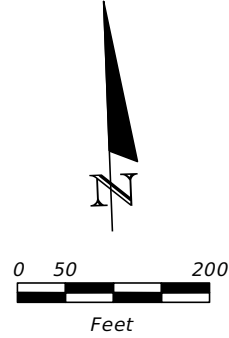
H. W. LOCHNER, INC.  
4350 W. CYPRESS ST., SUITE 800  
TAMPA, FLORIDA 33607  
FBPR CERTIFICATE OF AUTH. #894

FLORIDA'S TURNPIKE (SR 91) WIDENING  
FROM JUPITER (INDIANTOWN ROAD/SR 706)  
TO FT. PIERCE (OKEECHOBEE ROAD/SR 70)  
PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY  
FINANCIAL PROJECT IDENTIFICATION (FPID) NUMBER: 423374-1

**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**35**





**LEGEND**

—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—	
—	RAILROAD						

NOTE: ROW = RIGHT OF WAY

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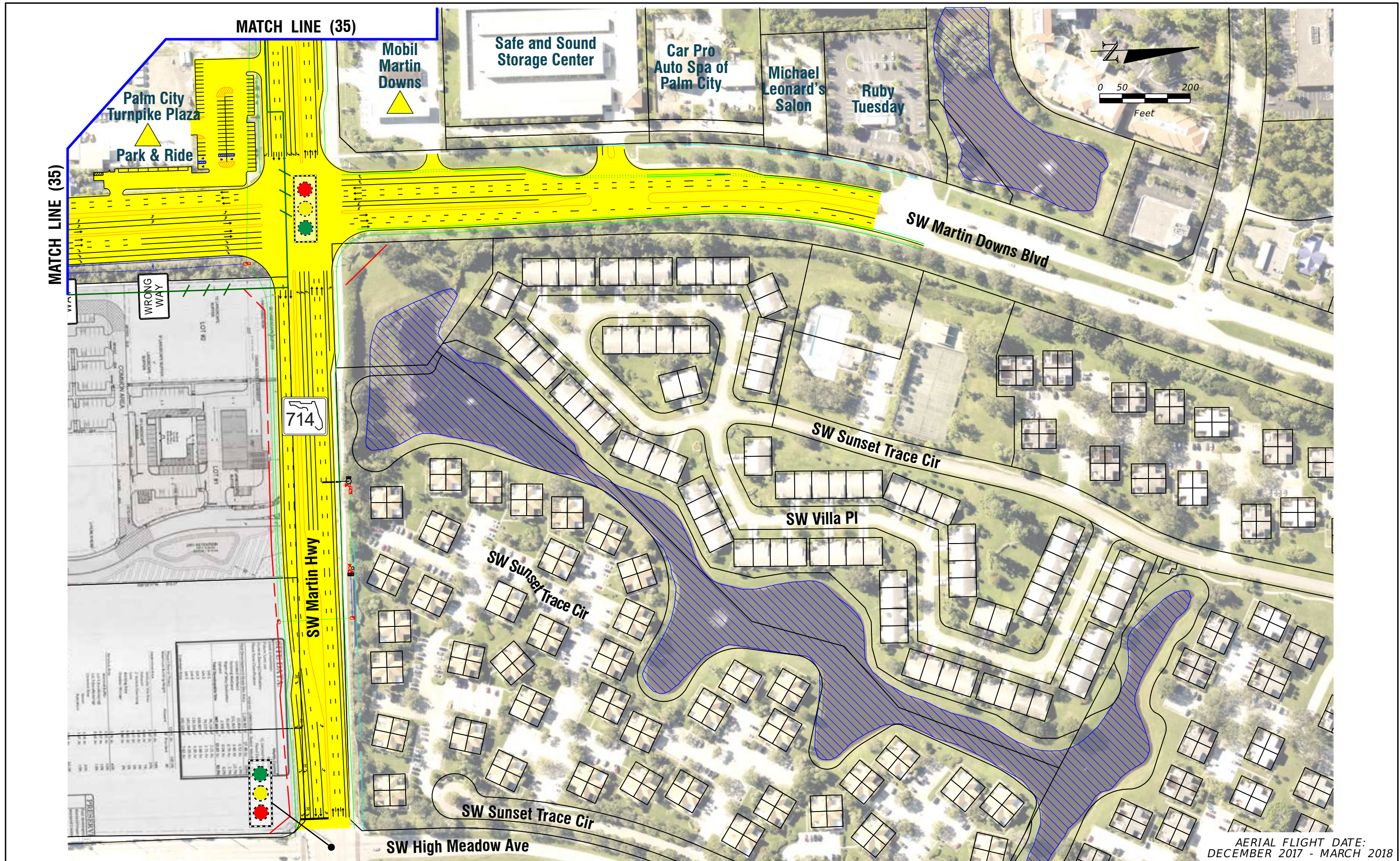
**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

SHEET NO.  
**35A**

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018

MATCH LINE (35)





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	—	
—	RAILROAD	—	—	

NOTE: ROW = RIGHT OF WAY

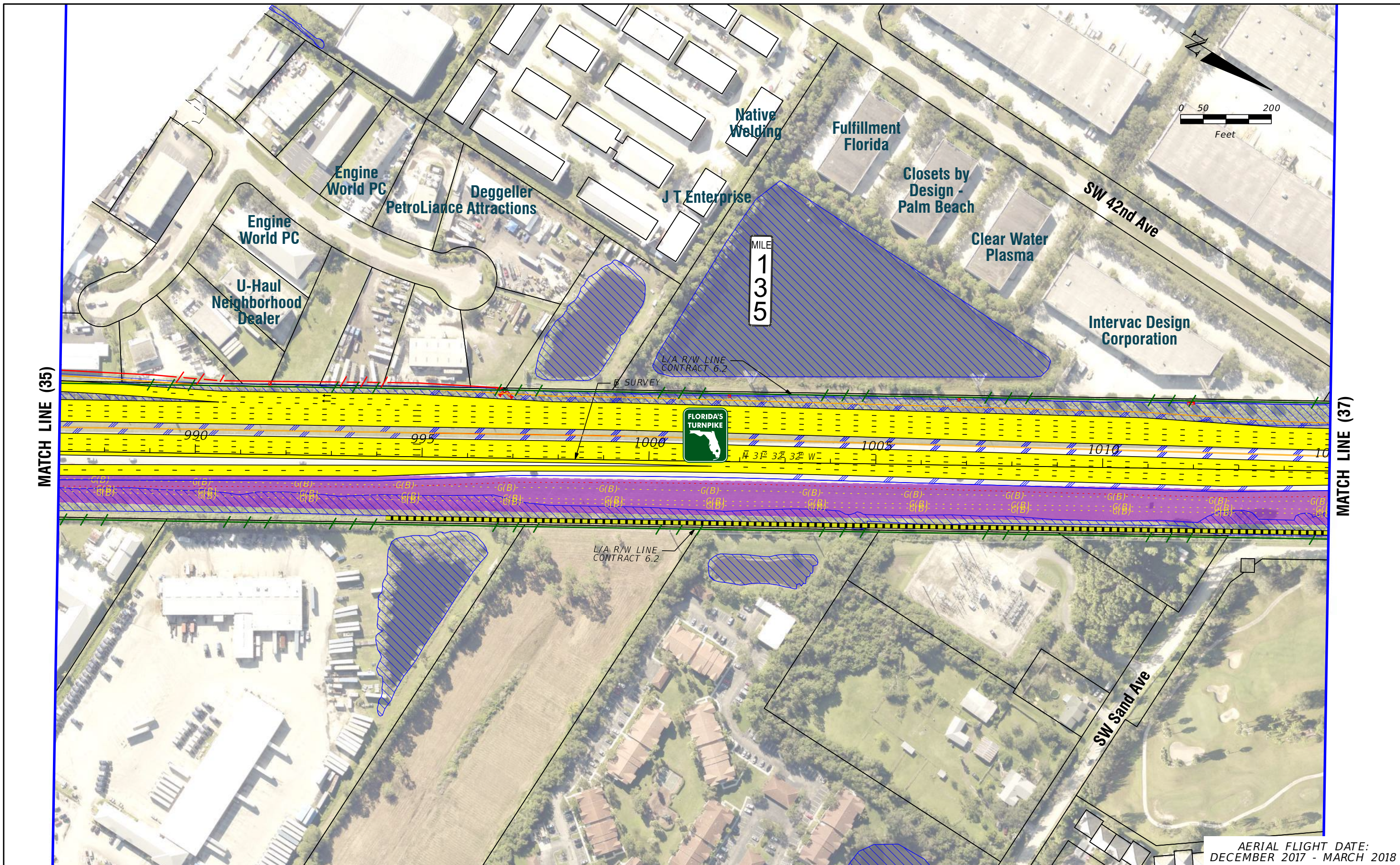
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**35B**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	—	
—	RAILROAD	—	—	

NOTE: ROW = RIGHT OF WAY

—	EXISTING BRIDGE
—	OTHER PROJECTS (BRIDGE)
—	OTHER PROJECTS (ROADS)
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
(B)	POTENTIAL BUSINESS RELOCATION
(R)	POTENTIAL RESIDENTIAL RELOCATION

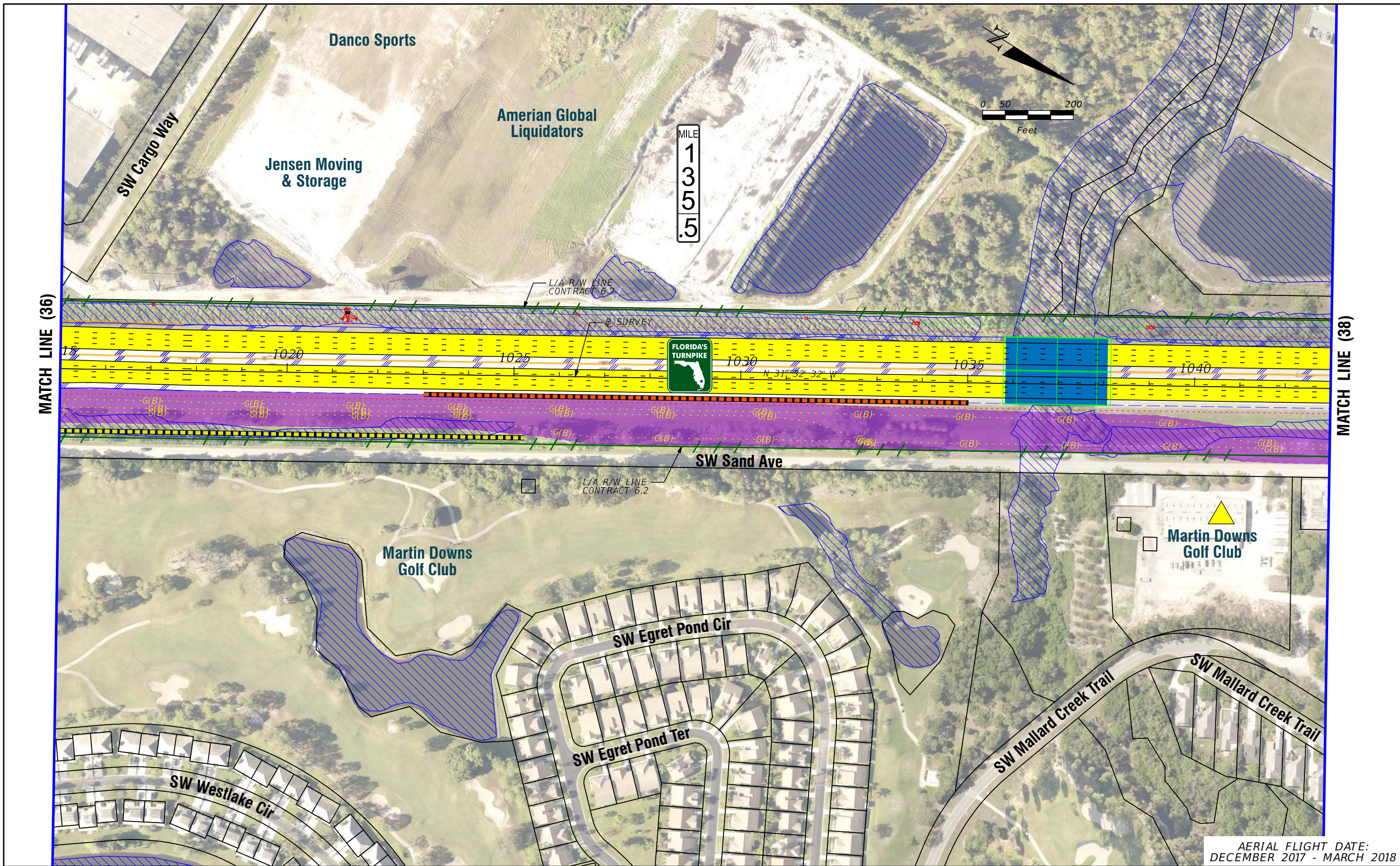
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**36**





LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

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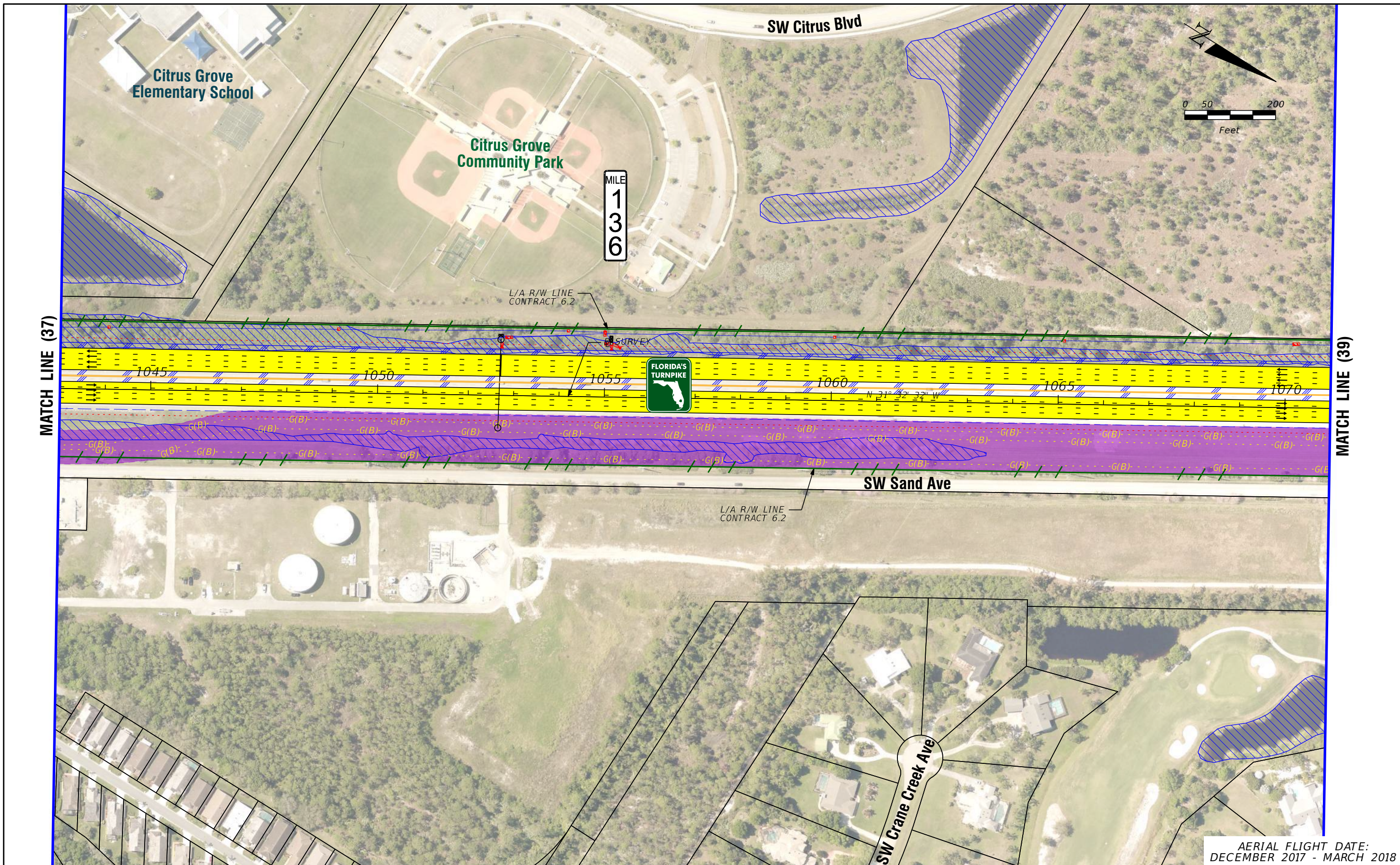
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**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

SHEET NO.  
 37

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018





LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

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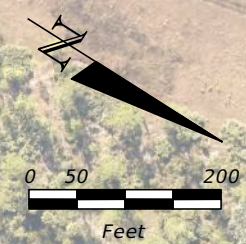
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**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

SHEET NO.  
 38

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018

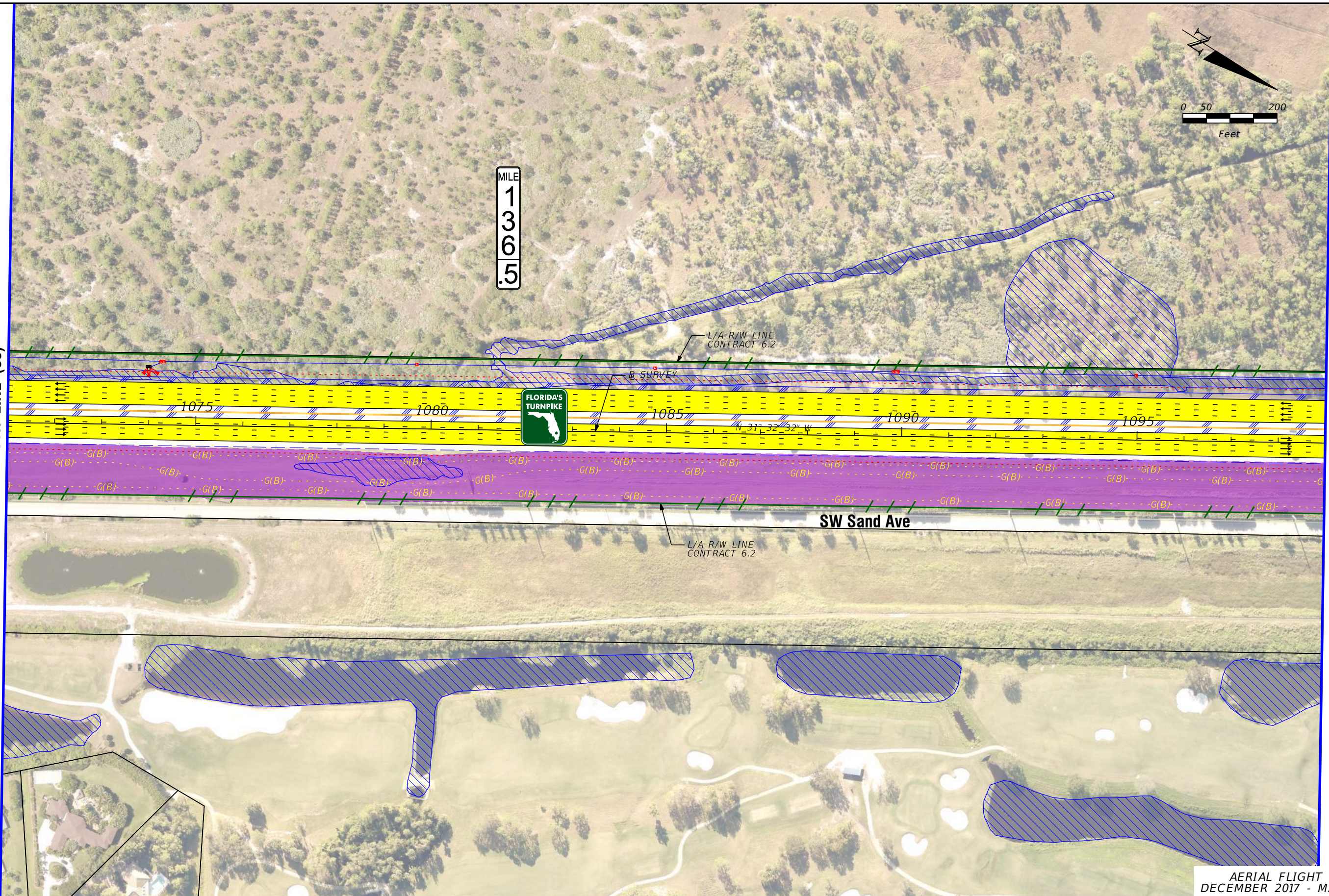




MILE  
1  
3  
6  
.5

MATCH LINE (38)

MATCH LINE (40)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

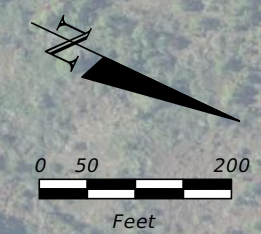
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
39

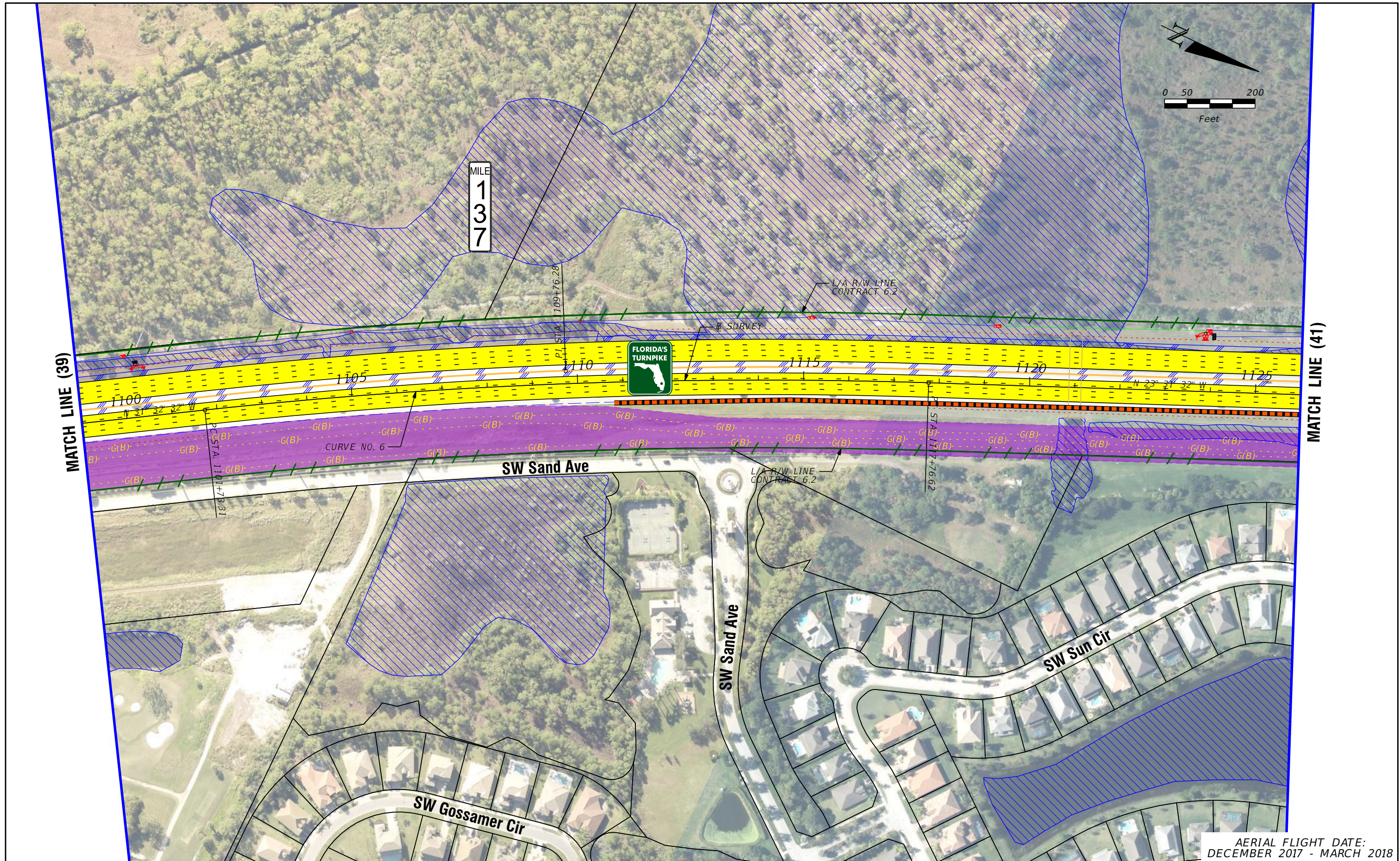




MILE  
137

MATCH LINE (39)

MATCH LINE (41)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	—	
—	RAILROAD	—	—	

NOTE: ROW = RIGHT OF WAY

—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE
—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)
—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)
—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION
—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION

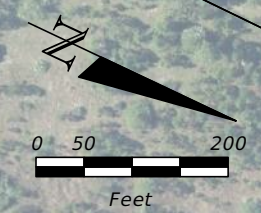
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
40





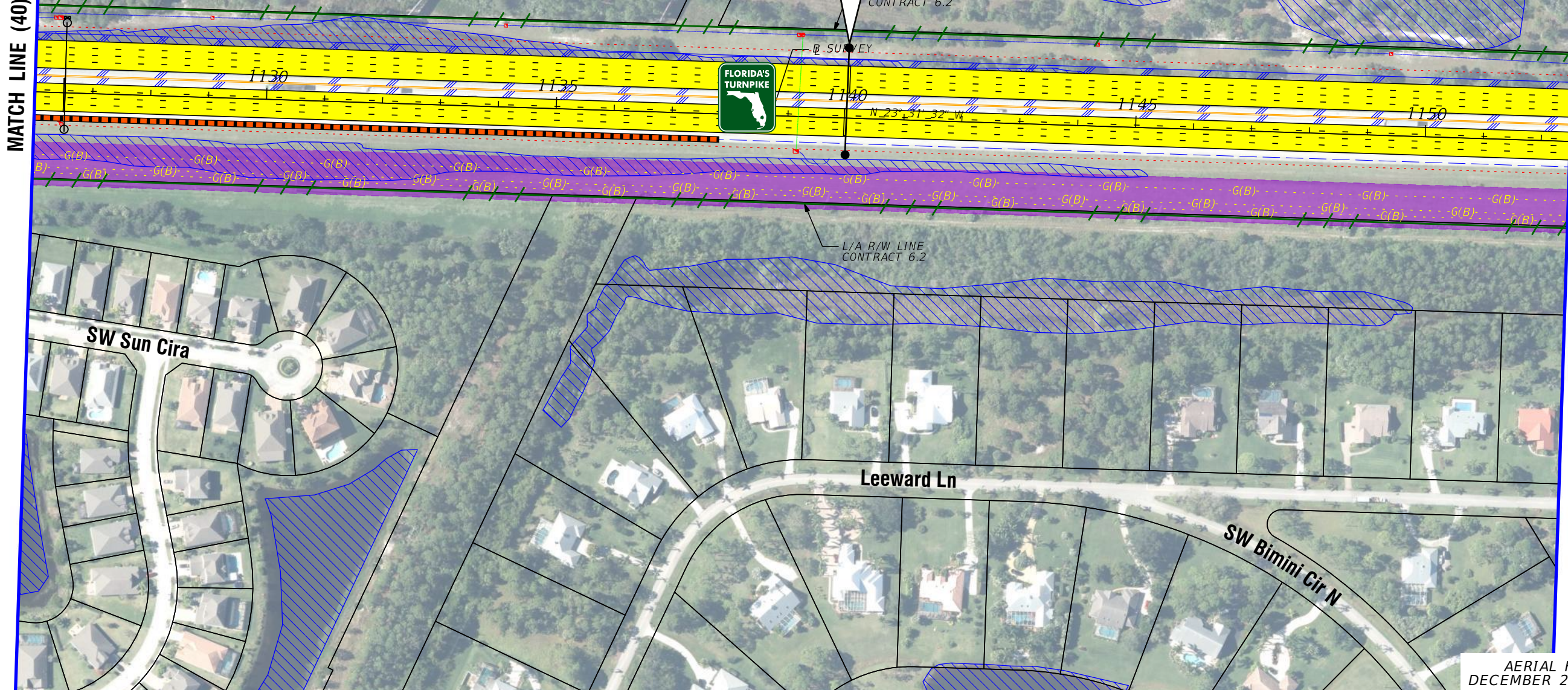
MILE  
1  
3  
7  
.5

EXISTING  
TOLL  
GANTRY



MATCH LINE (40)

MATCH LINE (42)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD		
—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE
—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)
—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)
—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION
—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION

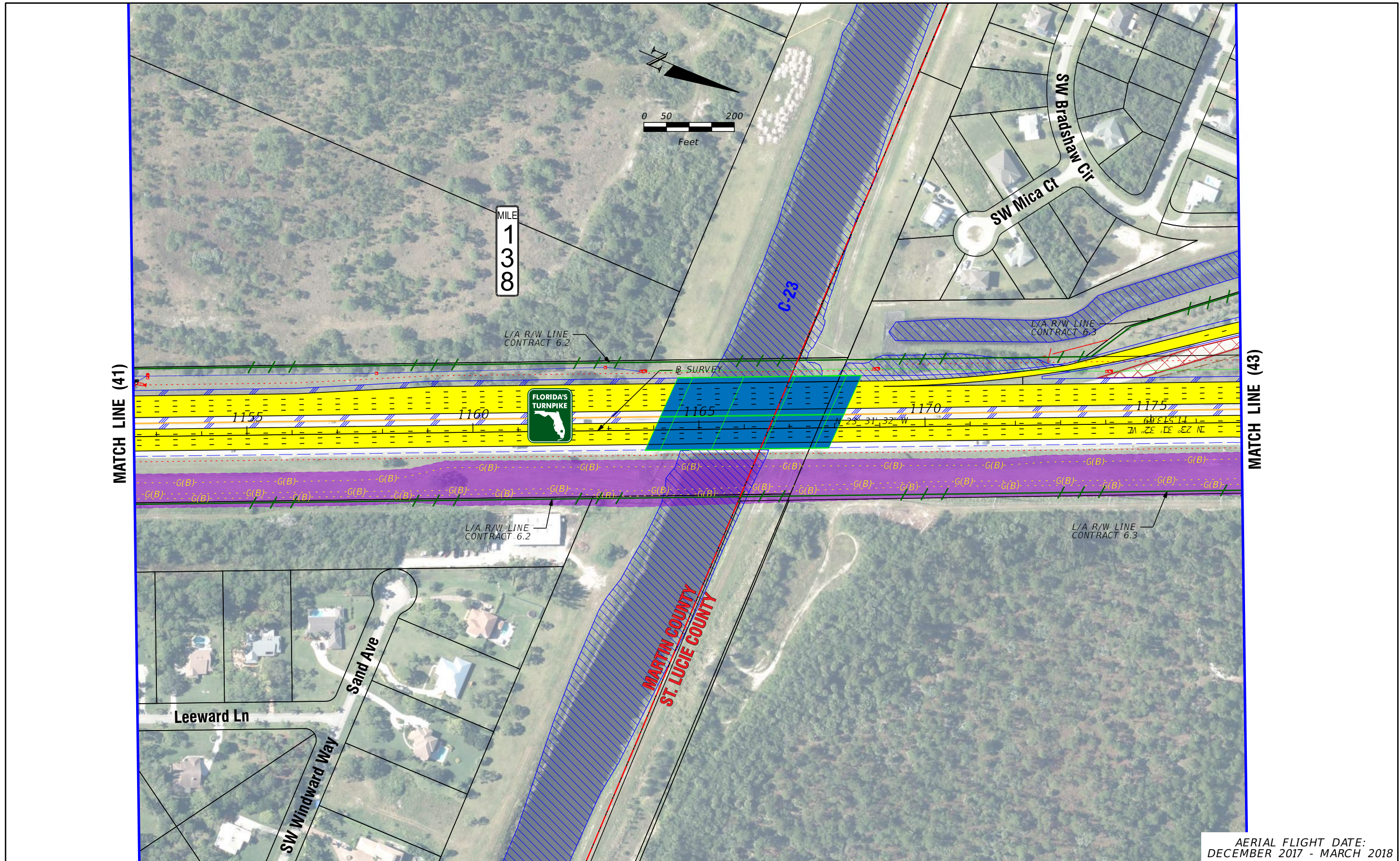
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET  
NO.  
**41**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

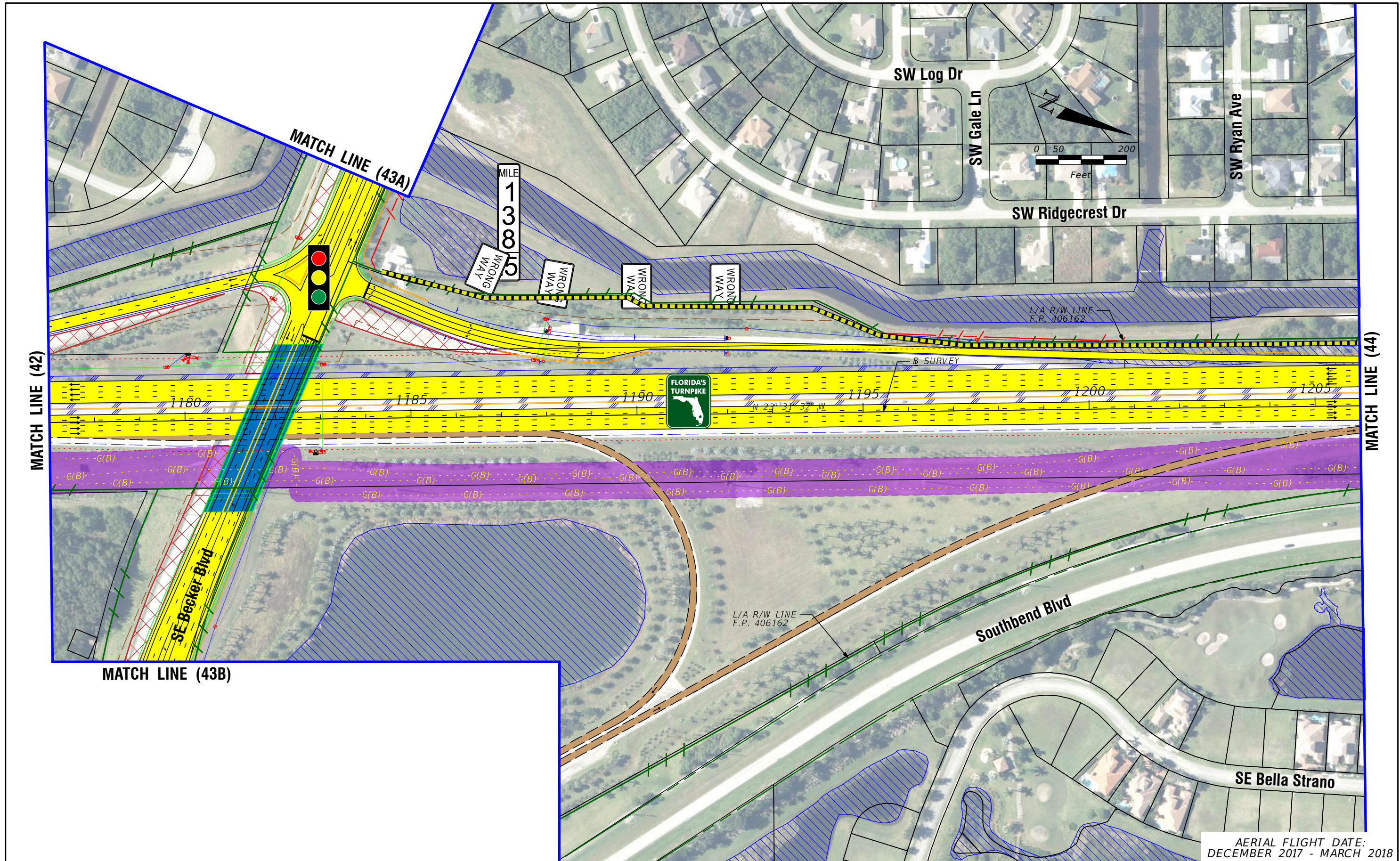
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**42**





LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

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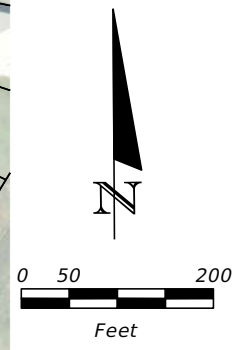
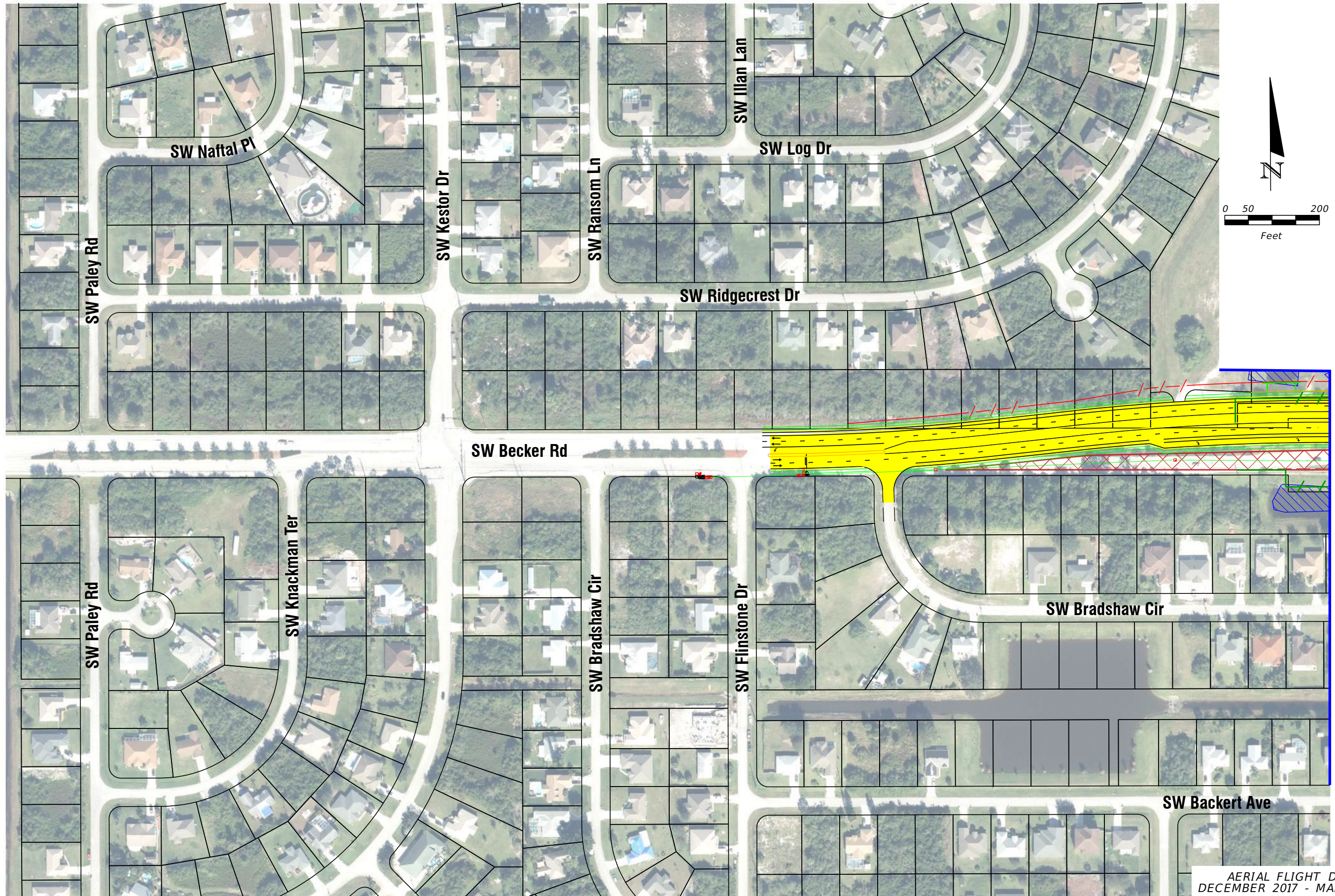
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**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018

SHEET NO.  
 43





MATCH LINE (43)

AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

<b>LEGEND</b>	PROPERTY LINES	PROPOSED ROW NOISE BARRIER	EXISTING BRIDGE	PROPOSED ITS
	TRAFFIC BARRIER WALL	PROPOSED SHOULDER NOISE BARRIER	OTHER PROJECTS (BRIDGE)	POTENTIAL NEW TRAFFIC SIGNAL
	EXISTING ROW LINE	PAVEMENT REMOVAL	OTHER PROJECTS (ROADS)	EXISTING TRAFFIC SIGNAL
	EXISTING LIMITED ACCESS ROW LINE	WETLANDS/OTHER SURFACE WATERS	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	TOLL GANTRY
POTENTIAL ROW LINE	PROPOSED ROADWAY	POTENTIAL BUSINESS RELOCATION	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
POTENTIAL LIMITED ACCESS ROW LINE	PROPOSED BRIDGE	POTENTIAL RESIDENTIAL RELOCATION		
RAILROAD				

NOTE: ROW = RIGHT OF WAY

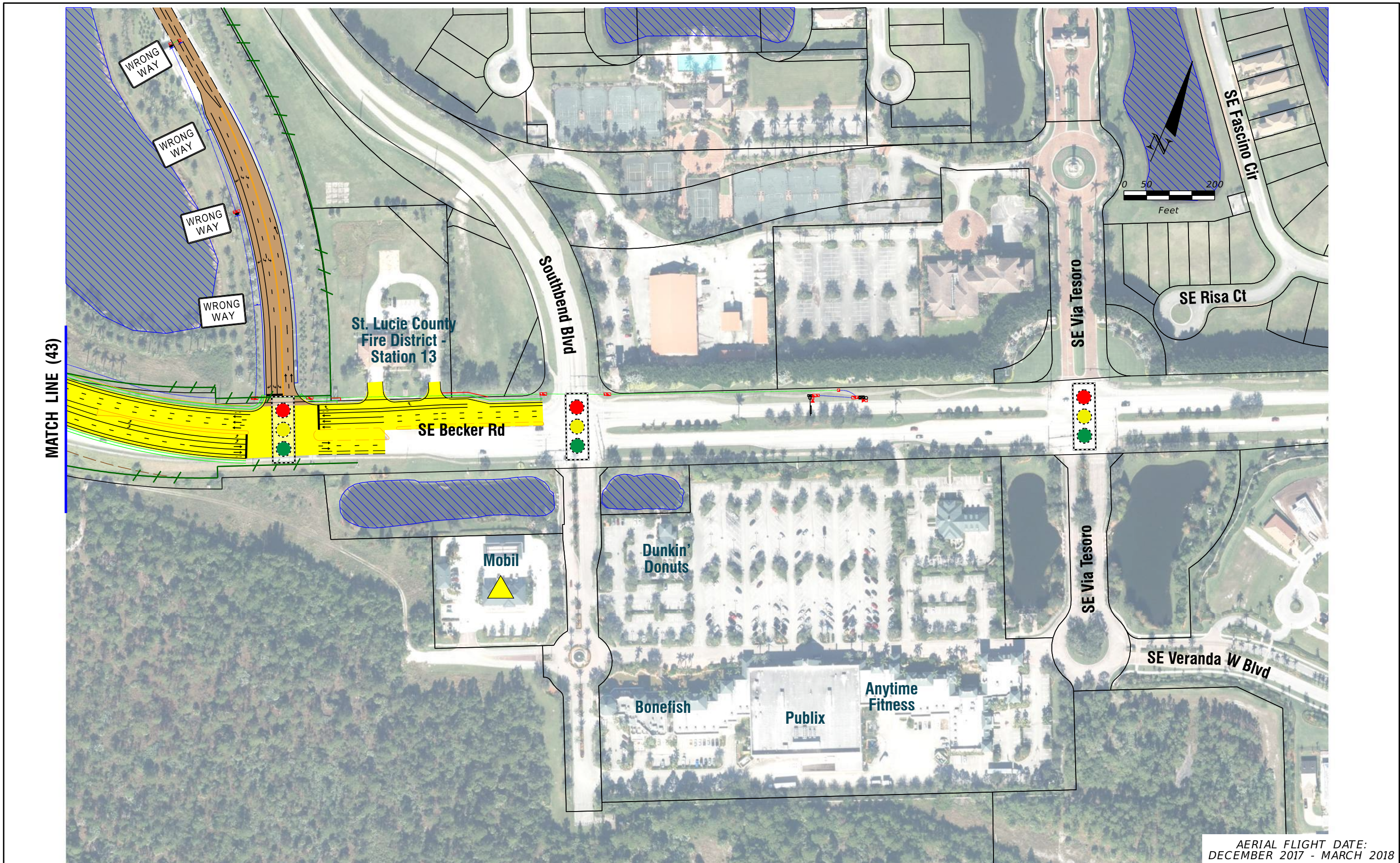
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**43A**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

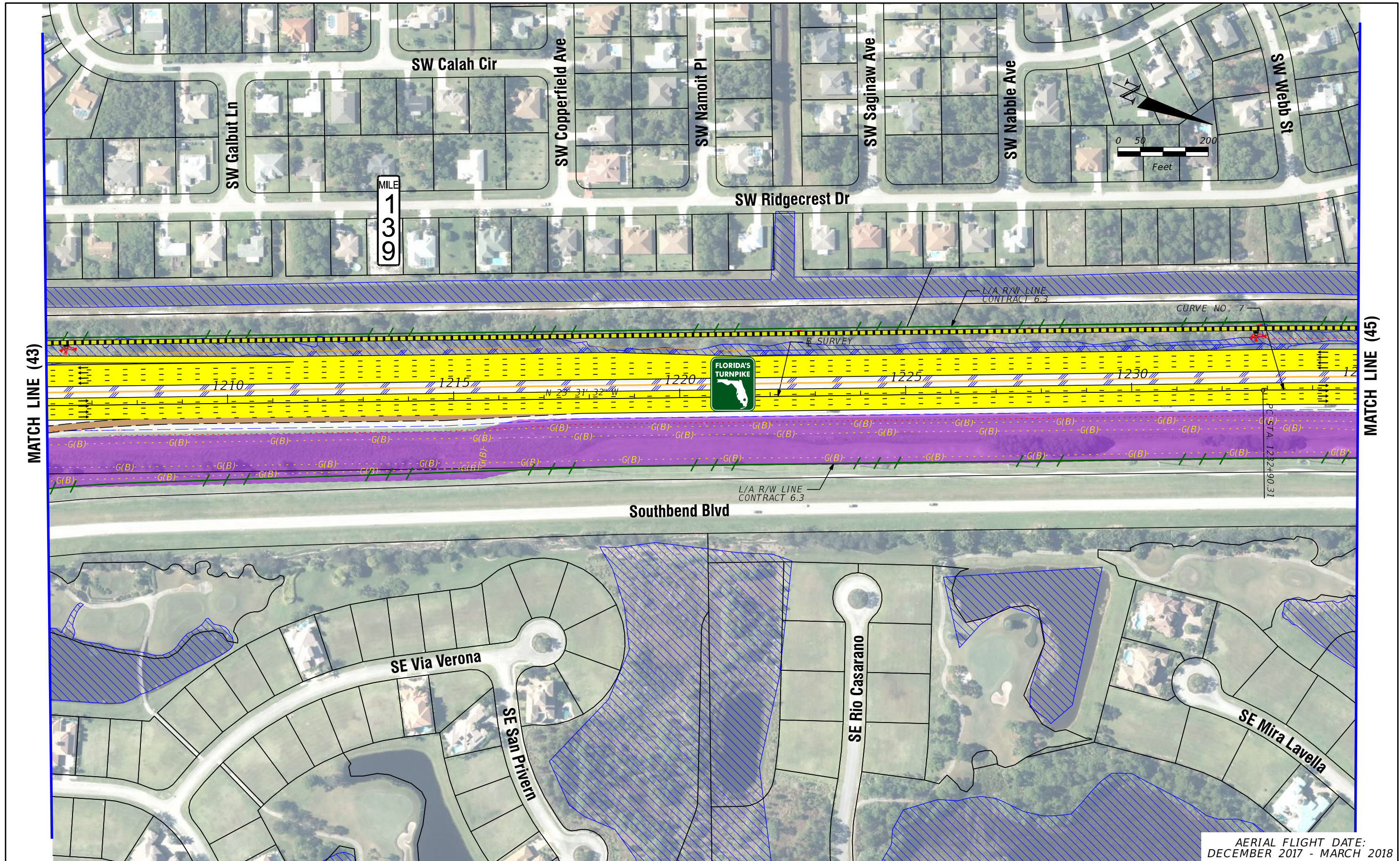
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**43B**





MATCH LINE (43)

MATCH LINE (45)

LEGEND		NOTE: ROW = RIGHT OF WAY	
	PROPERTY LINES		EXISTING BRIDGE
	TRAFFIC BARRIER WALL		OTHER PROJECTS (BRIDGE)
	EXISTING ROW LINE		OTHER PROJECTS (ROADS)
	EXISTING LIMITED ACCESS ROW LINE		FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
	POTENTIAL ROW LINE		POTENTIAL BUSINESS RELOCATION
	POTENTIAL LIMITED ACCESS ROW LINE		POTENTIAL RESIDENTIAL RELOCATION
	RAILROAD		PROPOSED ITS
	PROPOSED ROW NOISE BARRIER		POTENTIAL NEW TRAFFIC SIGNAL
	PROPOSED SHOULDER NOISE BARRIER		EXISTING TRAFFIC SIGNAL
	PAVEMENT REMOVAL		TOLL GANTRY
	WETLANDS/OTHER SURFACE WATERS		POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
	PROPOSED ROADWAY		
	PROPOSED BRIDGE		

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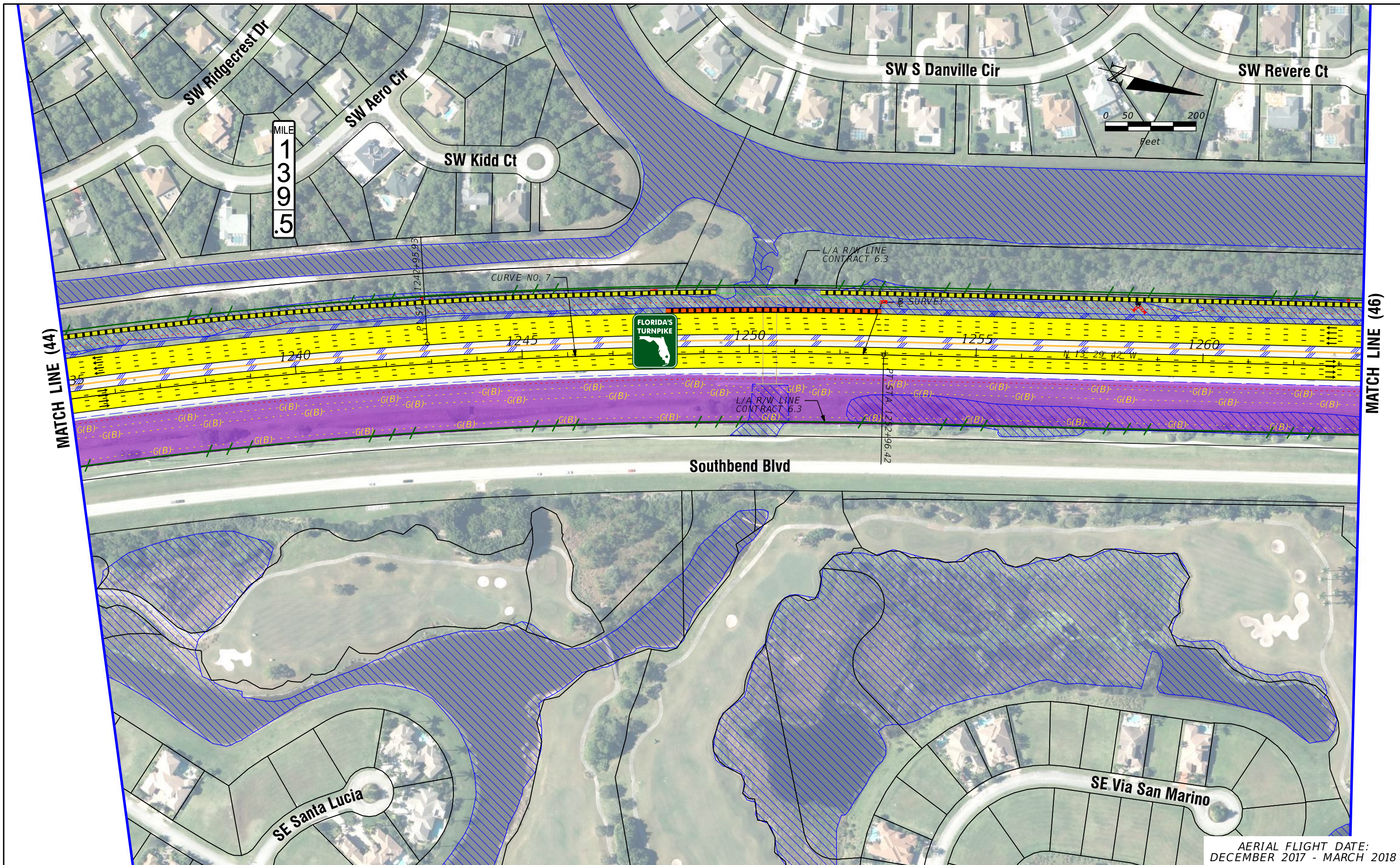
FLORIDA'S TURNPIKE (SR 91) WIDENING  
 FROM JUPITER (INDIANTOWN ROAD/SR 706)  
 TO FT. PIERCE (OKEECHOBEE ROAD/SR 70)  
 PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY  
 FINANCIAL PROJECT IDENTIFICATION (FPID) NUMBER: 423374-1

**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018

SHEET NO.  
 44





<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

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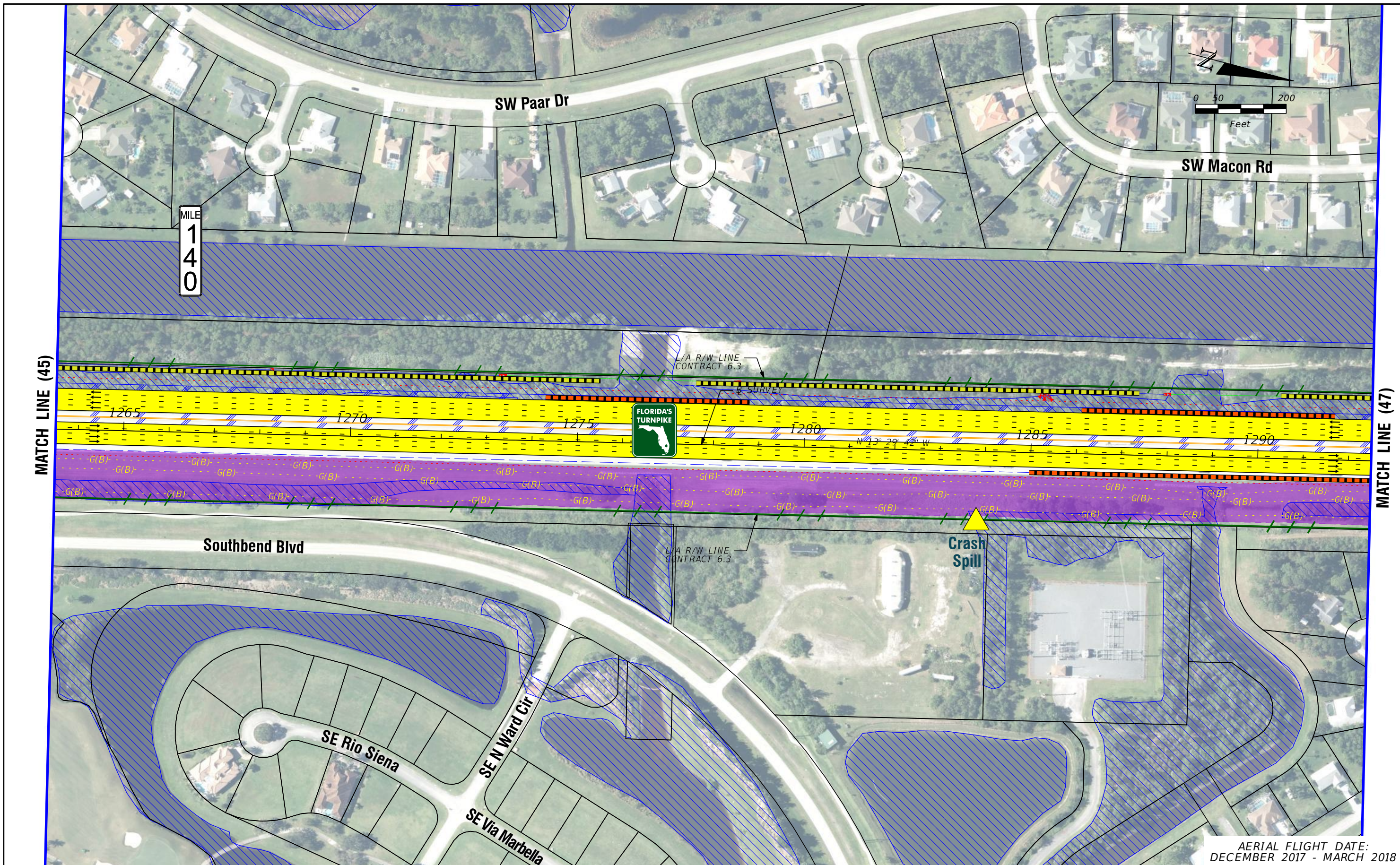
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**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
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SHEET NO.  
 45

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018





LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

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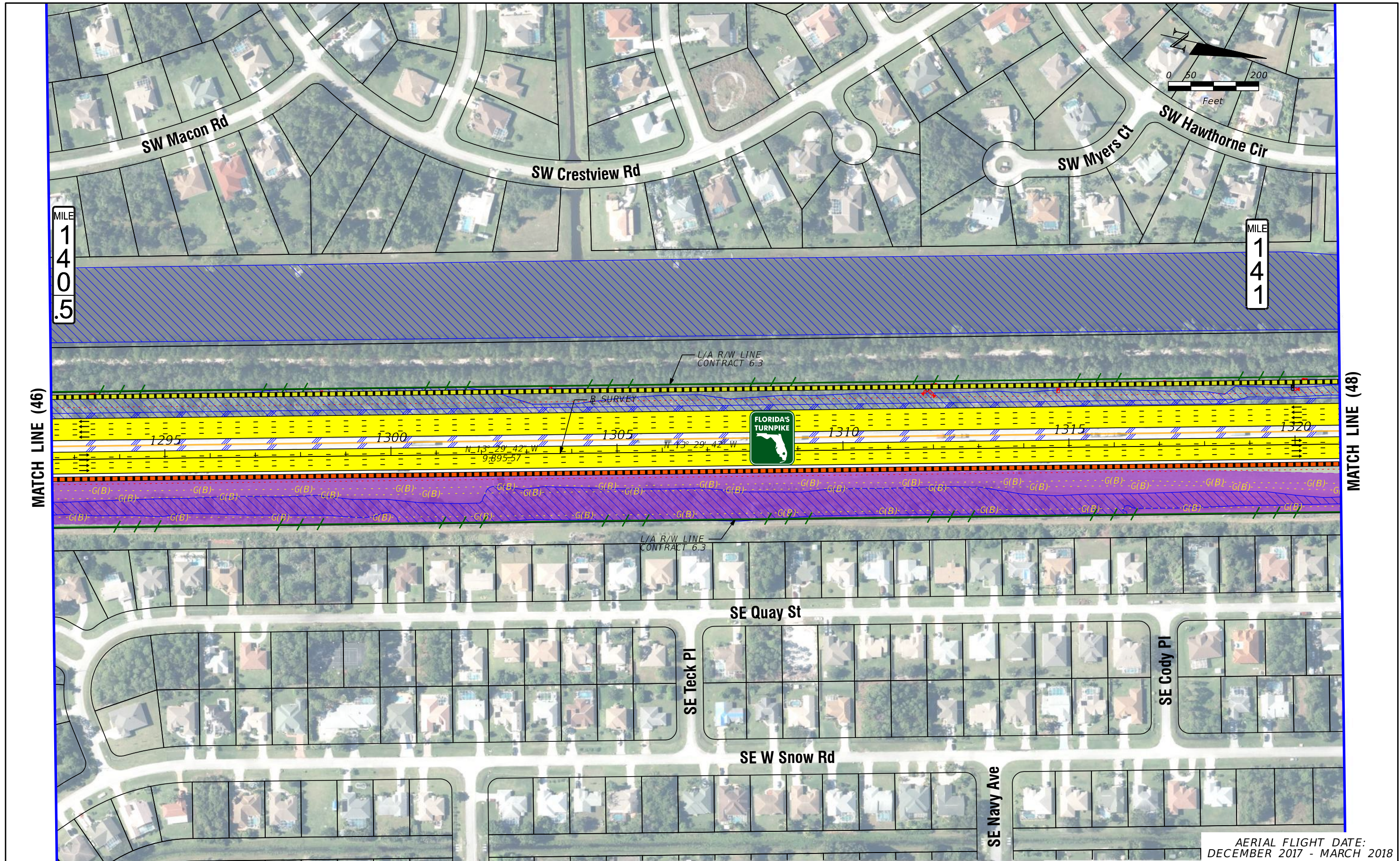
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**FL TPK PD&E WIDENING  
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 DECEMBER 2017 - MARCH 2018

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LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

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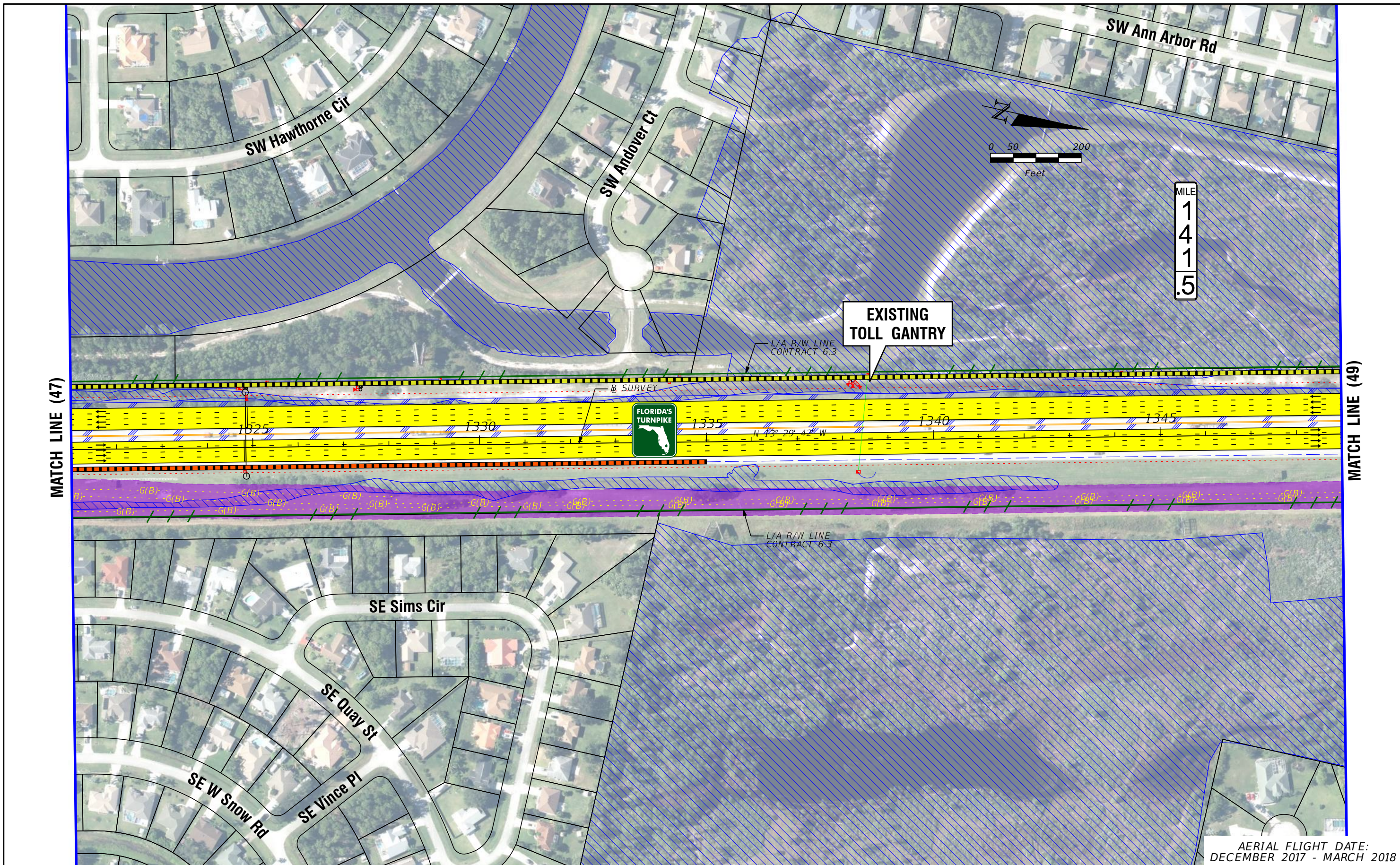
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**FL TPK PD&E WIDENING  
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AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

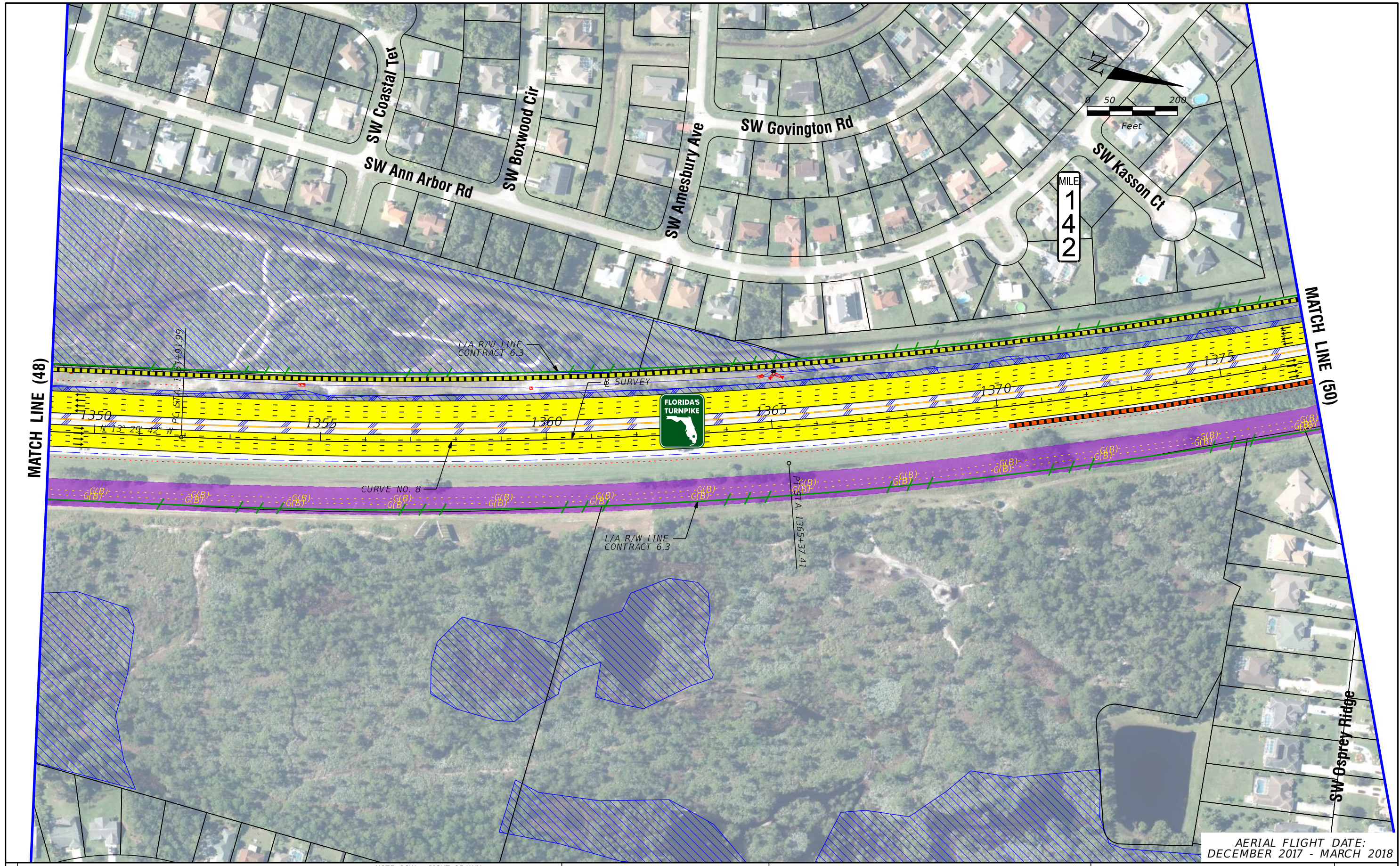
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**48**





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LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

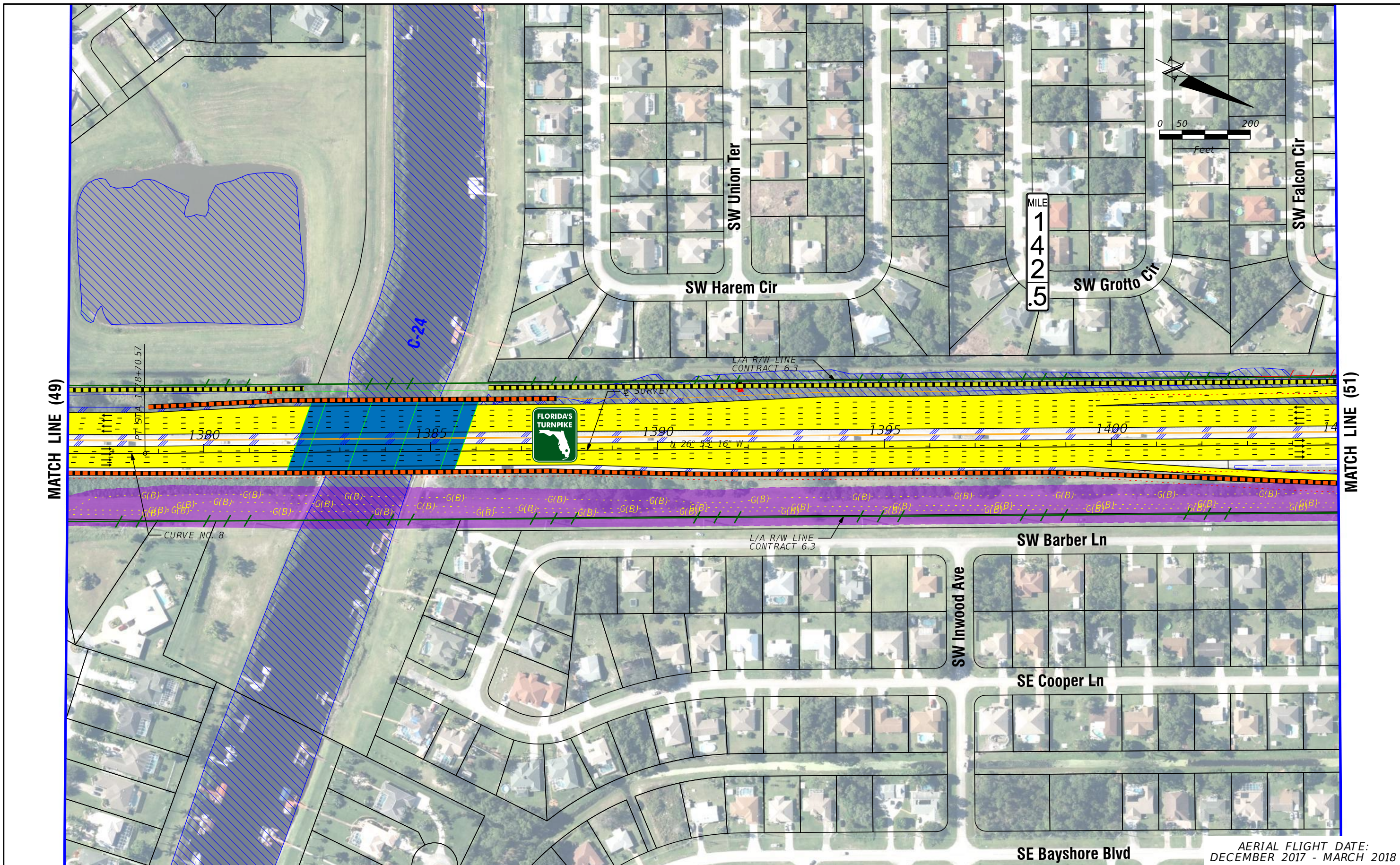
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

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**49**





<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

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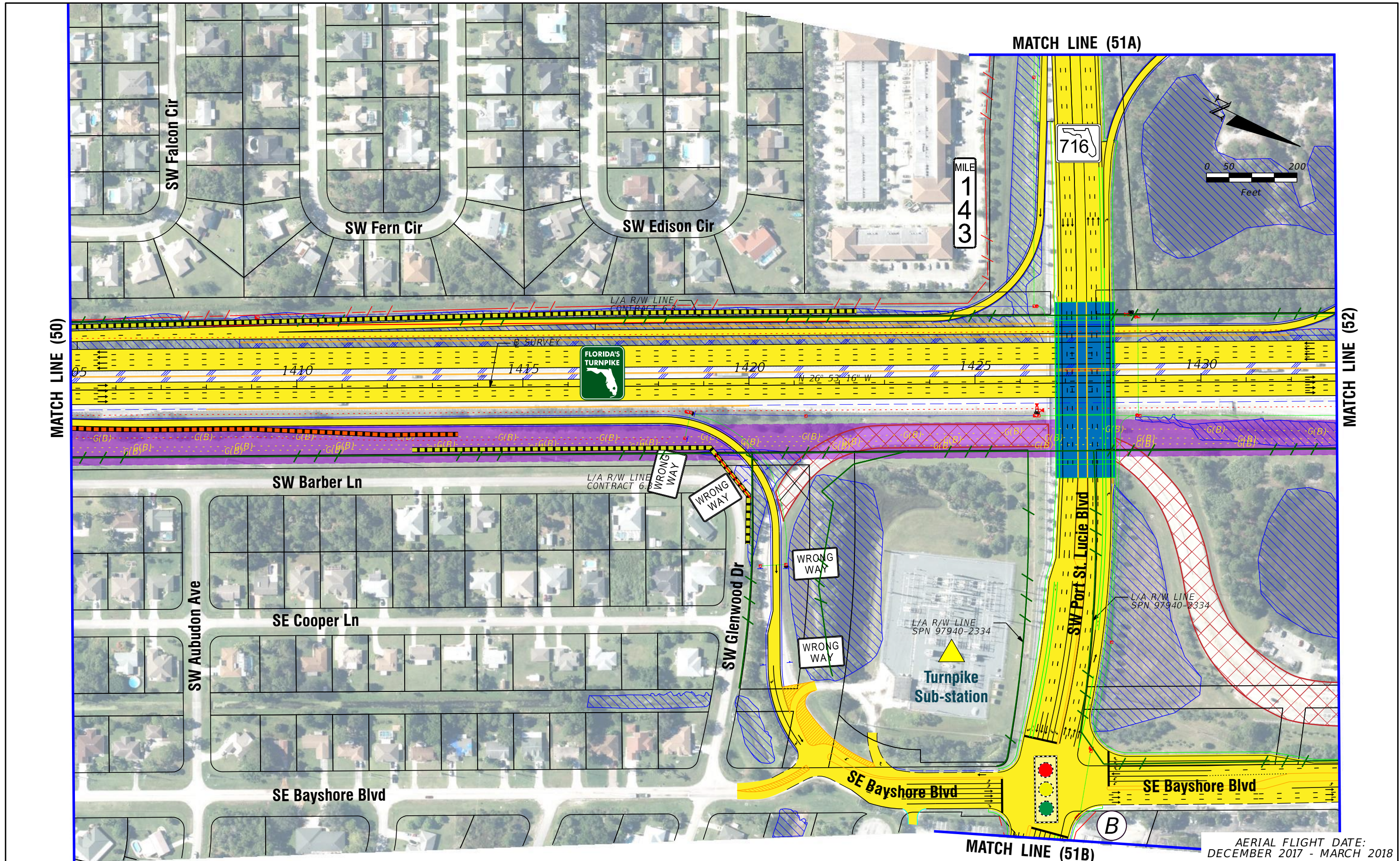
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**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

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LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	NOTE: ROW = RIGHT OF WAY	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)		—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)		—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH		—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	ⓑ	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE		POTENTIAL RESIDENTIAL RELOCATION	ⓓ			
—	RAILROAD								

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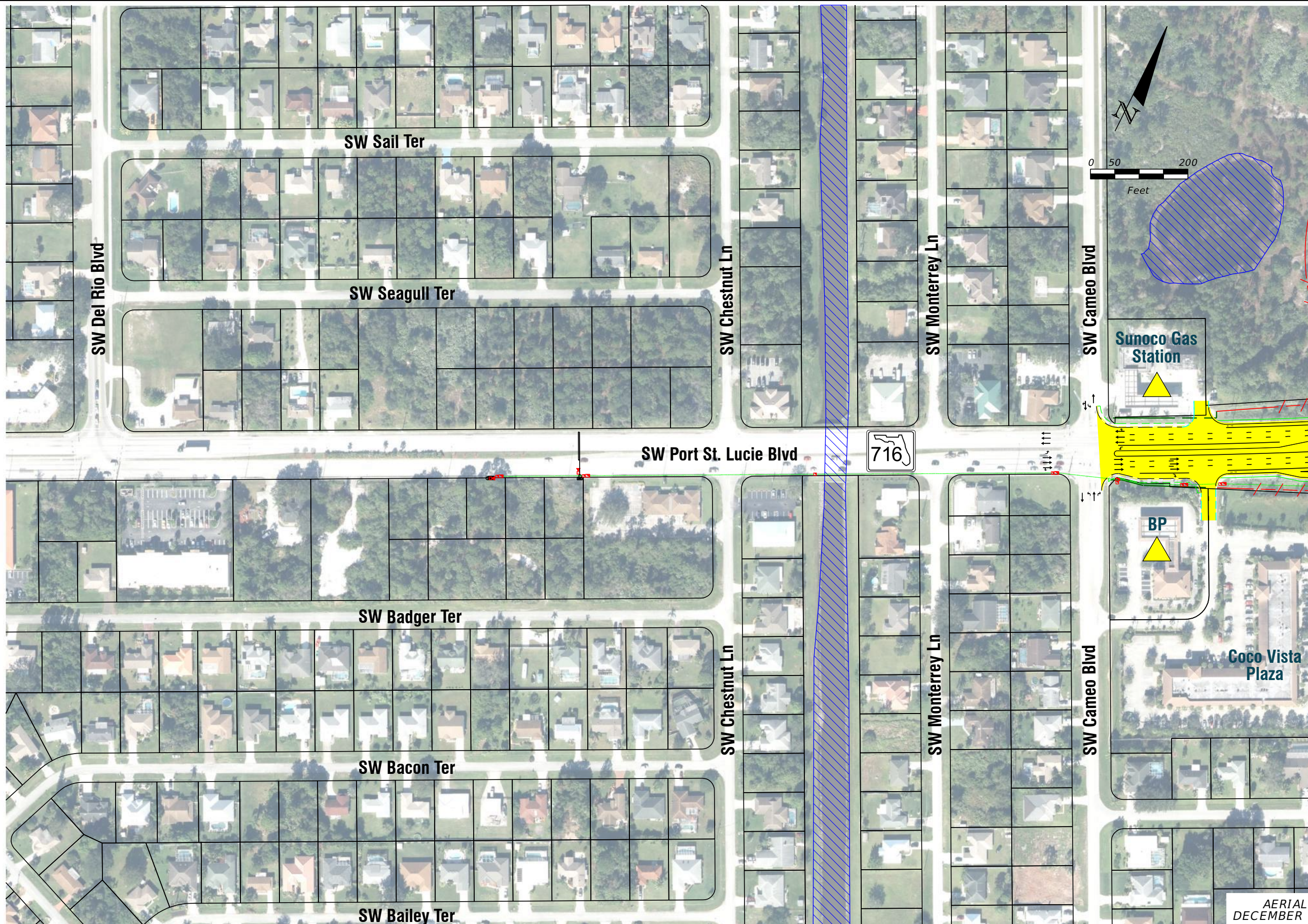
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**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

SHEET NO.  
**51**

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

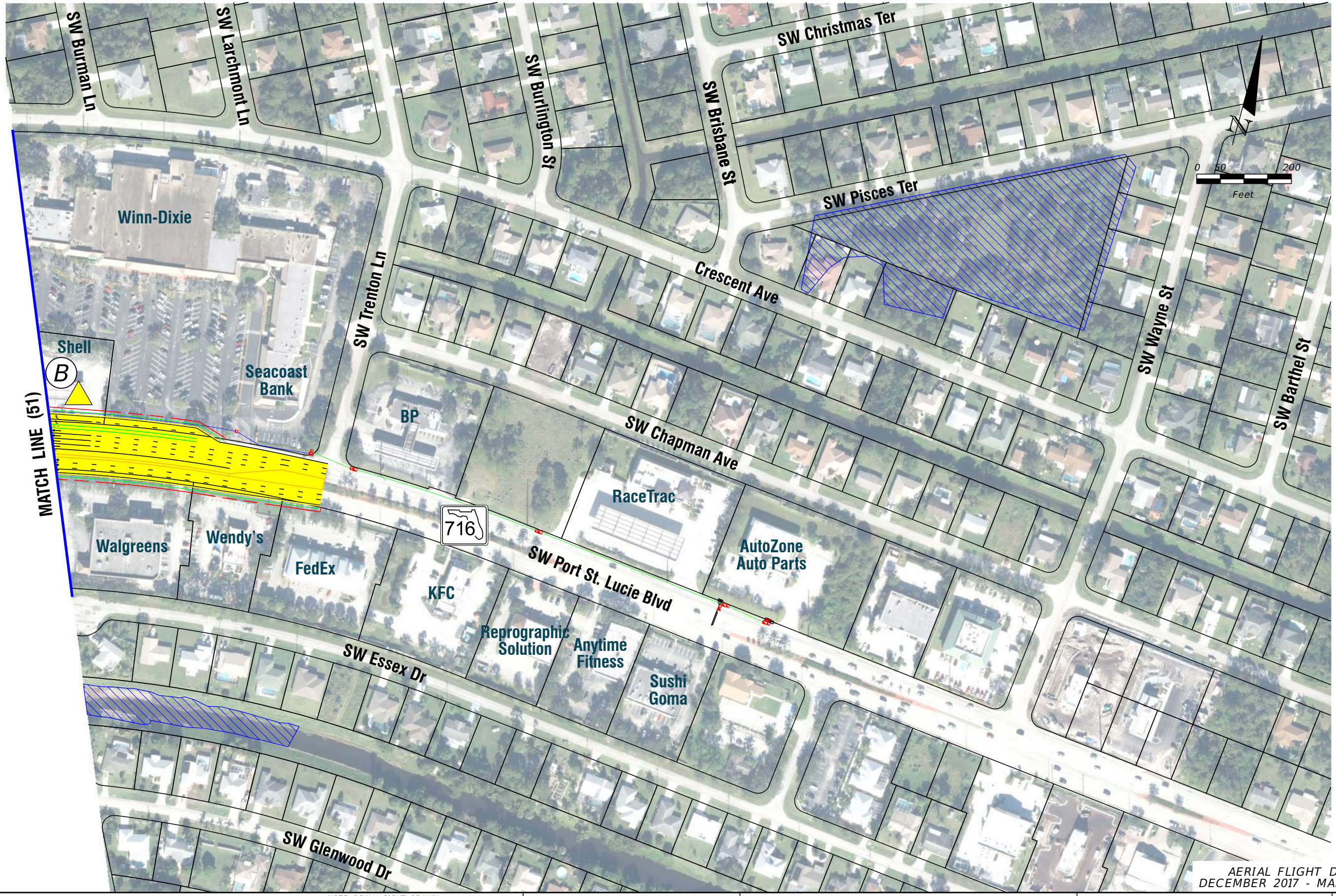
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
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SHEET NO.  
**51A**





AERIAL FLIGHT DATE:  
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<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	—	
—	RAILROAD	—	—	

—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE
—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)
—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)
—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION
—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION

NOTE: ROW = RIGHT OF WAY

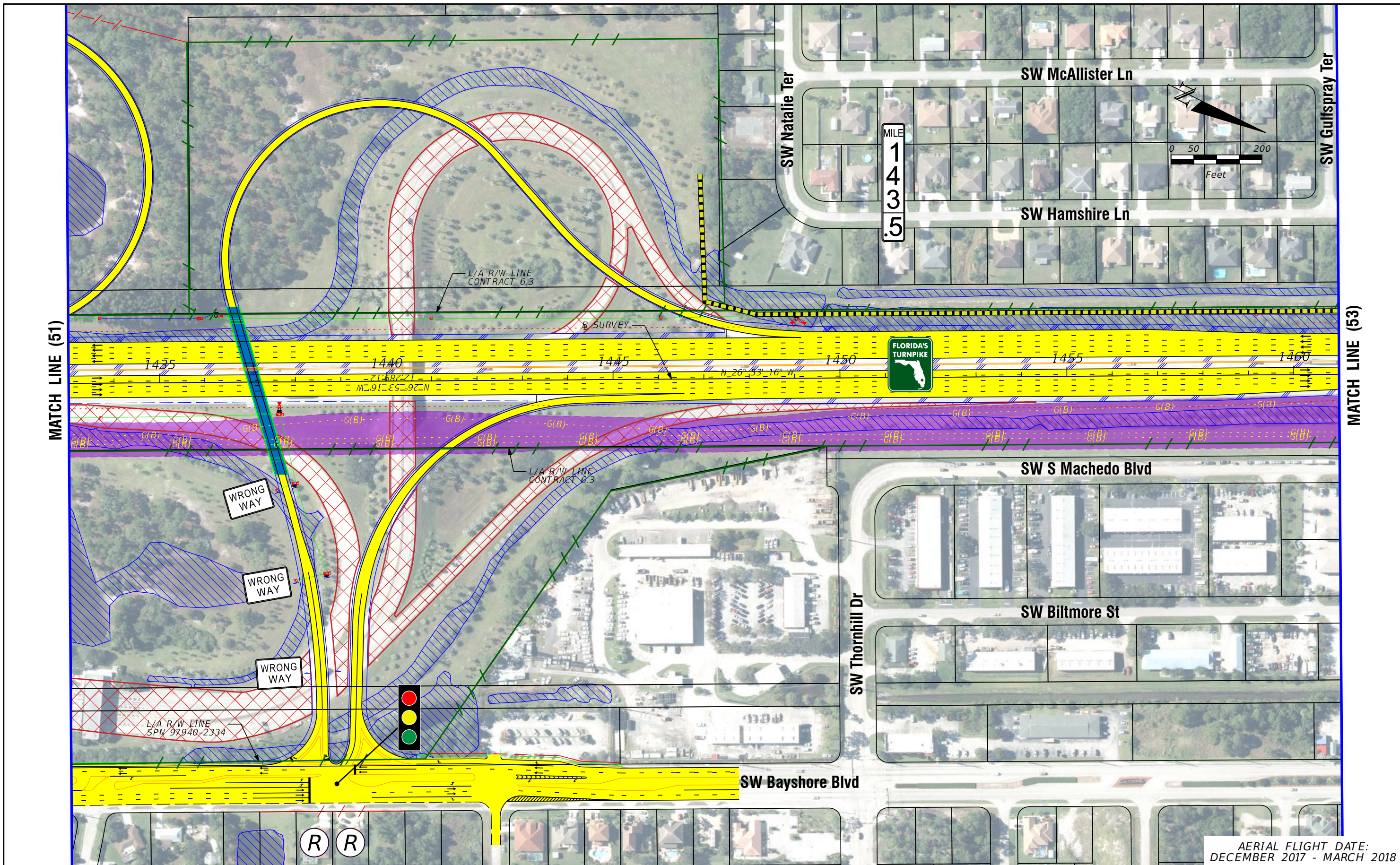
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**51B**





<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

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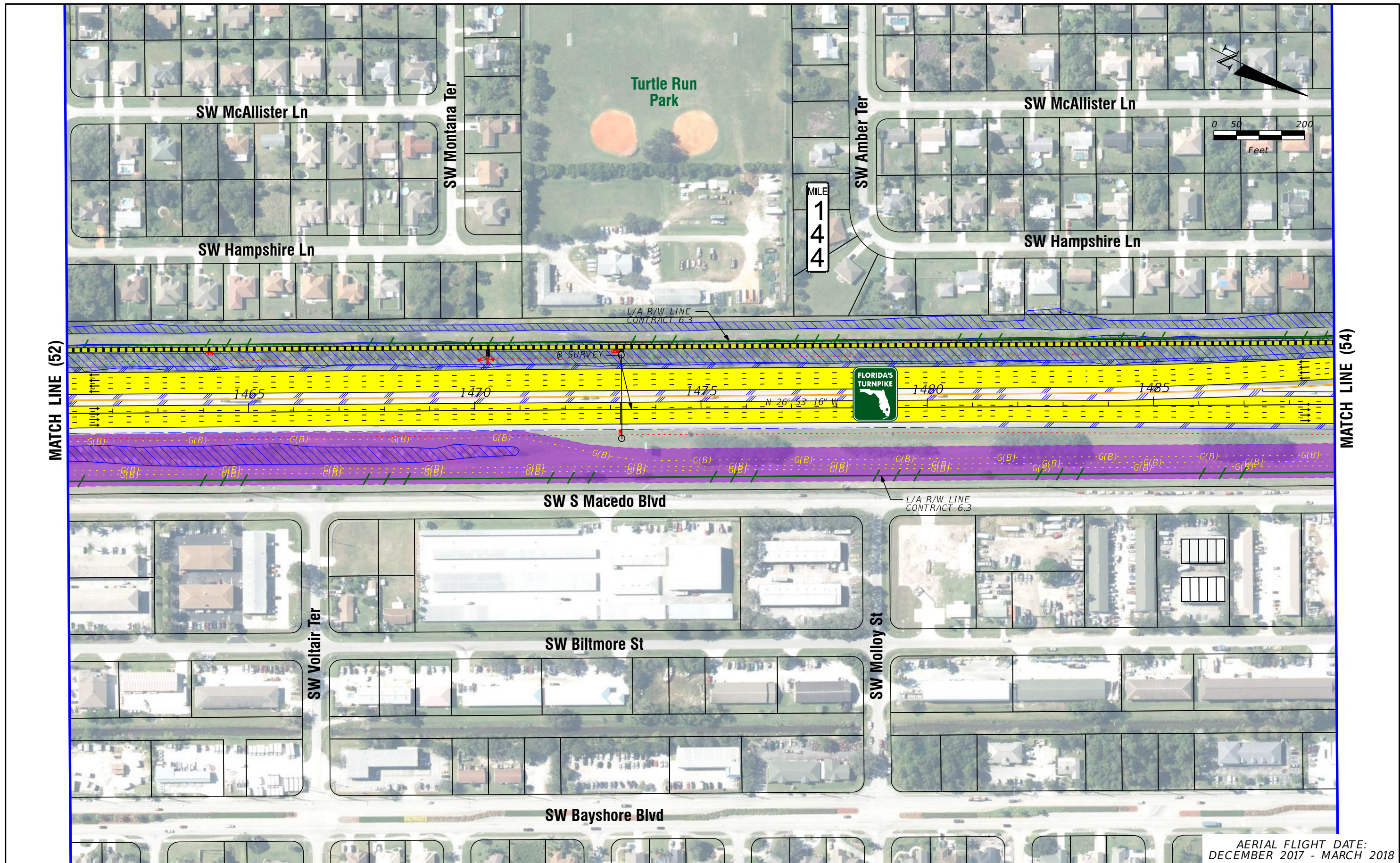
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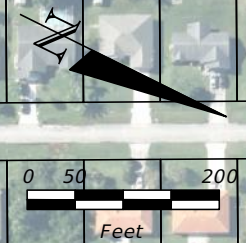
SHEET NO.  
 52





MATCH LINE (52)

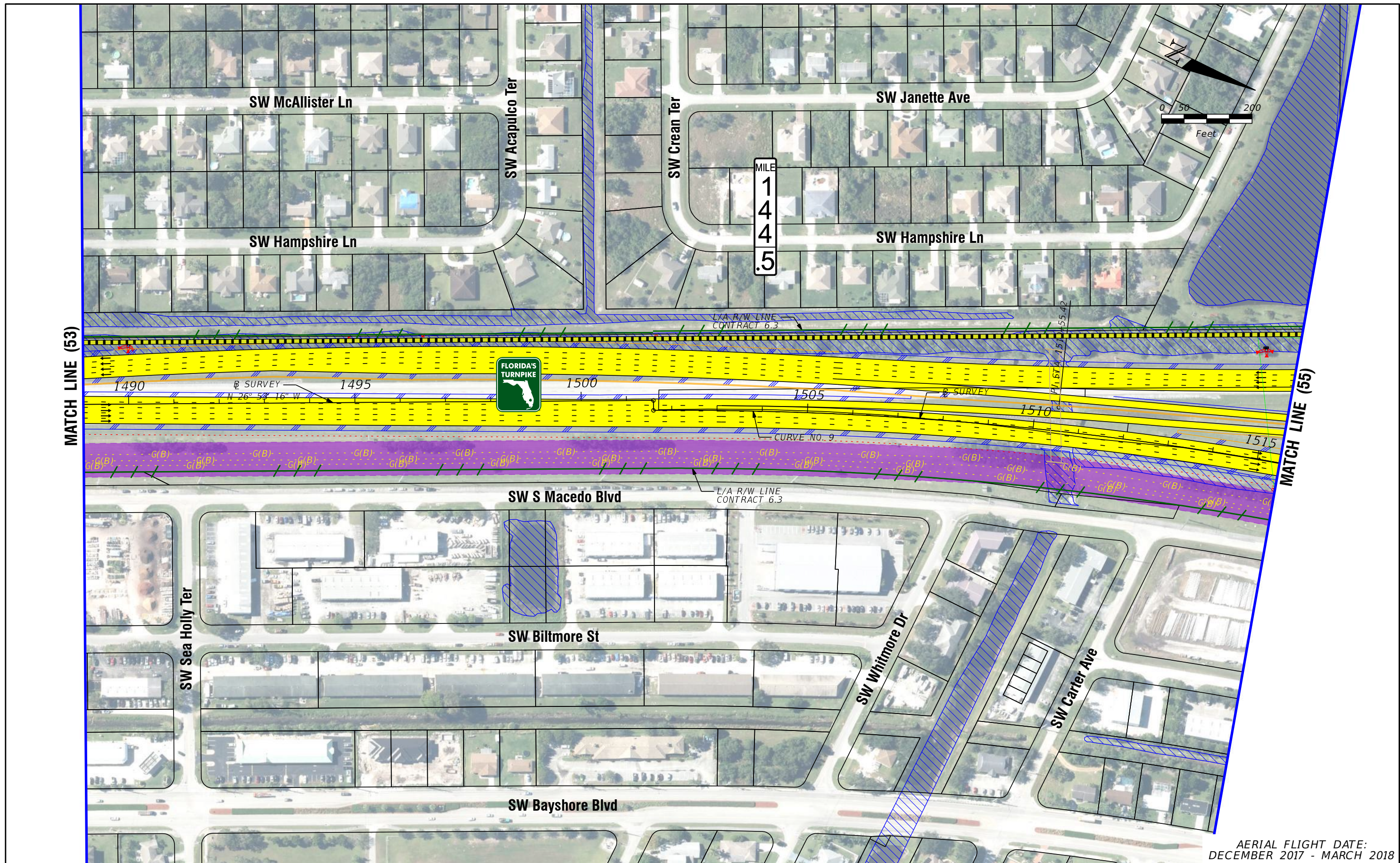
MATCH LINE (54)



MILE  
1  
4  
4

<b>LEGEND</b> — PROPERTY LINES — TRAFFIC BARRIER WALL - - - EXISTING ROW LINE - - - EXISTING LIMITED ACCESS ROW LINE - - - POTENTIAL ROW LINE - - - POTENTIAL LIMITED ACCESS ROW LINE — RAILROAD [Yellow dashed] PROPOSED ROW NOISE BARRIER [Orange dashed] PROPOSED SHOULDER NOISE BARRIER [Red X] PAVEMENT REMOVAL [Blue wavy] WETLANDS/OTHER SURFACE WATERS [Yellow] PROPOSED ROADWAY [Blue] PROPOSED BRIDGE [Orange] EXISTING BRIDGE [Brown] OTHER PROJECTS (BRIDGE) [Brown] OTHER PROJECTS (ROADS) [Purple] FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH (B) POTENTIAL BUSINESS RELOCATION (R) POTENTIAL RESIDENTIAL RELOCATION [Green] PROPOSED ITS [Traffic light] POTENTIAL NEW TRAFFIC SIGNAL [Traffic light] EXISTING TRAFFIC SIGNAL [Circle] TOLL GANTRY [Triangle] POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	NOTE: ROW = RIGHT OF WAY H. W. LOCHNER, INC. 4350 W. CYPRESS ST., SUITE 800 TAMPA, FLORIDA 33607 FBPR CERTIFICATE OF AUTH. #894	FLORIDA'S TURNPIKE (SR 91) WIDENING FROM JUPITER (INDIANTOWN ROAD/SR 706) TO FT. PIERCE (OKEECHOBEE ROAD/SR 70) PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY FINANCIAL PROJECT IDENTIFICATION (FPID) NUMBER: 423374-1	<b>FL TPK PD&amp;E WIDENING          PREFERRED ALTERNATIVE          CONCEPT PLAN</b>	SHEET NO. 53
	AERIAL FLIGHT DATE: DECEMBER 2017 - MARCH 2018			





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

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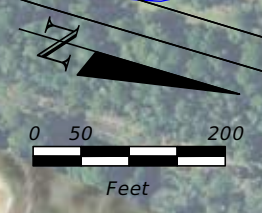
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**54**



MATCH LINE (55A)



MILE 145

FUTURE MULTI-USE TRAIL

L/A R/W LINE CONTRACT 6.3

Crosstown Pkwy



Ft. Pierce Service Plaza

Ft. Pierce Service Plaza

PROPOSED TOLL GANTRY

CURVE NO. 9

1520

1525

1530

1535

1540

MATCH LINE (54)

MATCH LINE (56)



SW S Macedo Blvd

L/A B/W LINE CONTRACT 6.3

SW Grove Ave

SW Biltmore St

SW Eyerly Ave

AERIAL FLIGHT DATE: DECEMBER 2017 - MARCH 2018

—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION		
—	RAILROAD						

NOTE: ROW = RIGHT OF WAY

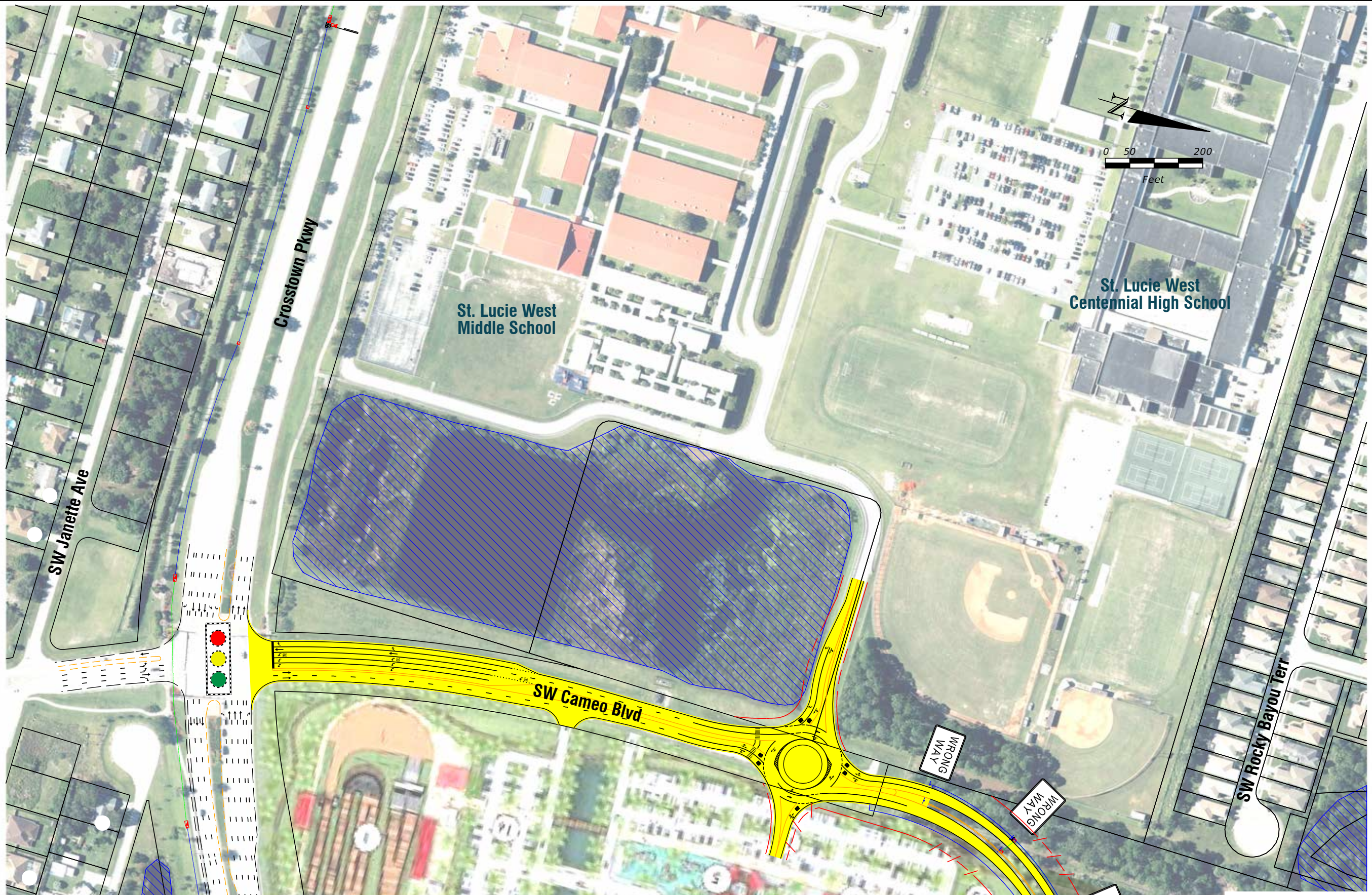
H. W. LOCHNER, INC.  
4350 W. CYPRESS ST., SUITE 800  
TAMPA, FLORIDA 33607  
FBPR CERTIFICATE OF AUTH. #894

FLORIDA'S TURNPIKE (SR 91) WIDENING  
FROM JUPITER (INDIANTOWN ROAD/SR 706)  
TO FT. PIERCE (OKEECHOBEE ROAD/SR 70)  
PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY  
FINANCIAL PROJECT IDENTIFICATION (FPID) NUMBER: 423374-1

FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN

SHEET NO.  
55





**MATCH LINE (55)**

AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	—	
—	RAILROAD	—	—	

—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	NOTE: ROW = RIGHT OF WAY
—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	
—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	
—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	
—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	
—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	

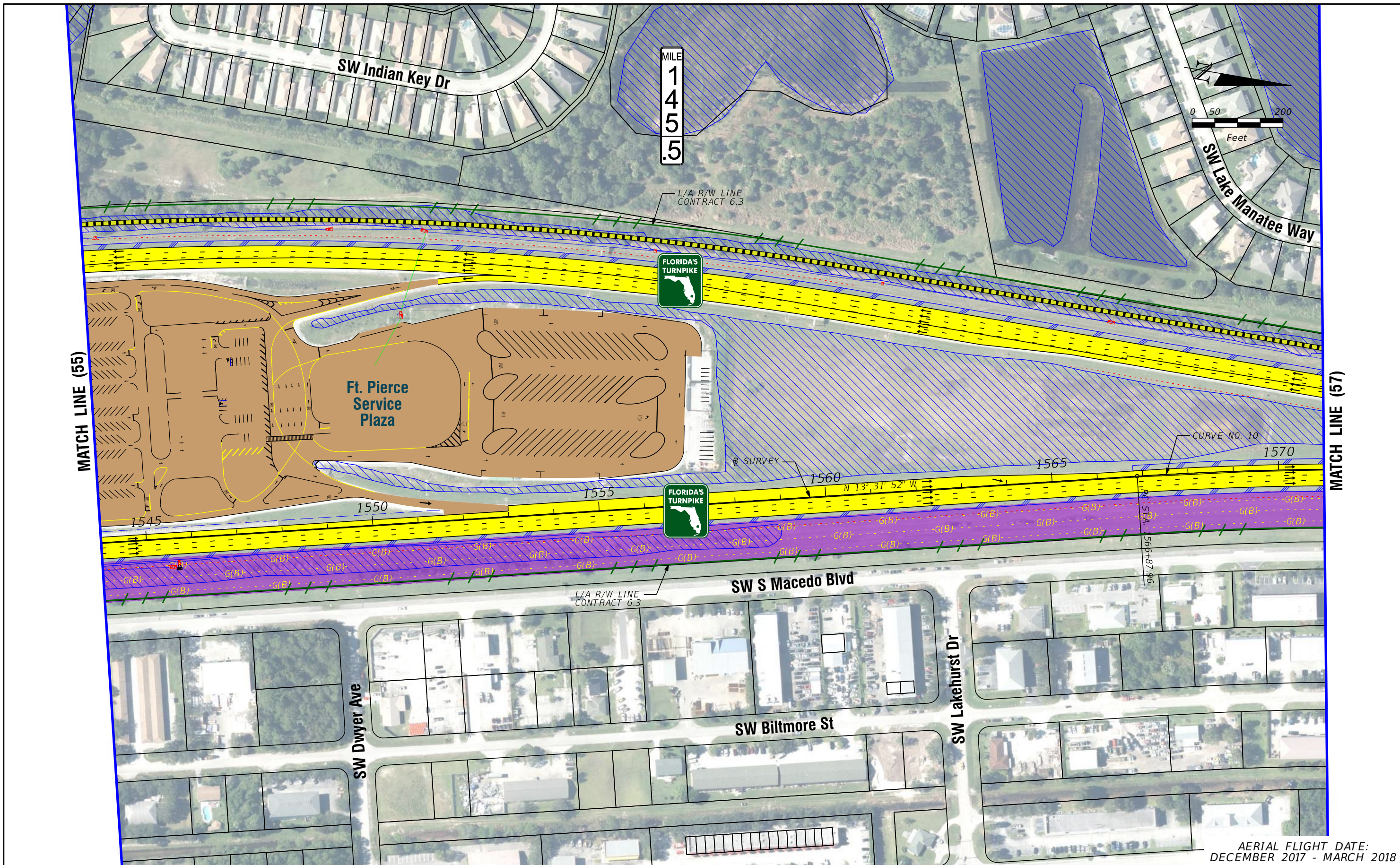
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FINANCIAL PROJECT IDENTIFICATION (FPID) NUMBER: 423374-1

**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**55A**





LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

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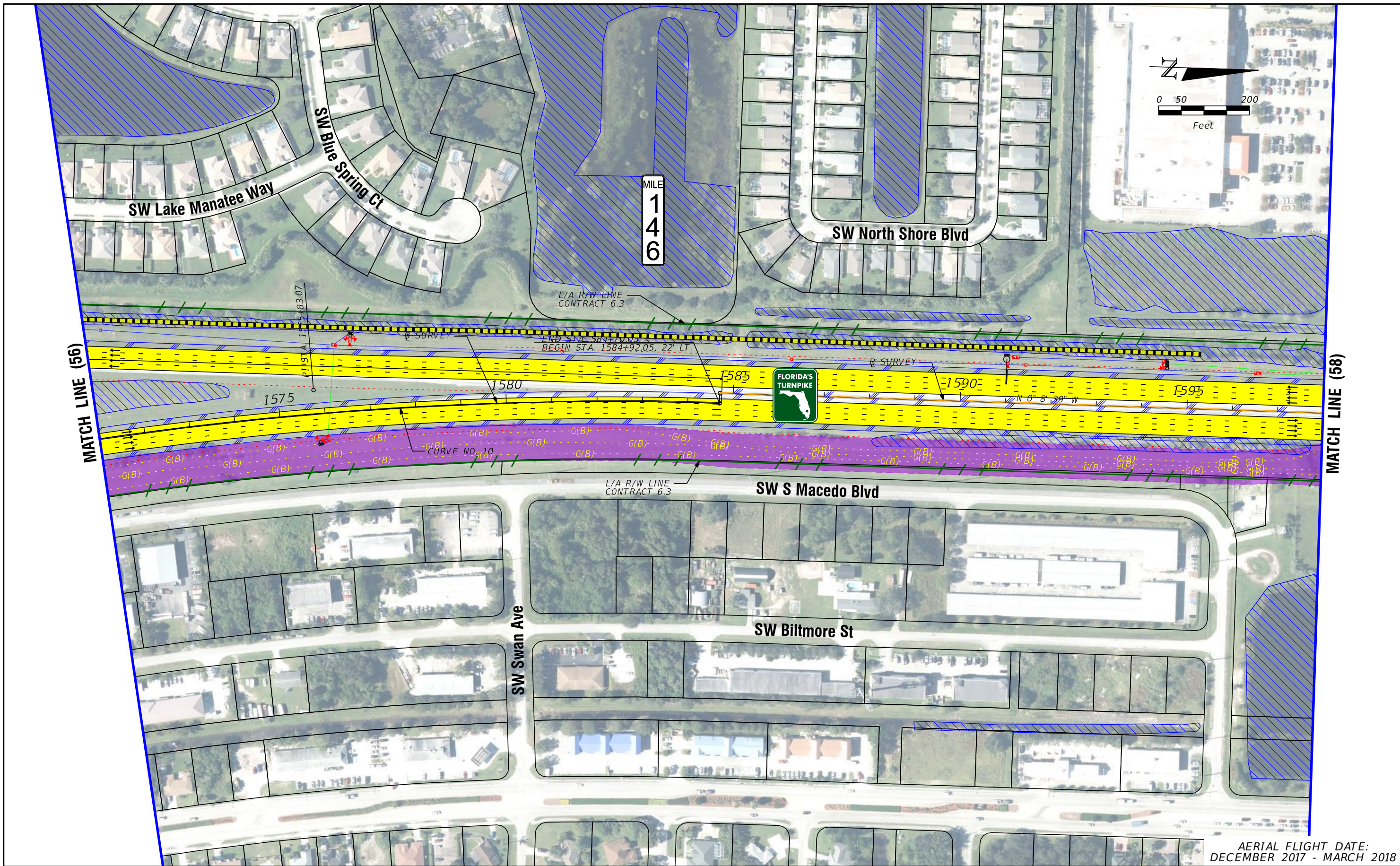
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**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

SHEET NO.  
 56

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	—	
—	RAILROAD	—	—	

NOTE: ROW = RIGHT OF WAY

—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE
—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)
—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)
—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION
—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION

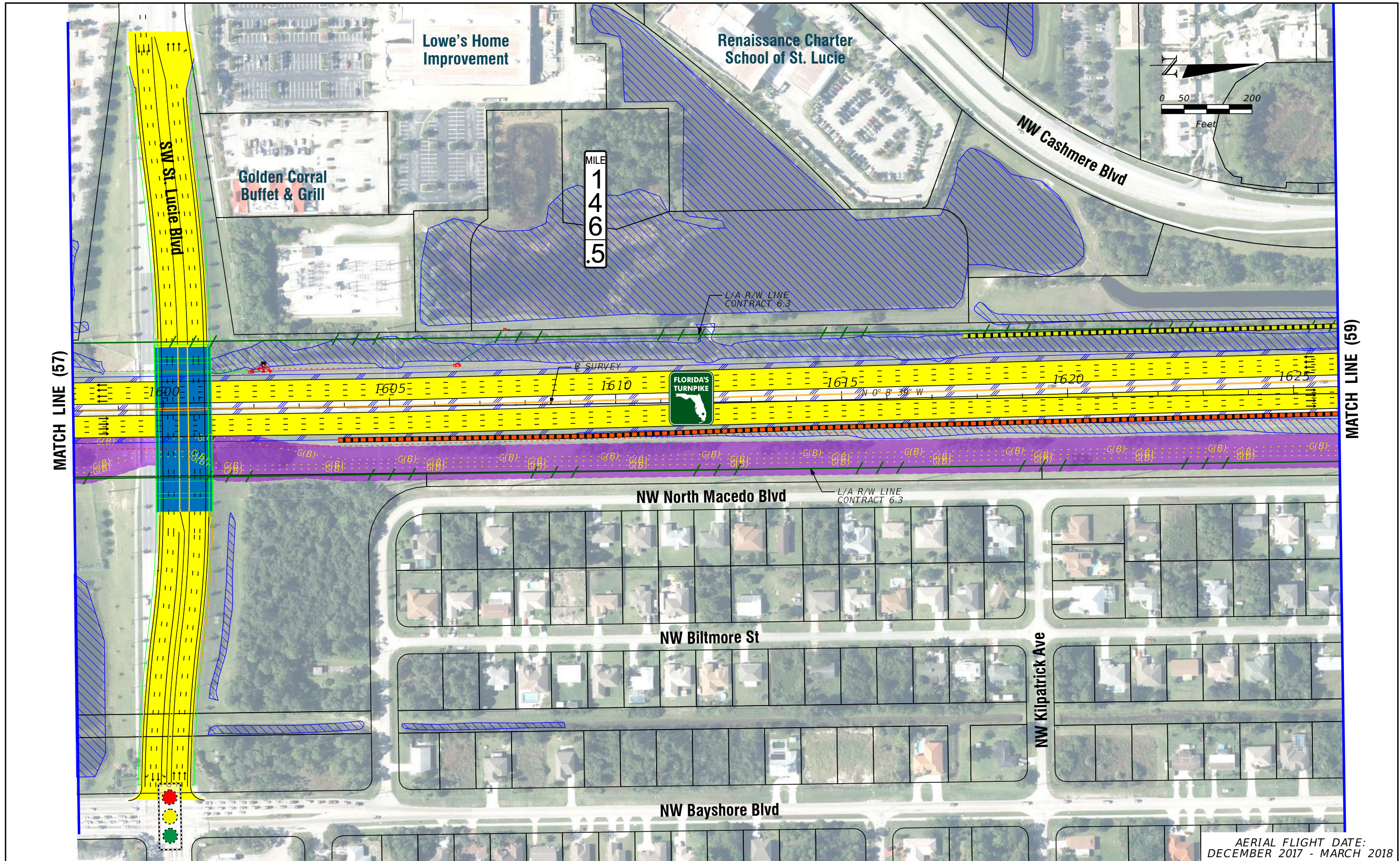
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

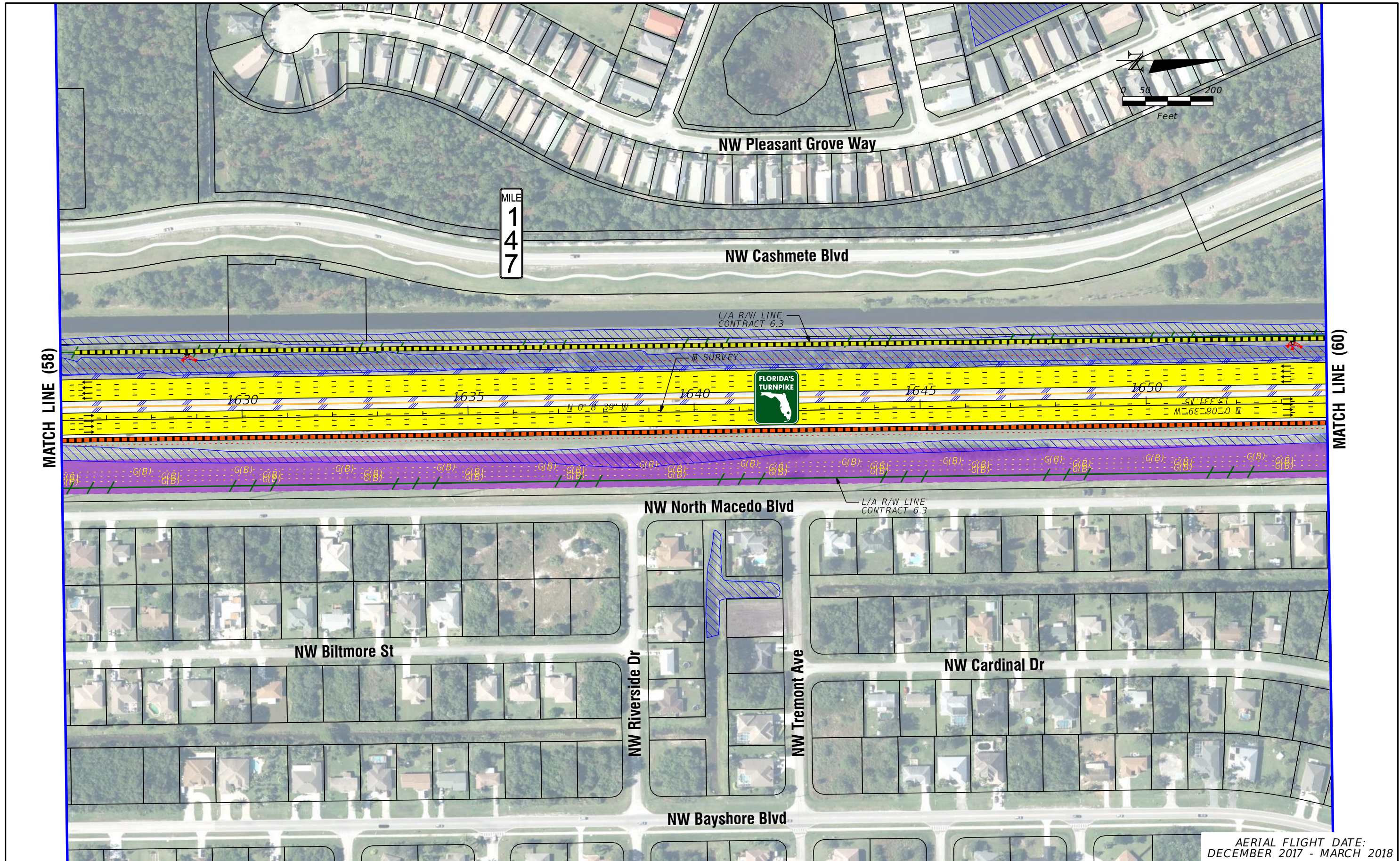
SHEET NO.  
57





<b>LEGEND</b> — PROPERTY LINES — TRAFFIC BARRIER WALL — EXISTING ROW LINE - - - EXISTING LIMITED ACCESS ROW LINE - - - POTENTIAL ROW LINE - - - POTENTIAL LIMITED ACCESS ROW LINE — RAILROAD — PROPOSED ROW NOISE BARRIER — PROPOSED SHOULDER NOISE BARRIER — PAVEMENT REMOVAL — WETLANDS/OTHER SURFACE WATERS — PROPOSED ROADWAY — PROPOSED BRIDGE — EXISTING BRIDGE — OTHER PROJECTS (BRIDGE) — OTHER PROJECTS (ROADS) — FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH (B) POTENTIAL BUSINESS RELOCATION (R) POTENTIAL RESIDENTIAL RELOCATION — PROPOSED ITS — POTENTIAL NEW TRAFFIC SIGNAL — EXISTING TRAFFIC SIGNAL — TOLL GANTRY — POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	NOTE: ROW = RIGHT OF WAY	H. W. LOCHNER, INC. 4350 W. CYPRESS ST., SUITE 800 TAMPA, FLORIDA 33607 FBPR CERTIFICATE OF AUTH. #894	FLORIDA'S TURNPIKE (SR 91) WIDENING FROM JUPITER (INDIANTOWN ROAD/SR 706) TO FT. PIERCE (OKEECHOBEE ROAD/SR 70) PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY FINANCIAL PROJECT IDENTIFICATION (FPID) NUMBER: 423374-1	<b>FL TPK PD&amp;E WIDENING          PREFERRED ALTERNATIVE          CONCEPT PLAN</b>	AERIAL FLIGHT DATE: DECEMBER 2017 - MARCH 2018	SHEET NO. 58
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<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	—	
—	RAILROAD	—	—	

NOTE: ROW = RIGHT OF WAY

—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE
—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)
—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)
—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION
—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION

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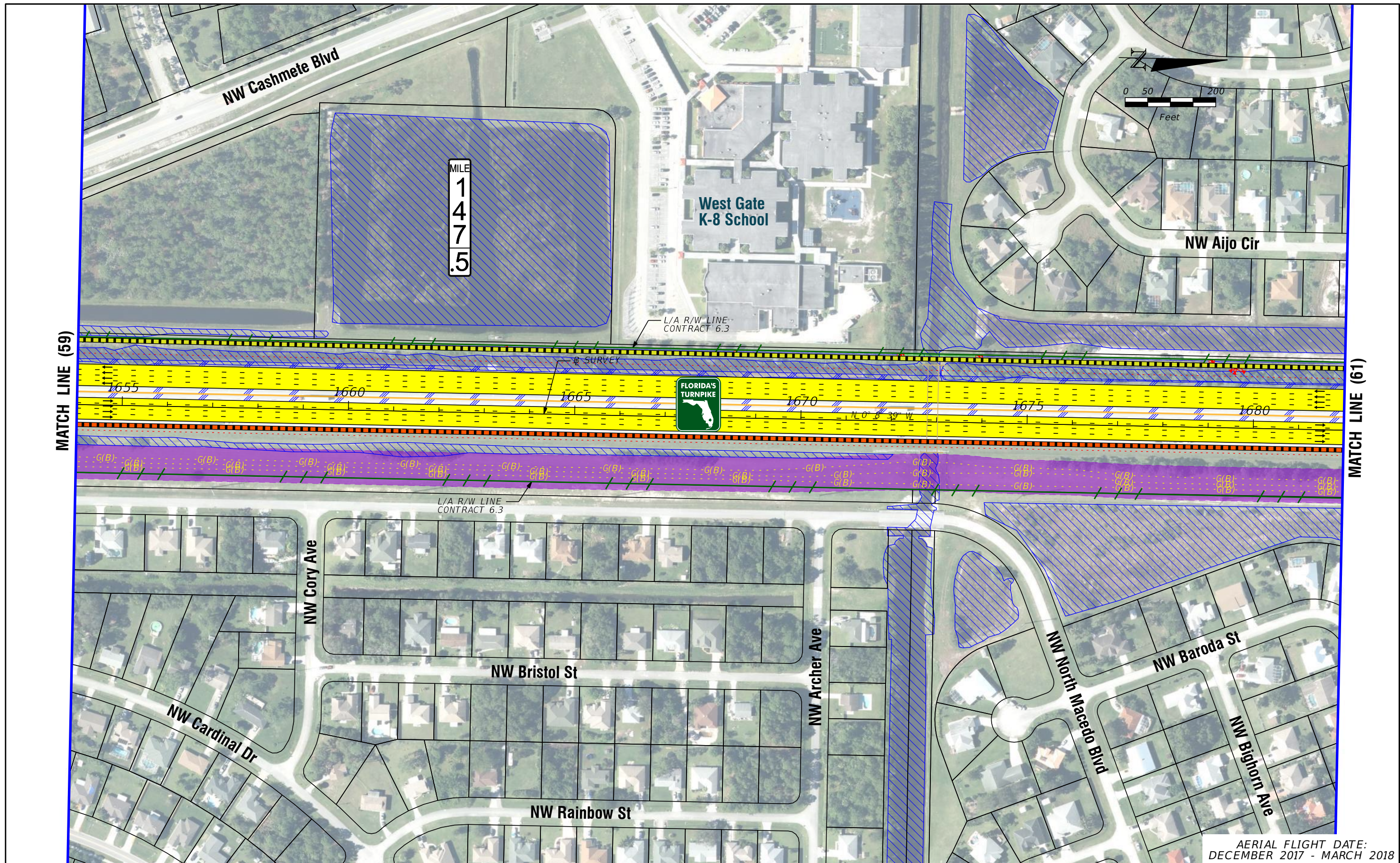
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**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018

SHEET NO.  
 59





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**60**

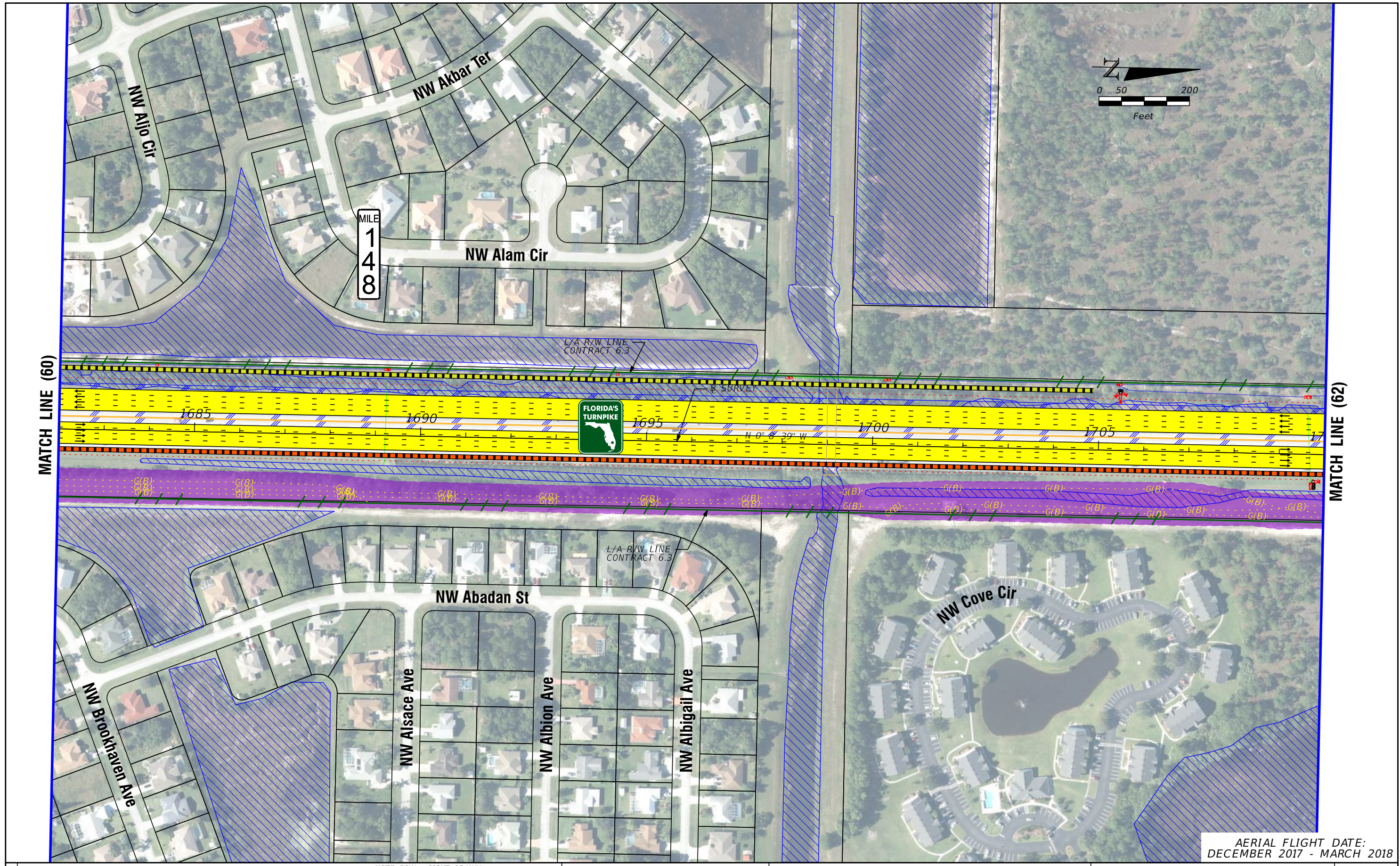




MILE  
1  
4  
8

MATCH LINE (60)

MATCH LINE (62)



LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

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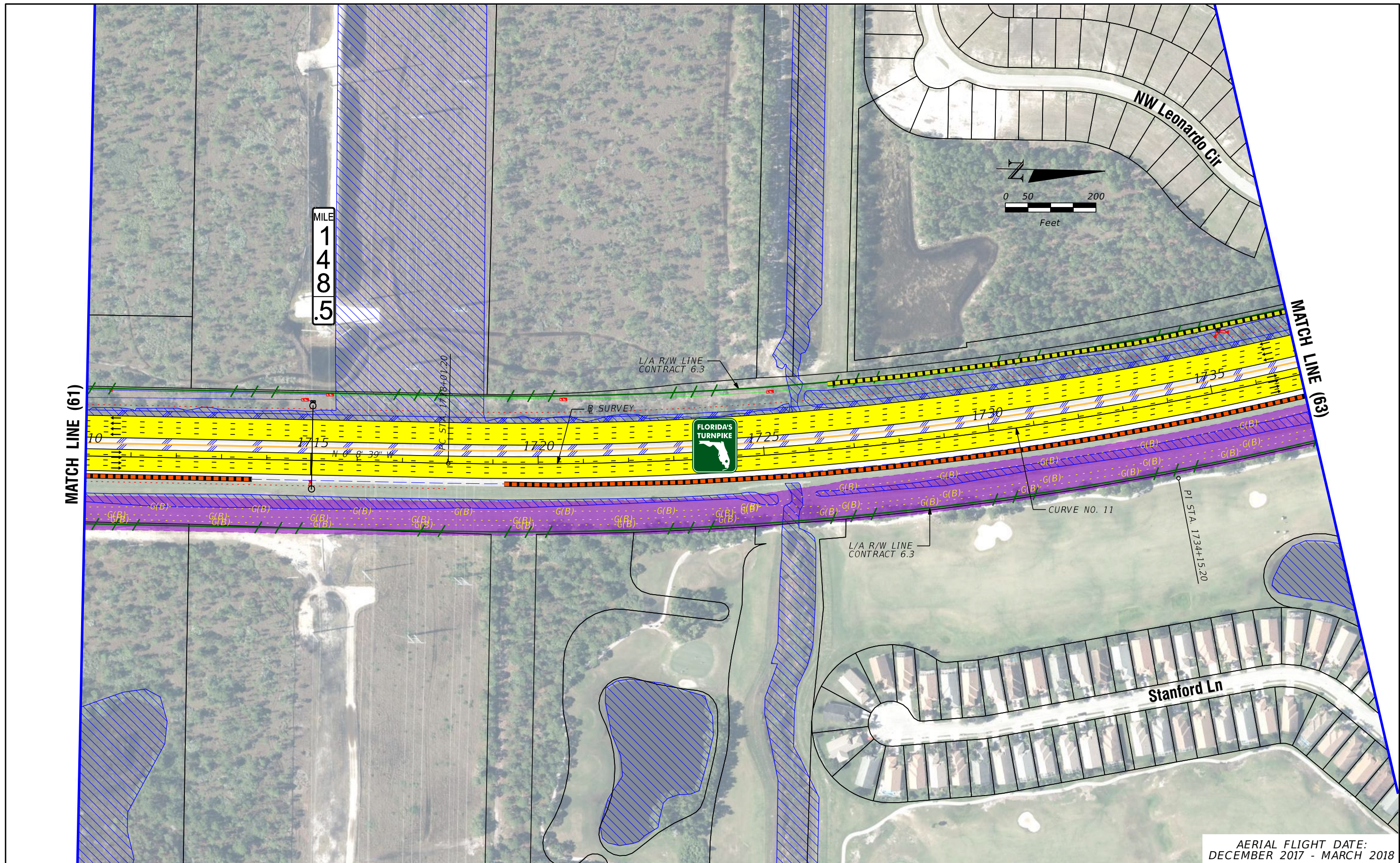
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

SHEET NO.  
61





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

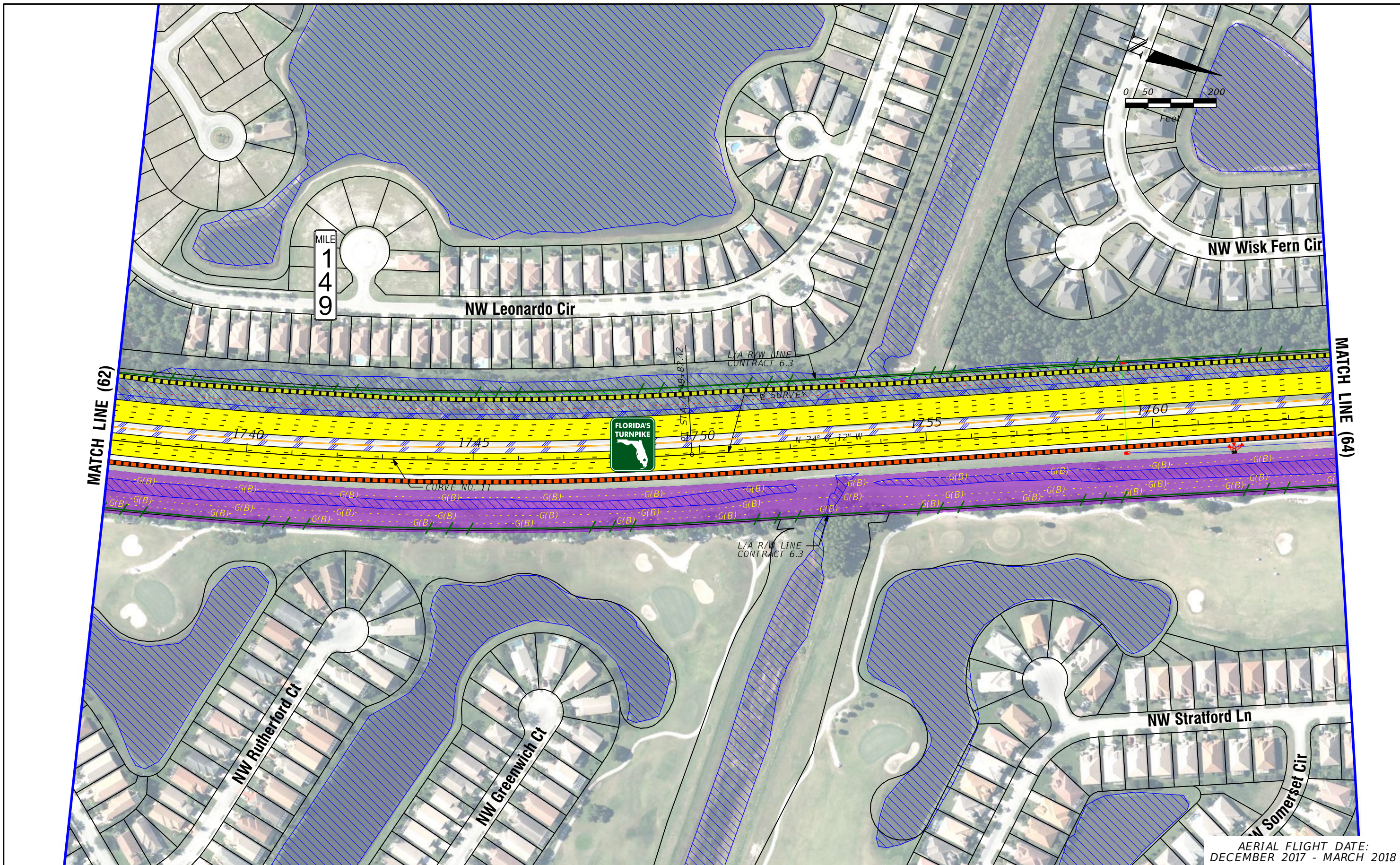
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**62**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

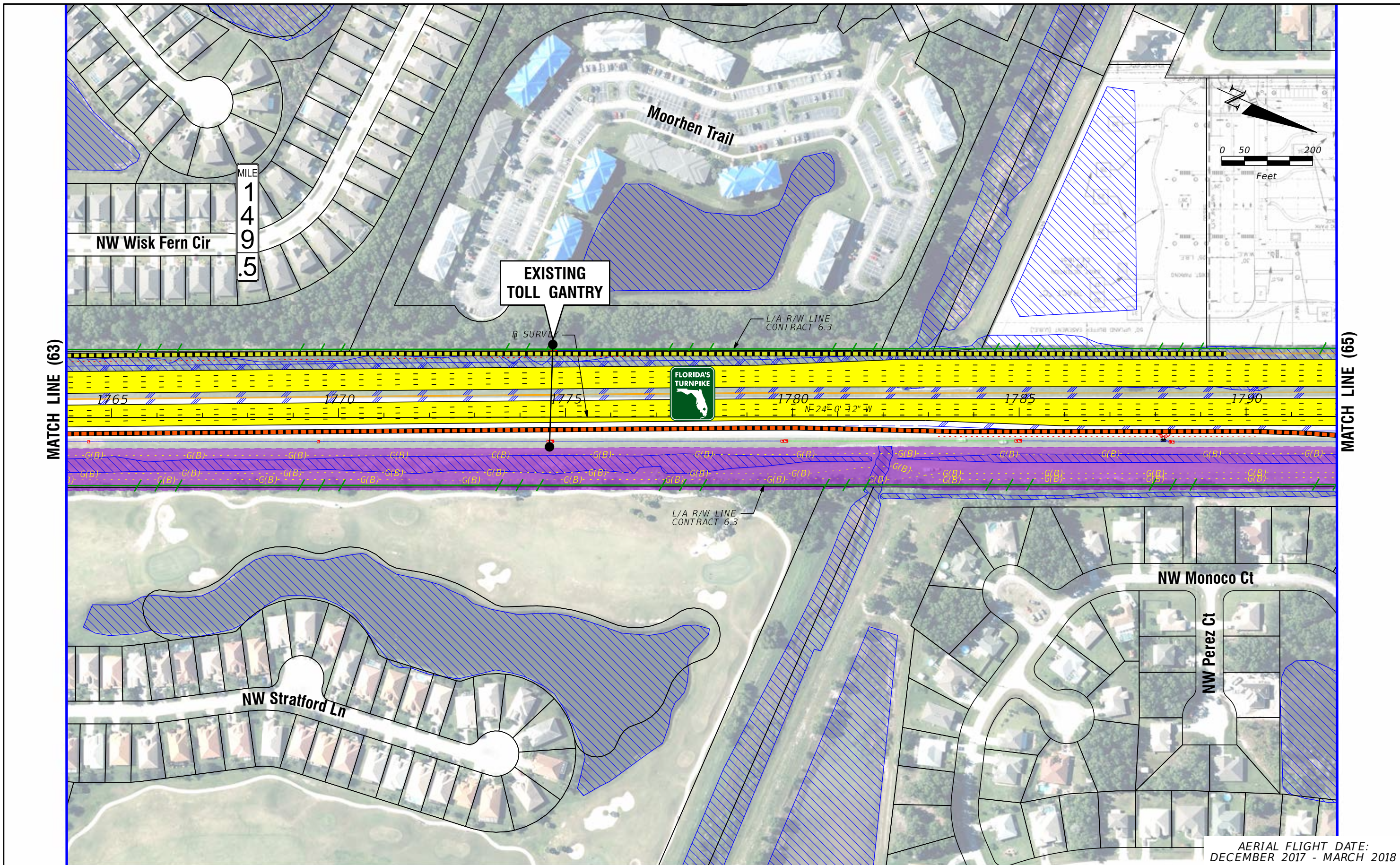
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**63**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

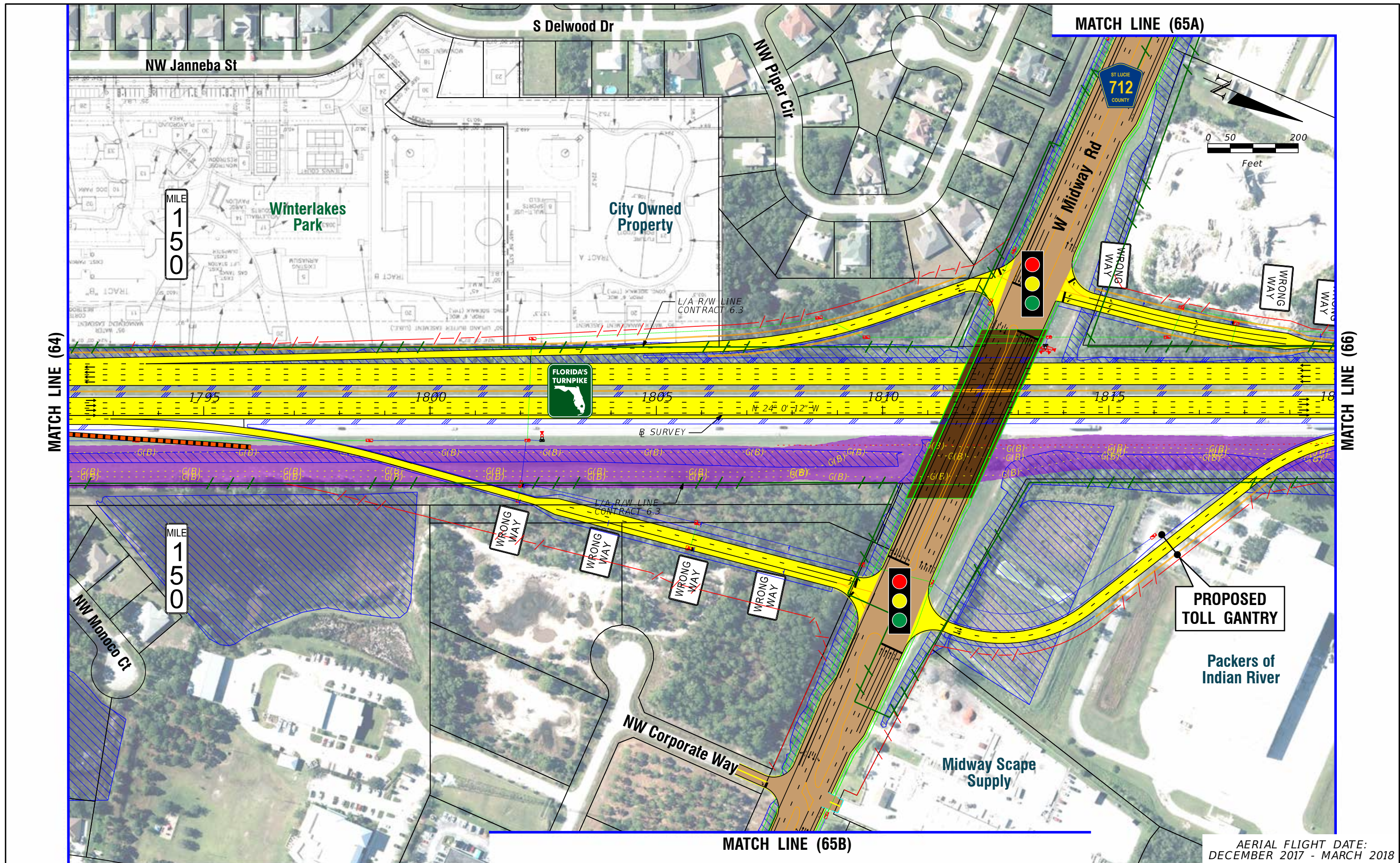
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PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**64**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

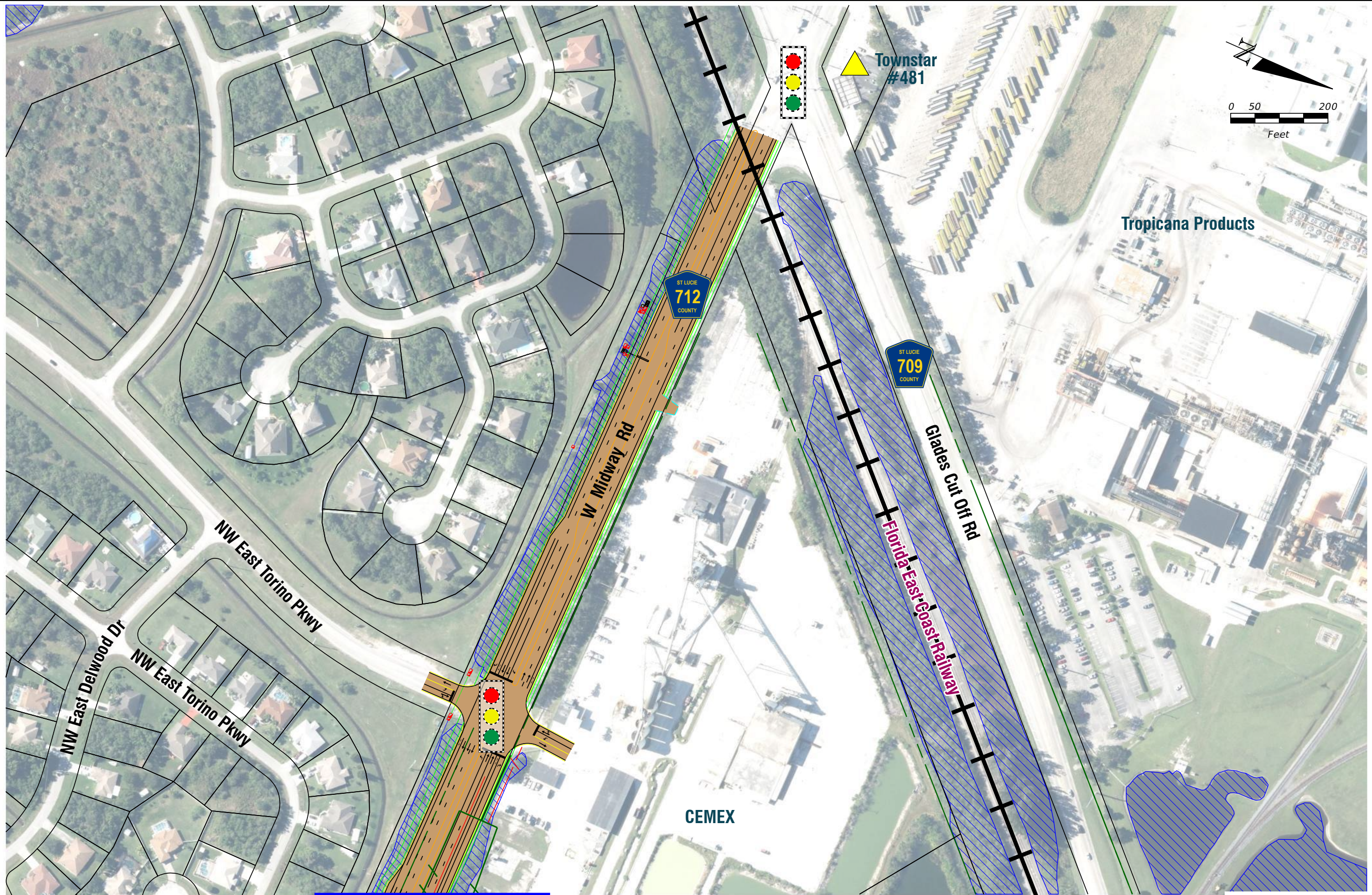
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PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**65**





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DECEMBER 2017 - MARCH 2018

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	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	—	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—	—	—	—
—	RAILROAD	—	—	—	—	—	—	—	—

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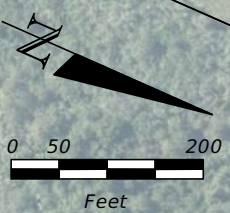
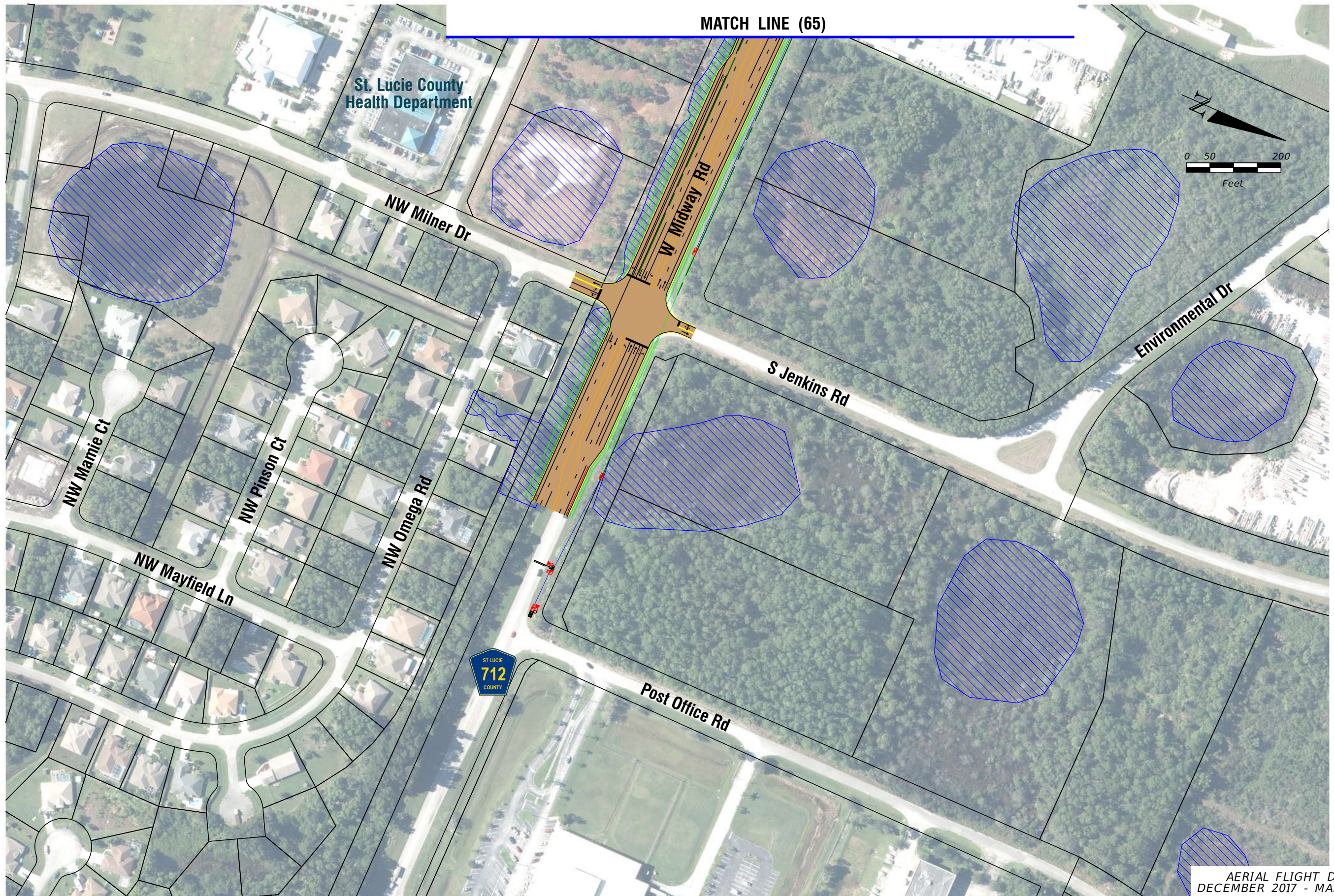
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**65A**



MATCH LINE (65)



AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

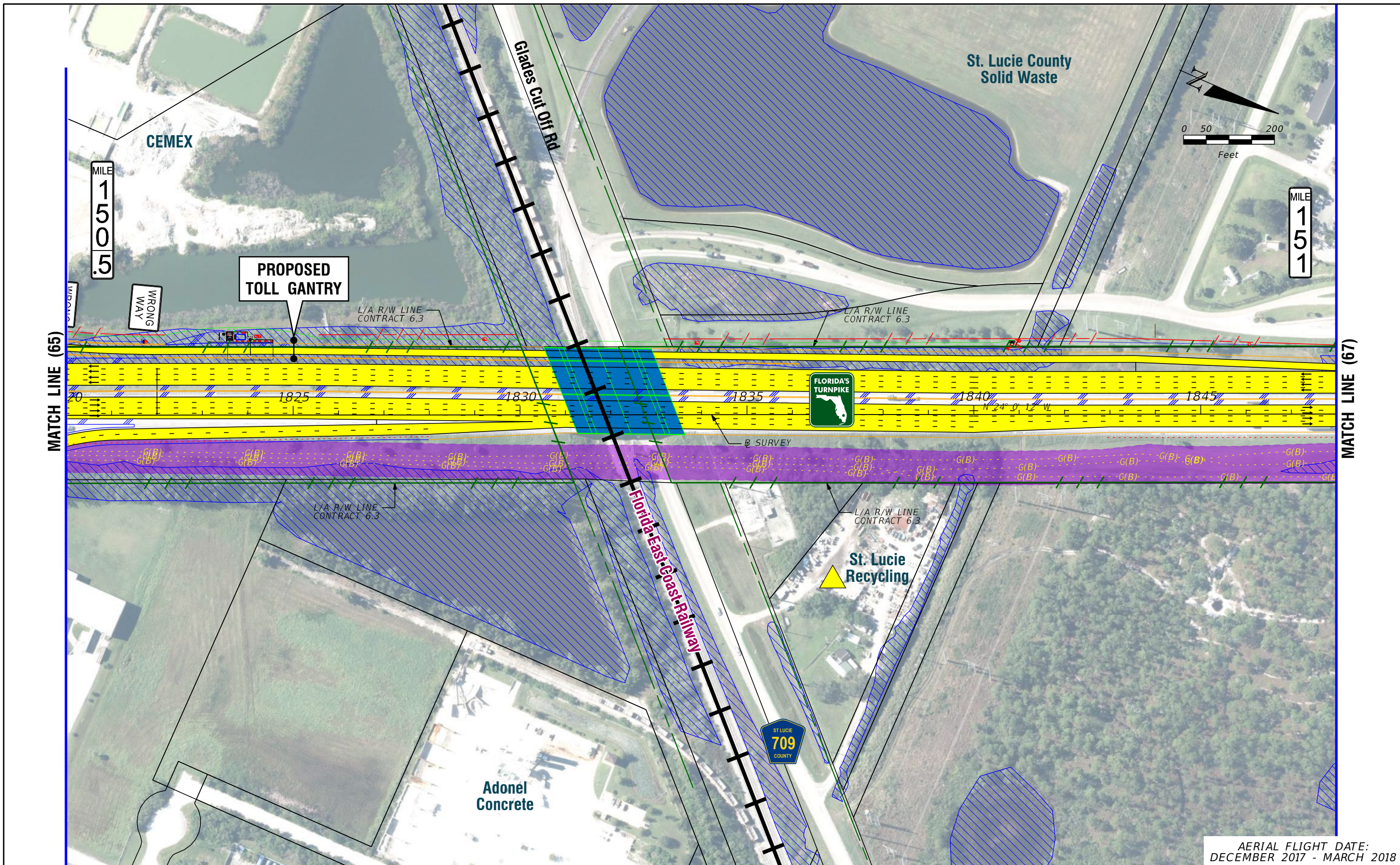
H. W. LOCHNER, INC.  
4350 W. CYPRESS ST., SUITE 800  
TAMPA, FLORIDA 33607  
FBPR CERTIFICATE OF AUTH. #894

FLORIDA'S TURNPIKE (SR 91) WIDENING  
FROM JUPITER (INDIANTOWN ROAD/SR 706)  
TO FT. PIERCE (OKEECHOBEE ROAD/SR 70)  
PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY  
FINANCIAL PROJECT IDENTIFICATION (FPID) NUMBER: 423374-1

**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**65B**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

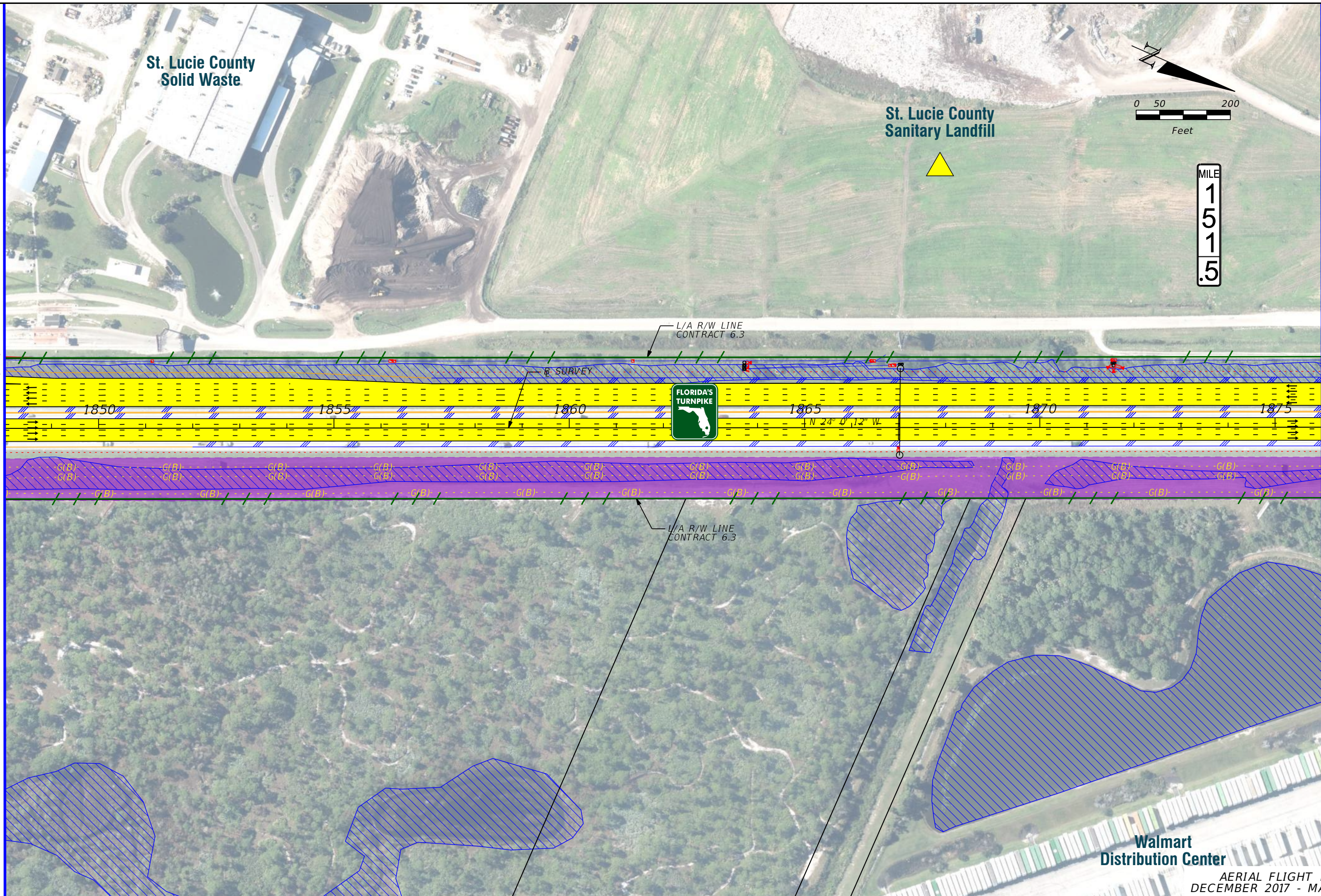
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**66**





MATCH LINE (66)

MATCH LINE (68)

LEGEND

—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION		
—	RAILROAD						

NOTE: ROW = RIGHT OF WAY

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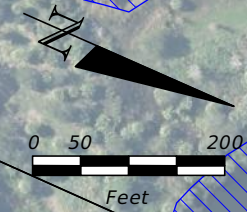
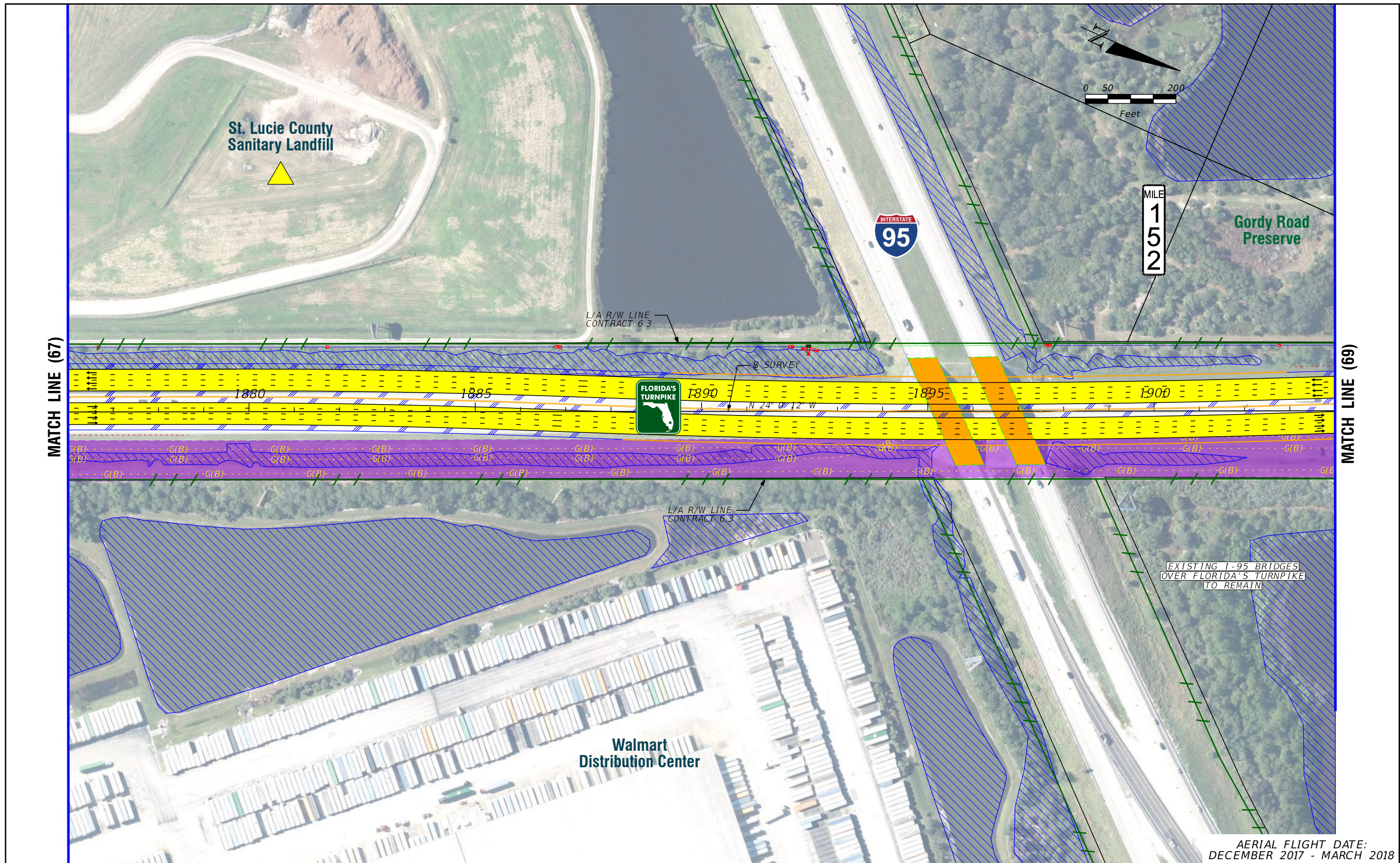
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**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

SHEET NO.  
 67

AERIAL FLIGHT DATE:  
 DECEMBER 2017 - MARCH 2018





MILE  
1  
5  
2



L/A R/W LINE  
CONTRACT 6.3

B SURVEY

N 24° 0' 12" W

L/A R/W LINE  
CONTRACT 6.3

EXISTING I-95 BRIDGES  
OVER FLORIDA'S TURNPIKE  
TO REMAIN

Walmart  
Distribution Center

St. Lucie County  
Sanitary Landfill

AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	—	
—	RAILROAD	—	—	

NOTE: ROW = RIGHT OF WAY

—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE
—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)
—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)
—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION
—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION

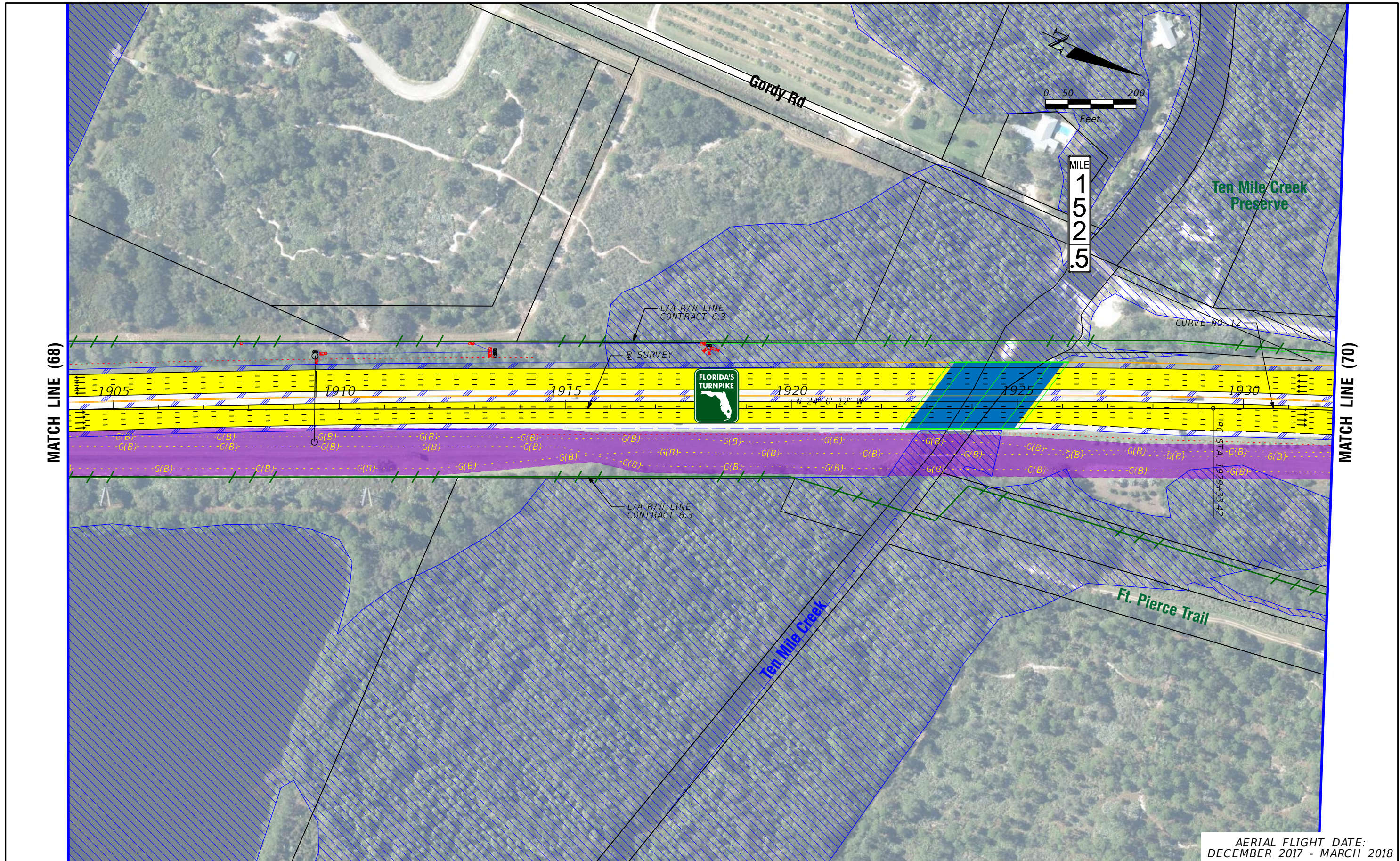
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
68





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

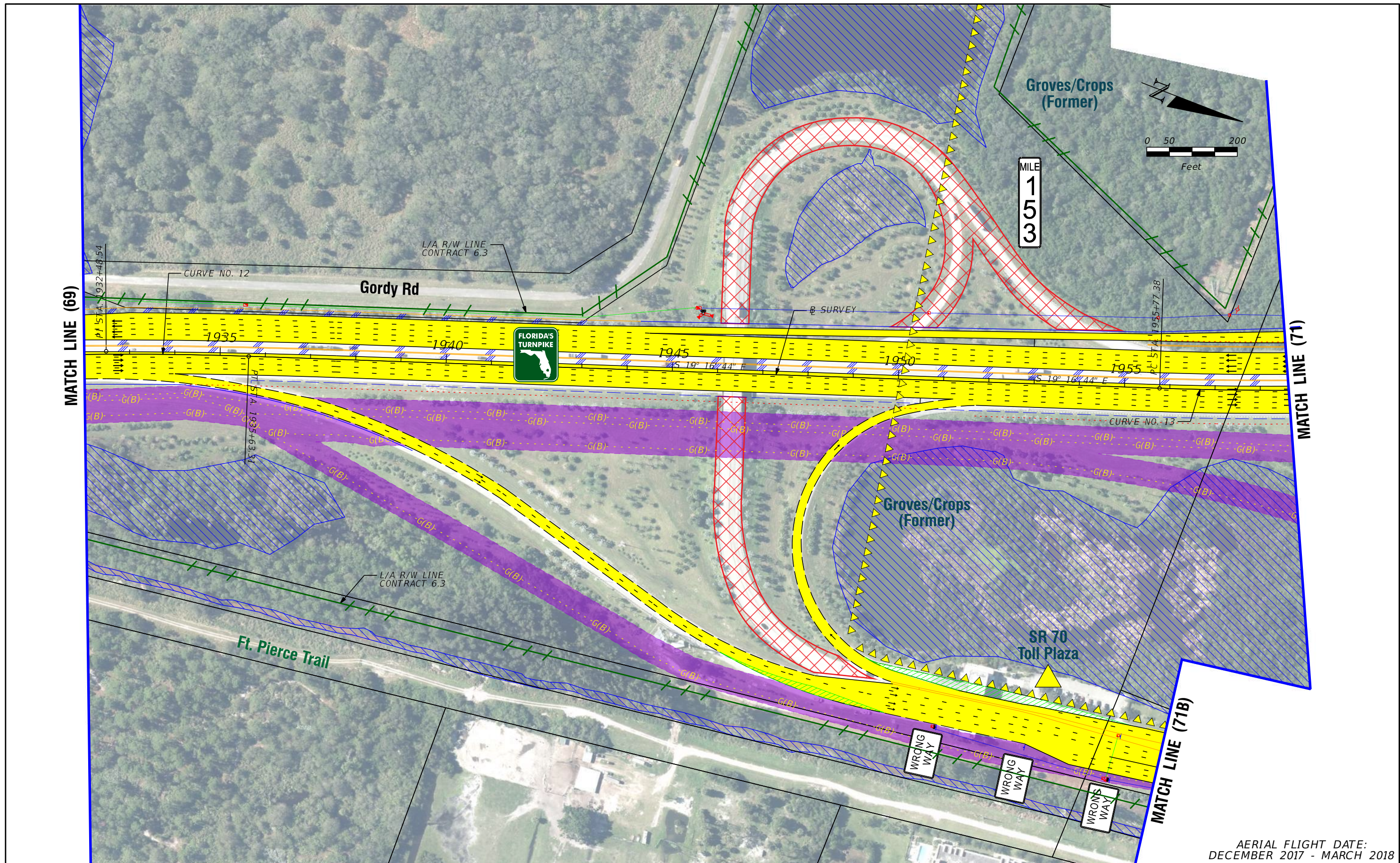
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**69**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD		
—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE
—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)
—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)
—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH
—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION
—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION

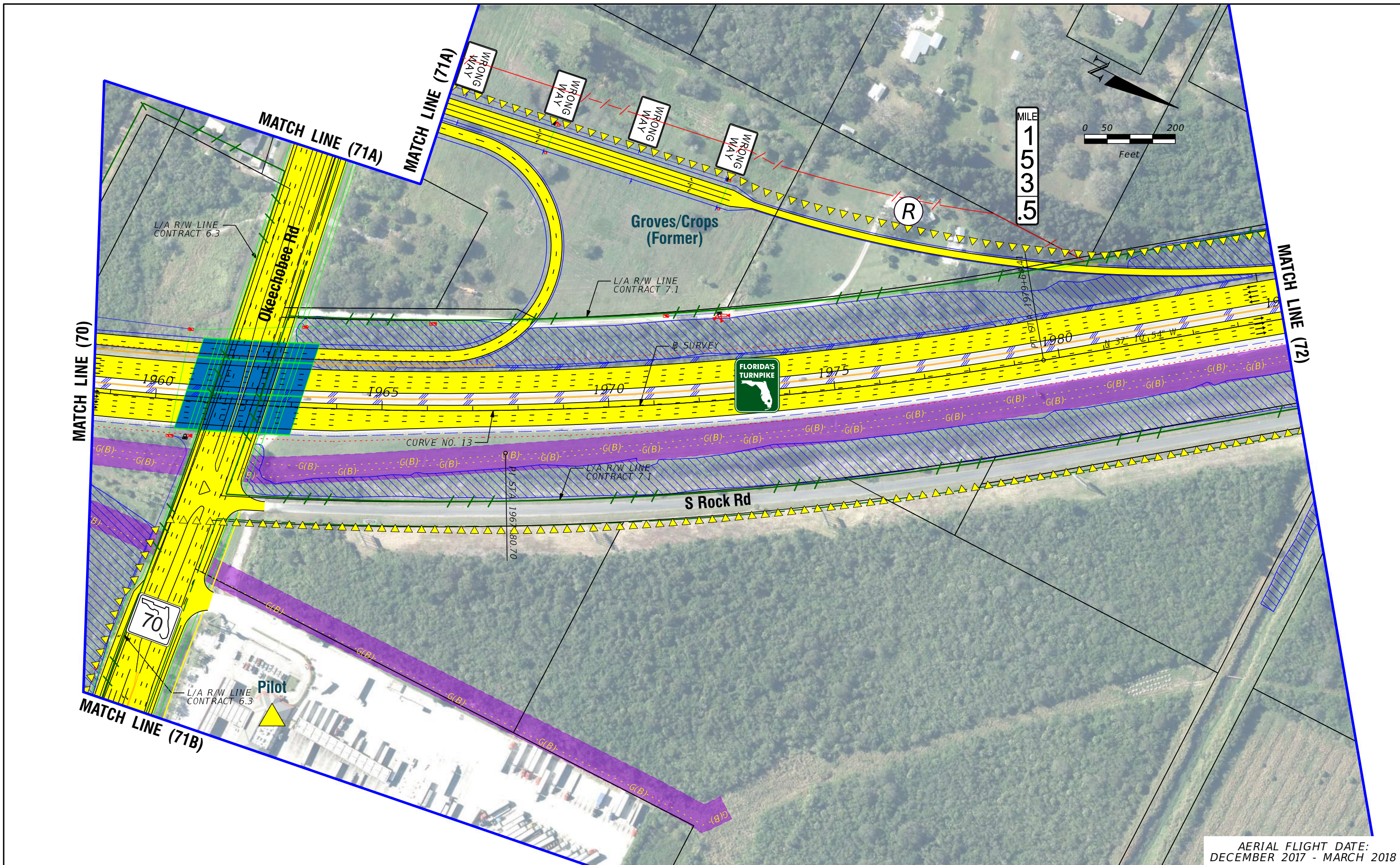
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**70**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

<b>LEGEND</b>	—	PROPERTY LINES	—	PROPOSED ROW NOISE BARRIER	—	EXISTING BRIDGE	—	PROPOSED ITS
	—	TRAFFIC BARRIER WALL	—	PROPOSED SHOULDER NOISE BARRIER	—	OTHER PROJECTS (BRIDGE)	—	POTENTIAL NEW TRAFFIC SIGNAL
	—	EXISTING ROW LINE	—	PAVEMENT REMOVAL	—	OTHER PROJECTS (ROADS)	—	EXISTING TRAFFIC SIGNAL
	—	EXISTING LIMITED ACCESS ROW LINE	—	WETLANDS/OTHER SURFACE WATERS	—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	PROPOSED ROADWAY	—	POTENTIAL BUSINESS RELOCATION	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES	
—	POTENTIAL LIMITED ACCESS ROW LINE	—	PROPOSED BRIDGE	—	POTENTIAL RESIDENTIAL RELOCATION	—		
—	RAILROAD							

NOTE: ROW = RIGHT OF WAY

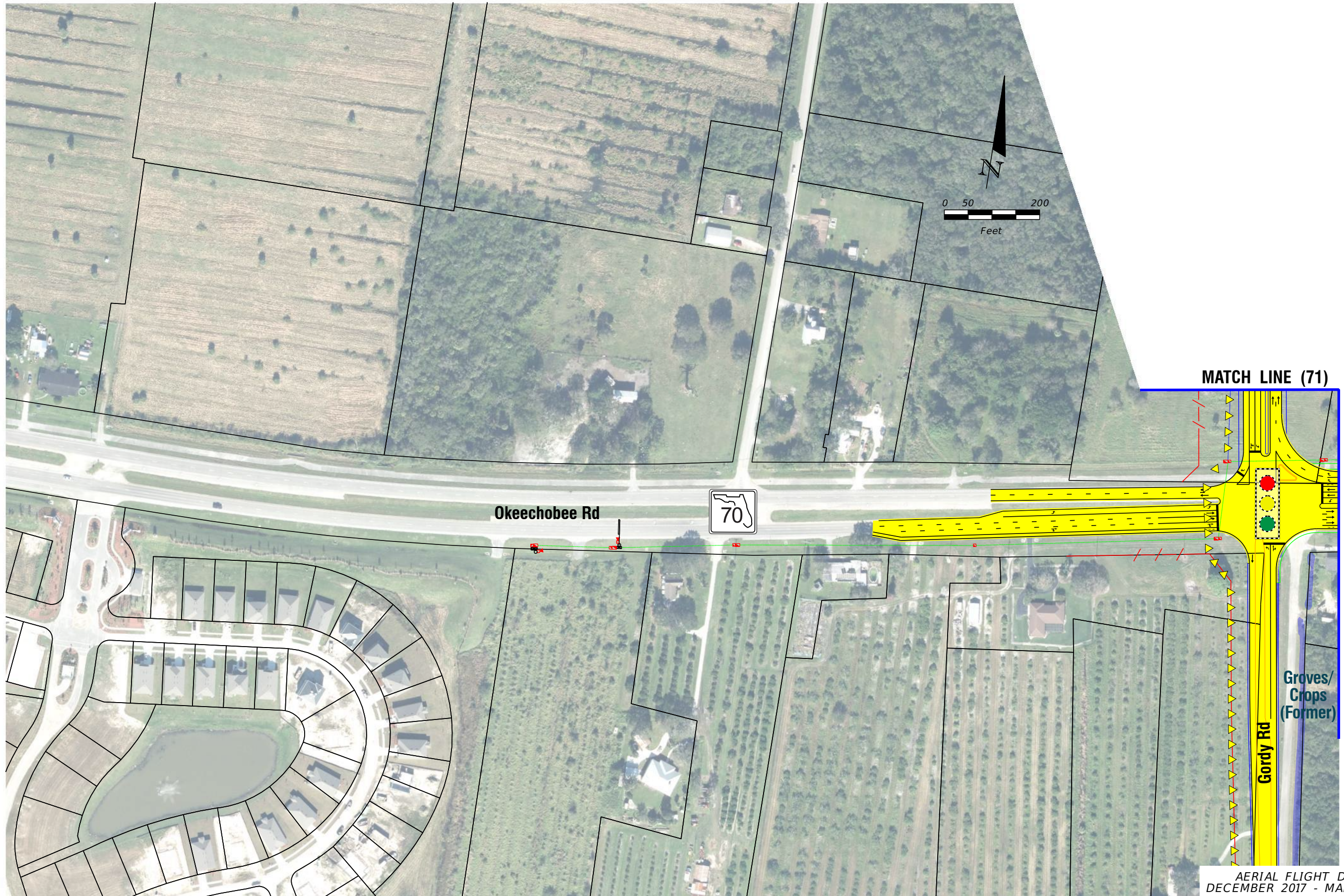
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**71**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

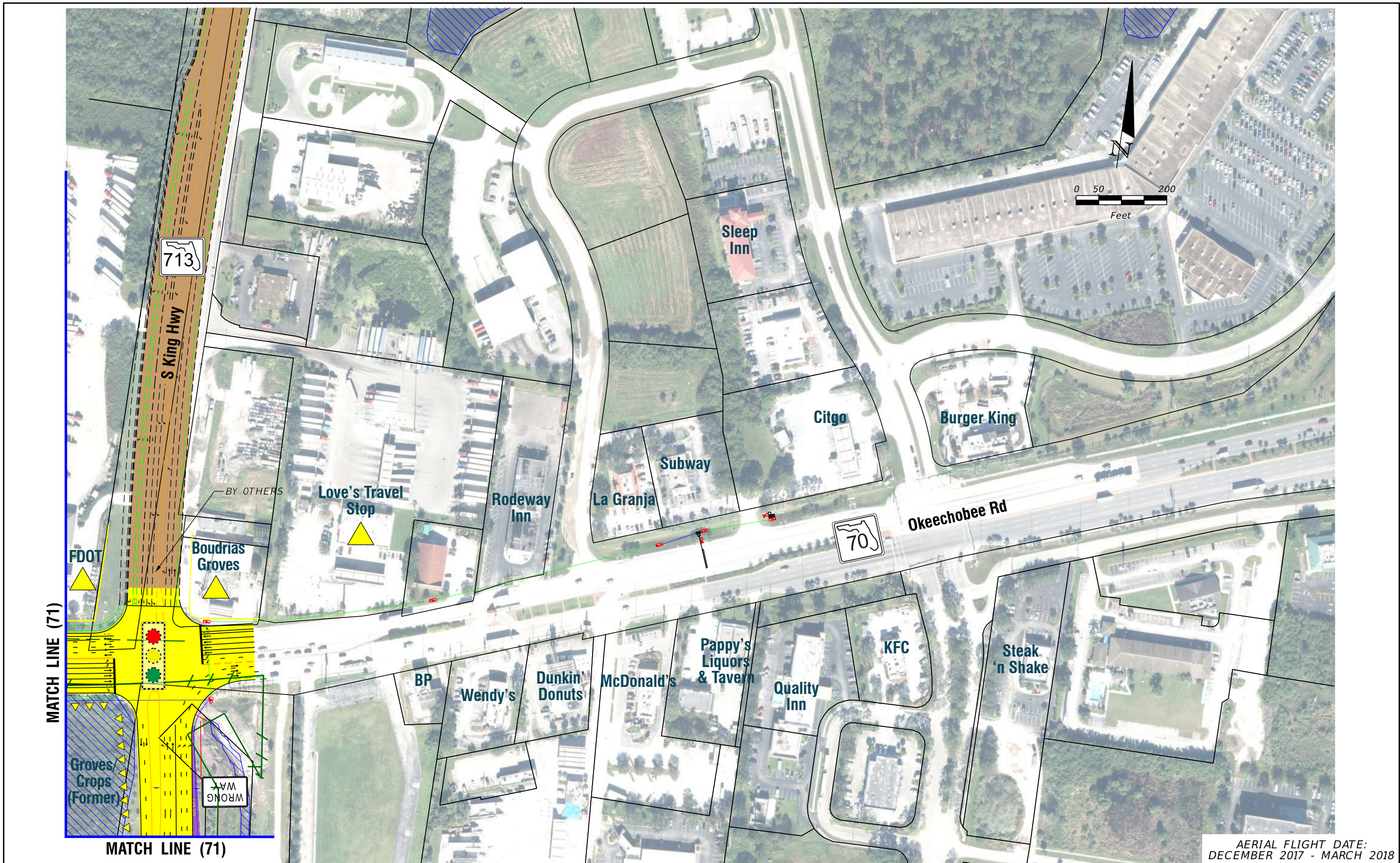
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FINANCIAL PROJECT IDENTIFICATION (FPID) NUMBER: 423374-1

**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**71A**





AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

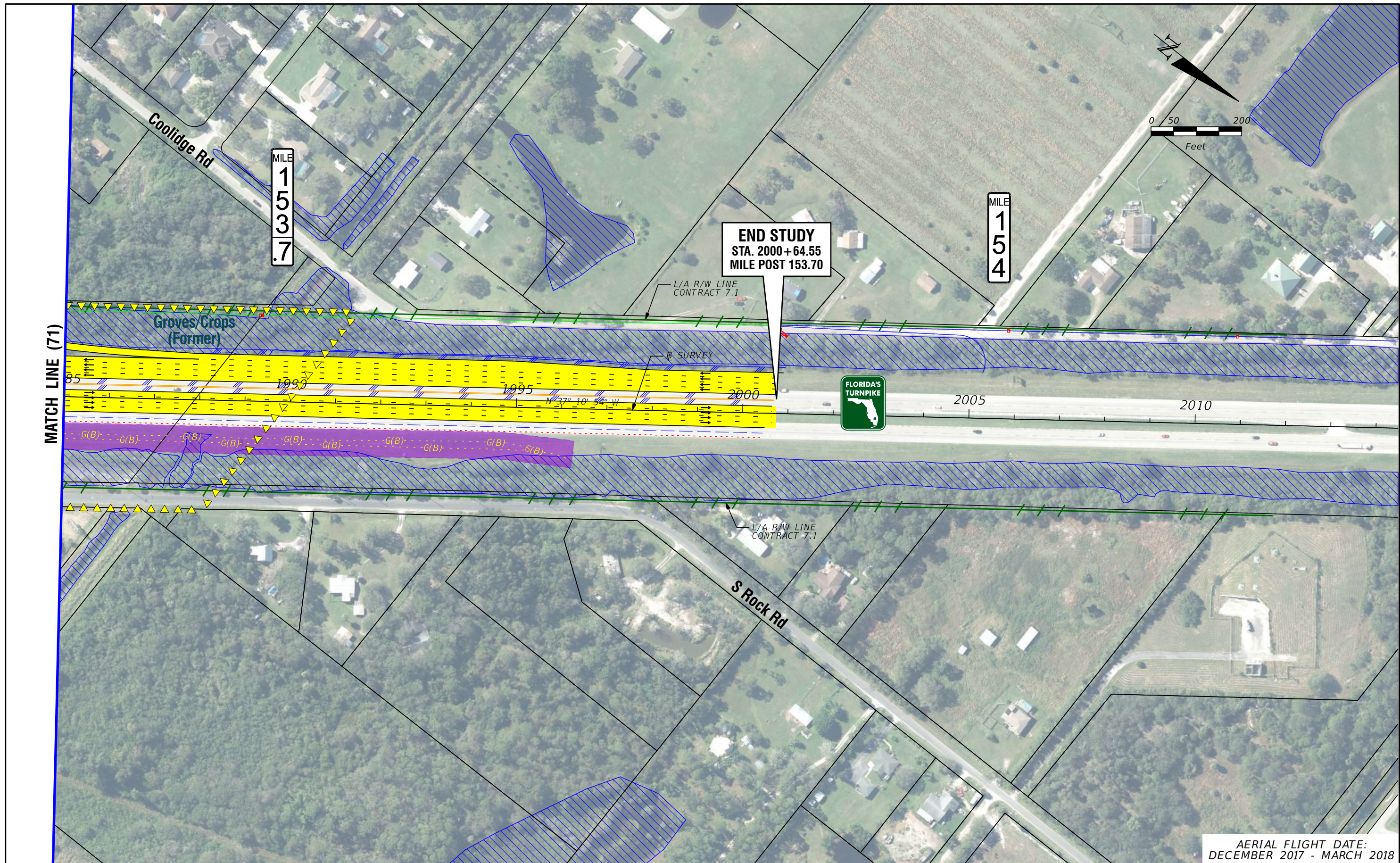
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**FL TPK PD&E WIDENING  
PREFERRED ALTERNATIVE  
CONCEPT PLAN**

SHEET NO.  
**71B**





**END STUDY**  
 STA. 2000+64.55  
 MILE POST 153.70



MILE  
153.7

MILE  
154

MATCH LINE (71)

Groves/Crops  
(Former)



S Rock Rd

AERIAL FLIGHT DATE:  
DECEMBER 2017 - MARCH 2018

LEGEND		NOTE: ROW = RIGHT OF WAY	
—	PROPERTY LINES	—	PROPOSED ITS
—	TRAFFIC BARRIER WALL	—	POTENTIAL NEW TRAFFIC SIGNAL
—	EXISTING ROW LINE	—	EXISTING TRAFFIC SIGNAL
—	EXISTING LIMITED ACCESS ROW LINE	—	TOLL GANTRY
—	POTENTIAL ROW LINE	—	POTENTIAL MEDIUM/HIGH CONTAMINATION SITES
—	POTENTIAL LIMITED ACCESS ROW LINE	—	
—	RAILROAD	—	
—	PROPOSED ROW NOISE BARRIER	—	
—	PROPOSED SHOULDER NOISE BARRIER	—	
—	PAVEMENT REMOVAL	—	
—	WETLANDS/OTHER SURFACE WATERS	—	
—	PROPOSED ROADWAY	—	
—	PROPOSED BRIDGE	—	
—	EXISTING BRIDGE	—	
—	OTHER PROJECTS (BRIDGE)	—	
—	OTHER PROJECTS (ROADS)	—	
—	FLORIDA GAS TRANSMISSION (FGT) SPECIFIED WIDTH	—	
—	POTENTIAL BUSINESS RELOCATION	—	
—	POTENTIAL RESIDENTIAL RELOCATION	—	

H. W. LOCHNER, INC.  
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**FL TPK PD&E WIDENING  
 PREFERRED ALTERNATIVE  
 CONCEPT PLAN**

SHEET NO.  
72



**APPENDIX D**  
*SHPO Coordination Letters*





## Florida Department of Transportation

RON DESANTIS  
GOVERNOR

Florida's Turnpike Enterprise  
P.O. Box 613069, Ocoee, FL 34761  
407-532-3999

KEVIN J. THIBAUT, P.E.  
SECRETARY

September 16, 2020

Dr. Timothy Parsons, Director  
Florida Division of Historical Resources  
Department of State, R.A. Gray Building  
500 South Bronough Street  
Tallahassee, FL 32399-0250

Attn: Transportation Compliance Review Program

RE: **Cultural Resource Assessment Survey Report**  
**Florida's Turnpike (SR 91) Mainline Widening PD&E Study**  
**From Jupiter (Indiantown Road) to Ft. Pierce (Okeechobee Road/SR 70)**  
**(MP 117 to 153.7)**  
**Palm Beach, Martin and St. Lucie Counties, Florida**  
**FDOT Financial ID No.: 423374-1-22-01**

Dear Dr. Parsons:

This Cultural Resource Assessment Survey (CRAS) is part of a Project Development and Environment (PD&E) Study for Florida's Turnpike Enterprise (FTE) to develop and evaluate potential capacity improvements to the existing Florida's Turnpike (SR 91) corridor in Palm Beach, Martin and St. Lucie Counties, Florida. In 2017, Lochner engaged Janus Research to conduct a CRAS for the Florida's Turnpike (SR 91) Mainline Widening PD&E Study from Jupiter (Indiantown Road) at Mile Post (MP) 117 to Fort Pierce (Okeechobee Road/SR 70) at MP 153.7, a distance of approximately 36.7 miles. The project consists of the widening of Florida's Turnpike from four to eight lanes by adding two general toll lanes in each direction.

The purpose of this CRAS was to locate and evaluate archaeological and historic resources within the area of potential effect (APE) and to assess their eligibility for inclusion in the *National Register of Historic Places* (National Register) according to the criteria set forth in 36 CFR Section 60.4. The archaeological APE consisted of the footprint of all existing and proposed ROW associated with the project. The APE for historic resources included parcels directly adjacent to the edge of the proposed project improvements for a distance of up to 150 feet.

This project is state-funded and this assessment complies with the revised Chapter 267, *Florida Statutes (F.S.)*; and the standards embodied in the FDHR's *Cultural Resource Management Standards and Operational Manual* (February 2003), and Chapter 1A-46 (*Archaeological and Historical Report Standards and Guidelines*), Florida Administrative Code. In addition, this report was prepared in conformity with standards set forth in Part 2, Chapter 8 (Archaeological and Historical Resources) of the *FDOT Project Development and Environment Manual*. All work also conforms to professional guidelines set forth in the Secretary of the Interior's Professional Qualification Standards (48 Federal Register [FR] 44716, as amended and annotated). Principal Investigators meet the Secretary of the Interior's Professional



Qualification Standards (48 FR 44716) for archaeology, history, architecture, architectural history, or historic architecture.

No archaeological sites were identified within the archaeological APE. Background research, a pedestrian survey, and extensive subsurface testing conducted during the current survey determined that large portions of the archaeological APE have been subjected to land modification associated with the construction of Florida's Turnpike and its numerous interchanges, the surrounding development, and the installation of underground utilities. While subsurface testing was not feasible within areas of existing hardscape or underground utility corridors, 156 shovel tests were excavated throughout the archaeological APE, and no cultural material was identified within any of the tests. The results of the current survey confirmed a low potential for encountering intact archaeological resources within the archaeological APE.

The historic resources survey resulted in the identification of 31 previously recorded historic resources consisting of 15 canal segments (8MT1316, 8MT1517, 8MT1518, 8MT1591, 8MT1596, 8SL1809, 8SL3043, 8SL3120, 8SL3150-8SL3156), eight road segments (8MT1532, 8MT1597, 8MT1600/8SL1789, 8SL1657, 8SL1658, 8SL3114, 8SL3149, 8SL3158), one railroad segment (8SL3014), six buildings (8MT536, 8MT537, 8MT1667, 8SL1787, 8SL1788, 8SL3102), and one bridge (8SL3282) within the current project area. The survey also resulted in the identification of nine newly identified buildings (8MT1733-8MT1736, 8SL3329, 8SL3330, 8SL3332-8SL3334), 28 newly identified bridges (8MT1737-8MT1748, 8MT1749/8SL3335, 8MT1750, 8MT1751, 8PB16298, 8SL3336-8MT3347), and a newly identified segment of Florida's Turnpike (8PB19629) within the project area.

All but two of the historic linear resources (St. Lucie Canal (8MT1316) and Florida East Coast (FEC) Railway – Lake Harbor Branch (8SL3014)) have either been previously determined ineligible for the National Register or are considered ineligible for the National Register based on the results of this survey. Portions of the St. Lucie Canal (8MT1316) and FEC Railway - Lake Harbor Branch (8SL3014) have been determined eligible in other segments outside of the current project APE. Field survey reveals that these two resources maintain their historic associations and integrity within the current project APE and are therefore considered eligible for the National Register within the project APE.

All six of the previously recorded historic buildings (8MT536, 8MT537, 8MT1667, 8SL1787, 8SL1788, and 8SL3102) have been previously determined by the State Historic Preservation Officer (SHPO) to be National Register–ineligible. Field survey and historical research did not reveal any additional information to re-evaluate these resources, and therefore, they remain ineligible for the National Register.

Field survey resulted in the identification of nine newly identified historic buildings within the current project APE. All nine buildings (8MT1733-8MT1736; 8SL3329, 8SL3330, 8SL3332-8SL3334) are of a common style and type in South Florida and lack historical significance. Therefore, they are ineligible for individual listing in the National Register under Criteria A, B, C, or D.

The 29 bridges associated with Florida's Turnpike that are located within the current project APE are common types that were popular in the mid-twentieth century and were built throughout the US. Therefore, they are considered ineligible for the National Register under Criteria A, B, C, or D, individually and as a resource group.

Finally, there are no potential historic districts within, or partially within, the current project APE.



The CRAS Report for the Florida's Turnpike (SR 91) Widening PD&E Study from Jupiter to Fort Pierce is provided your review and comment. If you have any questions or need assistance, please contact me at 407.264.3301 or via email at [Philip.Stein@dot.state.fl.us](mailto:Philip.Stein@dot.state.fl.us). Thank you for your continued assistance on FTE projects.

Sincerely,



Philip Stein  
Environmental Administrator  
Florida's Turnpike Enterprise

Enclosures (*via temporary digital submittal policy*): PDF of signed transmittal letter, PDF of full CRAS report, collection of unflattened PDFs for the site file forms, collection of digital photographs associated with the historic resource site file forms, unflattened PDF version of the survey log, and a ZIP file containing the files making up the Shapefile for the survey area.

CC: Bill Howell, Lochner  
James Pepe, Janus Research

The Florida State Historic Preservation Officer (SHPO) finds the attached Cultural Resources Assessment Survey Report complete and sufficient and  concurs/  does not concur with the recommendations and findings provided in this cover letter for SHPO/FDHR Project File Number 2017-0763-B. Or, the SHPO finds the attached document contains \_\_\_\_\_ insufficient information.

SHPO Comments:


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Dr. Timothy Parsons, Director  
State Historic Preservation Officer  
Florida Division of Historical Resources

October 26, 2020  
Date



**APPENDIX E**  
*Agency Coordination*





**PARTICIPANTS:** Randall Overton, Mark Easley  
**COMPANY:** US Coast Guard / KCA  
**PHONE:** 305.415.6736

**DATE/TIME:** 14 August 2017 / 11:00 am

**PROJECT:** Florida's Turnpike Widening

**SUBJECT:** USCG Bridge Permits for Loxahatchee River Bridge and Thomas B Manual Bridge at Florida's Turnpike

**NOTES:**

I contacted Randy to discuss anticipated US Coast Guard (USCG) requirements associated with the replacement of the existing Florida's Turnpike bridge structure over the Loxahatchee River and the Florida's Turnpike southbound Thomas B Manual bridge structure over the St. Lucie River.

Randy stated that the segment of the Loxahatchee River containing the Florida's Turnpike bridge structure is not tidal and is presently not used for interstate commerce. In addition, because of the Wild & Scenic River designation on that segment of the river, it was unlikely that improvements to the channel, making it useable for interstate commerce is unlikely. As a result, the USCG would likely either make a determination of no jurisdiction or provide an advanced authorization for the replacement of the structure.

Replacement of the southbound Thomas B Manual Bridge structure would require a modification to the existing bridge permit at this location. The new structure would need to, at a minimum, meet the vertical and horizontal requirements of the newer northbound structure. In addition, because the proposed replacement involves the widening of the existing roadway (i.e., addition of a new travel lane), noise impacts associated with the residential development located in the south-west quadrant of the bridge would be a major concern and would need to be assessed as part of the project's PD&E study. The residents within this segment of the project were vocal during the last PD&E study that was done at this location. In addition, the logical termini would need to be determined for the study.

Randy recommended that the USCG be the lead federal agency on the PD&E study being done for the project.



# Meeting Minutes

**Project:** FPID 423374-1-22-01

**Description:** Turnpike Mainline (SR 91) Widening PD&E from Jupiter (Indiantown Road) to Okeechobee Road (SR 70) – Palm Beach, Martin, and St. Lucie Counties

**Meeting:** SFWMD/USACE/NMFS/FDOT Pre-Application Meeting

**Date/Time:** 11/16/17 @ 11:10 am

**Location:** SFWMD HQ, West Palm Beach

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## **Attendees:**

Beverly Miller (SFWMD)  
Jason Debish (SFWMD)  
Beth Kacvinsky (SFWMD)  
Carlos de Rojas, PE (SFWMD)  
Trisha Stone (SFWMD)  
Barbara Conmy (SFWMD)  
Tarrie Ostrofsky (USACE)  
Jennifer Schull (NMFS)  
Erin Yao, PE (FTE) – by phone  
Martin Horwitz (FTE) – by phone  
Fred Gaines, PWS (Atkins/FTE)  
Liz Bartell, PE (PGA)  
Tim Polk, PE (PGA)  
Sarah Johnson (KCA)  
Bill Howell, PE (Lochner) – by phone  
Tracy Ellison, PE (Lochner) – by phone  
Jack Miller, PE (Lochner) – by phone

## **1. Background**

- a. FTE introduced the project and stated that the PD&E Study limits are Turnpike Mainline (SR 91) from Indiantown Road (SR 706) to Okeechobee Road (SR 70), MP 117 to MP 153.7.
- b. PGA stated that the project will be permitted for the future (8-lane) condition.
- c. PGA stated that the proposed future improvements include widening the mainline from two to four lanes in each direction. The two alternatives being evaluated during the PD&E Study consist of four general toll lanes in each direction or two general toll lanes and two express toll lanes in each direction. FTE plans to account for the added impervious necessary for express lanes when permitting the project, even though the express lanes may not be constructed at this time.
- d. PGA stated that the project will also include improvements to the following interchanges: Stuart (SW Martin Highway/SR 714), Becker Road, Port St. Lucie Boulevard (SR 716), and Okeechobee Road (SR 70). The PD&E will also evaluate the potential for new interchanges. The major bridges within the project limits are the Loxahatchee River and Thomas B. Manuel Bridge over the St. Lucie



Canal. The project will also include bridge improvements over several other creeks and canals.

**2. Existing Permits**

- a. Turnpike mainline is permitted from MP 137.676 to 152.610 (Permit No. 56-00912-S). SFWMD confirmed that this permit should be modified for the proposed improvements. Several other permits exist within the 37-mile project for interchanges, the service plaza, bridges, and canal protection.

**3. Water Quality**

- a. SFWMD confirmed that the required water quality volume is 2.5" over the new impervious area in areas of reconstruction and widening but clarified that full treatment of new and existing impervious should be provided, if feasible. SFWMD stated that the required water quality volume shall also include the treatment volume provided in the existing condition, whether permitted or not. PGA confirmed that the new impervious area will be calculated for the future condition.
- b. SFWMD confirmed that an additional 50% of treatment shall be provided for any direct discharge to Outstanding Florida Waters (OFWs).
- c. SFWMD confirmed that nutrient loading is required for any direct discharge to water bodies that are impaired for nitrogen (TN) or phosphorus (TP). SFWMD clarified that although Dissolved Oxygen impairment is not typically a roadway impairment, there are times that it is related to high nutrient levels.
- d. PGA stated that there is a BMAP for St. Lucie River and Estuary Basin, but FTE is a de minimus stakeholder and has not been assigned an allocation for TN nor TP.
- e. PGA stated that the Loxahatchee TMDL Planning Unit (from Indiantown Road to SE Bridge Road) will be reviewed during the PD&E phase but stated that there are no current TMDLs within the project limits.

**4. Water Quantity**

- a. SFWMD confirmed that the proposed peak discharge for the 25-year, 3-day design shall not exceed that of the existing condition.
- b. PGA stated that she was aware of the following allowable discharge rates: C-23 Canal (31.5 csm for the 10-year design frequency) and C-24 Canal (30.25 csm for the 10-year design frequency). SFWMD stated that any widening of the bridges over these canals, or the C-18 and C-25 canals, will require a right-of-way permit.
- c. C-18, C-23 and C-24 will be handled by SFWMD WPB staff, while the C-25 will be handled by SFWMD Okeechobee staff.

**5. Environmental Look Around (ELA)**

- a. PGA stated that the ELA will be started during the PD&E phase. The PD&E Team plans to coordinate with the following Special WMDs: Northern Palm Beach County Improvement District, Loxahatchee River Environmental Control District, Hobe-St. Lucie Conservancy District, and North St. Lucie River Water Control District.
- b. PGA asked whether SFWMD was aware of any regional opportunities within the project limits, such as funding a SFWMD project for nutrient removal credit, and



discussed some alternative permitting approaches that may be necessary where the project is adjacent to sensitive lands to avoid off-site ponds?

- i. The project corridor is adjacent to two miles of SFWMD-owned property and two miles of Florida Forever lands. One alternative is to make use of SFWMD-owned lands and Florida Forever acquisitions. SFWMD stated that there may be an opportunity for funding of the pepper farm restoration located on the SFWMD-owned lands (Martin County is part owner). SFWMD added that the pepper farm could also provide a potential for floodplain compensation by reconnecting Cypress Creek. SFWMD stated that there is also a plan to construct a flow through marsh on the Florida Forever land to capture agricultural discharge and provide attenuation. PGA stated that this project would also be suitable for floodplain compensation and pollutant loading reductions, and SFWMD agreed. The Florida Forever property was purchased with Comprehensive Everglades Restoration Plan (CERP) funds. SFWMD stated that there are no current opportunities for funding the flow through marsh, but there may be an opportunity for funding in the future. SFWMD indicated that there is bridge culvert system connecting the east and west sides of the Florida Forever property that is important for access and requested that the connection not be removed in the future.
  - ii. Another alternative PGA presented was to provide attenuation in the State-owned lands. PGA stated that this approach was used for the SR 710 from Martin/Palm Beach County Line to Pratt and Whitney Entrance (SFWMD Permit No. 50-04716-P), which was successfully permitted through SFWMD. The SR 710 project provided full treatment on-site, but attenuation was provided off-site in adjacent wetlands to avoid the need for off-site ponds within sensitive lands. Modeling was used to demonstrate a negligible stage increase in the wetlands and no adverse impacts to adjacent properties. SFWMD concurred.
  - iii. PGA said that another alternative that may be reviewed is the use of Bio-Sorption Activated Media (BAM) filters. SFWMD said they were not familiar with this new technology and would need more information before granting approval to use for TN reduction. PGA stated that BAM has been permitted in other water management districts and additional information would be provided if the PD&E study identifies this alternative as a recommended approach.
  - iv. PGA stated that Martin County has been implementing septic-to-sewer conversions and asked whether nutrient removal credit could be obtained by funding a similar project. SFWMD said it would need to be discussed further if the PD&E study identifies this alternative as a recommended approach.
  - v. SFWMD does not know of any additional opportunities and reminded FTE that water quality and quantity aspects will need to stay within the basins impacted.
- c. PGA stated that the PD&E will look at potential joint-use opportunities with the adjacent golf course and the City of Port St. Lucie.

## 6. Floodplain



- a. PGA stated that there are several floodways within the project limits: Roebuck Creek, Danforth Creek, Bessy Creek, North Fork St. Lucie, and Tenmile Creek.
- b. PGA stated that the FEMA floodplains within the project limits are riverine and compensation would be provided for any impacts to these floodplains; however, a portion of the project is downstream of a SFWMD weir control structure. Floodplain impacts at this location would not require compensation, as they are considered tidal.
- c. SFWMD added that the proposed improvements shall not create a backwater increase nor reduce the cross-sectional area at the bridges.

## 7. Wetlands/Surface Waters

- a. KCA presented the types of wetlands anticipated within the project limits: freshwater marsh, forested wetlands, shrub wetlands, reservoirs, natural rivers, and drainage ditches and canals.
- b. SFWMD indicated that impacts to wetlands associated with OFWs need to show Avoidance and Minimization. Potential mitigation options exist with restoration/enhancement of OFWs and associated wetlands.
- c. KCA stated that the following mitigation options will be reviewed: Loxahatchee Mitigation Bank, Bluefield Ranch Mitigation Bank, R.G. Reserve Mitigation Bank, and DuPuis Reserve (Martin County). A cumulative impact analysis may be necessary based on the location of impacts and mitigation bank service area. SFWMD added that credits may be low or out at the R.G. Reserve Mitigation Bank.
- d. COE agreed with approach.

## 8. Protected Species

- a. KCA stated that no species-specific surveys have been conducted.
- b. KCA stated that there is a potential for the following protected species:
  - i. Federal
    1. Eastern indigo snake
    2. Wood stork
    3. Crested caracara\*
    4. Snail kite\*
    5. Manatee\*
    6. Wood stork (5 CFAs)
    7. Red-cockaded woodpecker\*
    8. Florida scrub-jay\*
    9. Florida grasshopper sparrow\*  
(\* project in species consultation area)
  - ii. State
    1. Wading birds
    2. Rookery at Okeechobee Road (SR 70) Toll Plaza
    3. Florida sandhill crane
    4. Gopher tortoise
    5. Southeastern American kestrel
    6. Sherman's fox squirrel
  - iii. Other
    1. Osprey
    2. Bald eagle



- c. FTE stated that the Florida Bonneted Bat Consultation Area (CA) may have recently changed, and the USFWS is in the process of expanding the CA and removing the focal areas, but it is currently still in a draft form.
- d. NMFS inquired about tidal systems and Essential Fish Habitat. Assumed to be minimal. FTE will research further and address in detail during design.

#### **9. Loxahatchee Wild and Scenic River**

- a. KCA discussed the project's Loxahatchee River involvement.
- b. Implemented under the Wild and Scenic River Act
  - i. SFWMD confirmed that Section 7a approval is needed.
- c. National Park Service is lead federal agency.
- d. SFWMD/FDEP develop and administer management plan – coordinate with Beth Kacvinsky
- e. Supported by Loxahatchee River Management Coordinating Council (25 members)
  - i. Three Federal Agencies
  - ii. Eight State Agencies
  - iii. Nine Local Agencies
  - iv. Five Non-Governmental Organizations
- f. Extends from southern end of Jonathan Dickinson State Park to southern end of Riverbend Park (Martin and Palm Beach Counties)
- g. Road crosses scenic segment of river.
- h. Addresses Impacts:
  - i. Free Flow Nature
  - ii. Water Quality
  - iii. Remarkable Values – (scenic, recreational, geological, fish & wildlife, historical, cultural)
- i. SFWMD added that Cypress Creek connects to the Loxahatchee River, but it is not considered part of wild and scenic river. KCA stated that the location of the Loxahatchee River within this PD&E project is considered scenic only (not wild).

#### **10. Cultural Resources**

- a. KCA stated that a CRAS will be completed as part of this PD&E.

#### **11. Permits and Approvals**

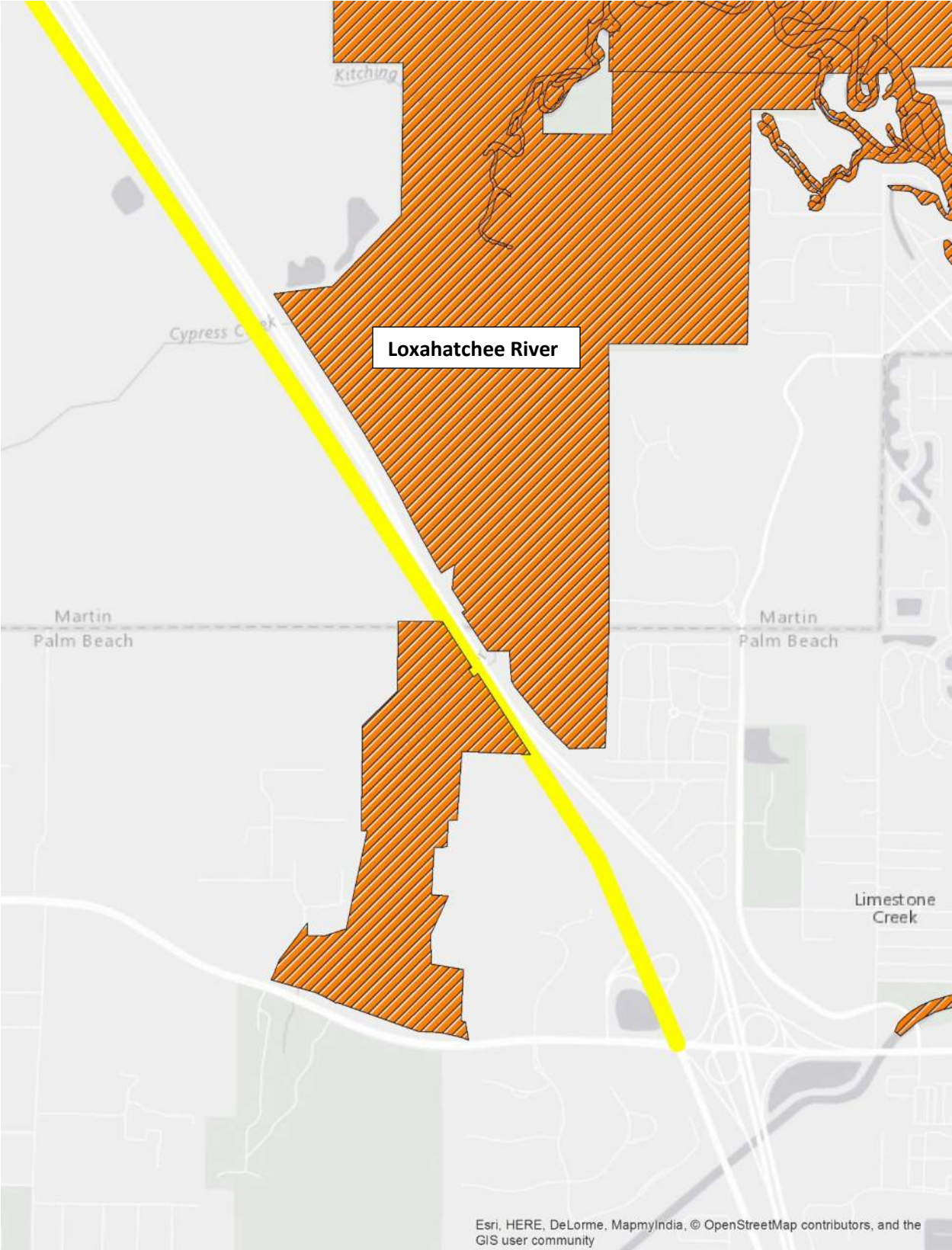
- a. KCA stated that the following permits and approvals are anticipated:
  - i. USACE – Section 404 Dredge and Fill Permit
  - ii. USACE – Section 408 Alteration of a USACE Civil Works Project
    - 1. SFWMD said that a Section 408 will be needed for the C-23 canal.
  - iii. US Coast Guard - General Bridge Act of 1946 (33 USC 525)
  - iv. NPS – Section 7a Wild and Scenic Rivers Act Approval
  - v. SFWMD – Environmental Resource Permit
  - vi. SFWMD - Right-of-Way Occupancy Permit
    - 1. SFWMD said that a Right-of-Way Occupancy permit will be necessary for the following canals: C-18 (if within the project limits), C-23 upstream of weir, C-24 downstream of weir, and C-25 downstream of weir.
  - vii. FDEP - Sovereign Submerged Lands Easements



1. This will be submitted with the ERP, and SFWMD will process.
  - viii. FDEP – NPDES – Obtained by Construction Contractor
  - ix. FWC - Gopher Tortoise Relocation Permit
  - x. FWC - Incidental Take Permit (Permitting requirements to be coordinated w/ FWC)
- b. FTE added that the ETDM number for this project is #14295.



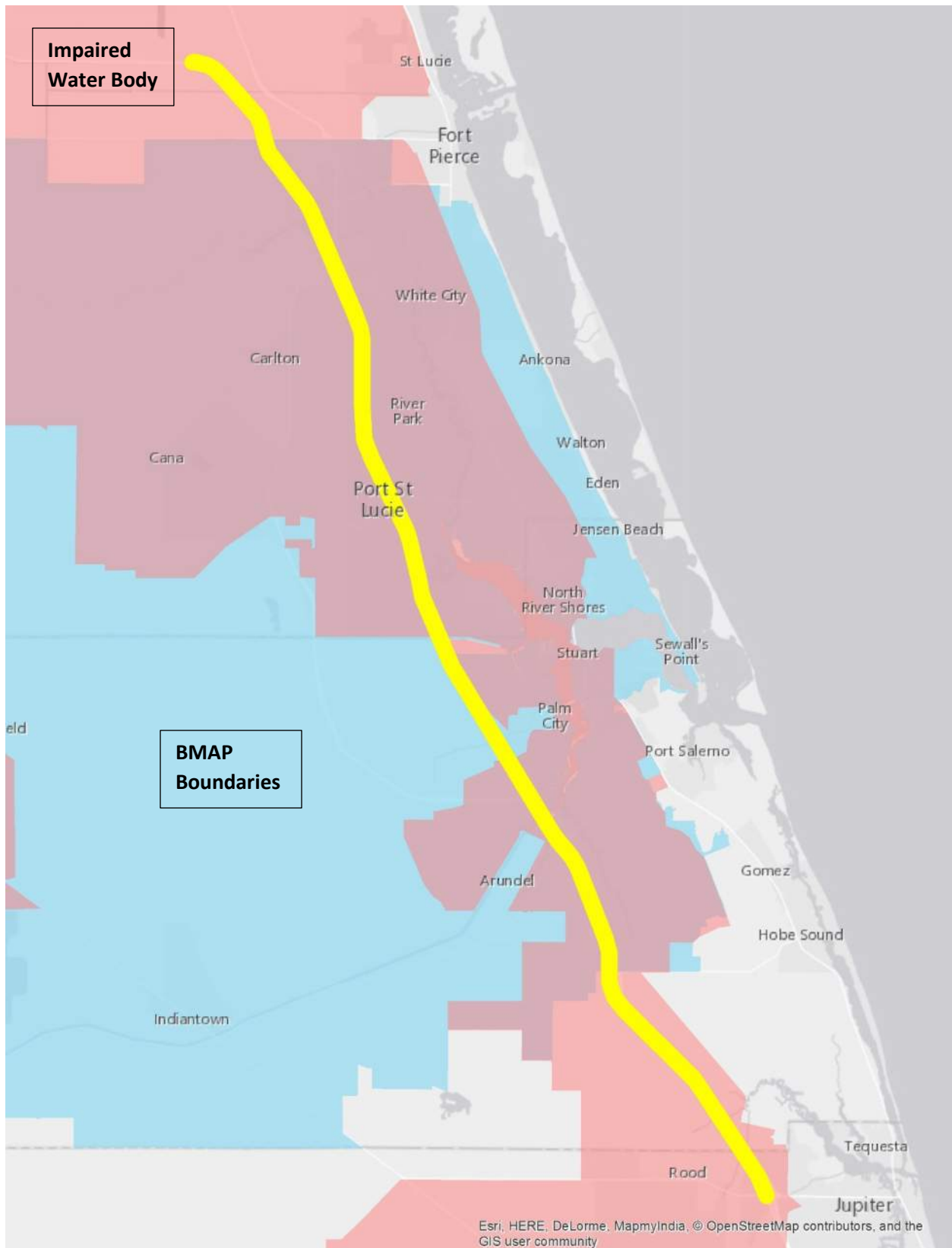
# Exhibit 1: Outstanding Florida Waters



*Florida's Turnpike Mainline (SR 91) from Jupiter (Indiantown Rd) to Okeechobee Road (SR 70)  
Palm Beach, Martin, and St. Lucie Counties  
FPID 423374-1-22-01*



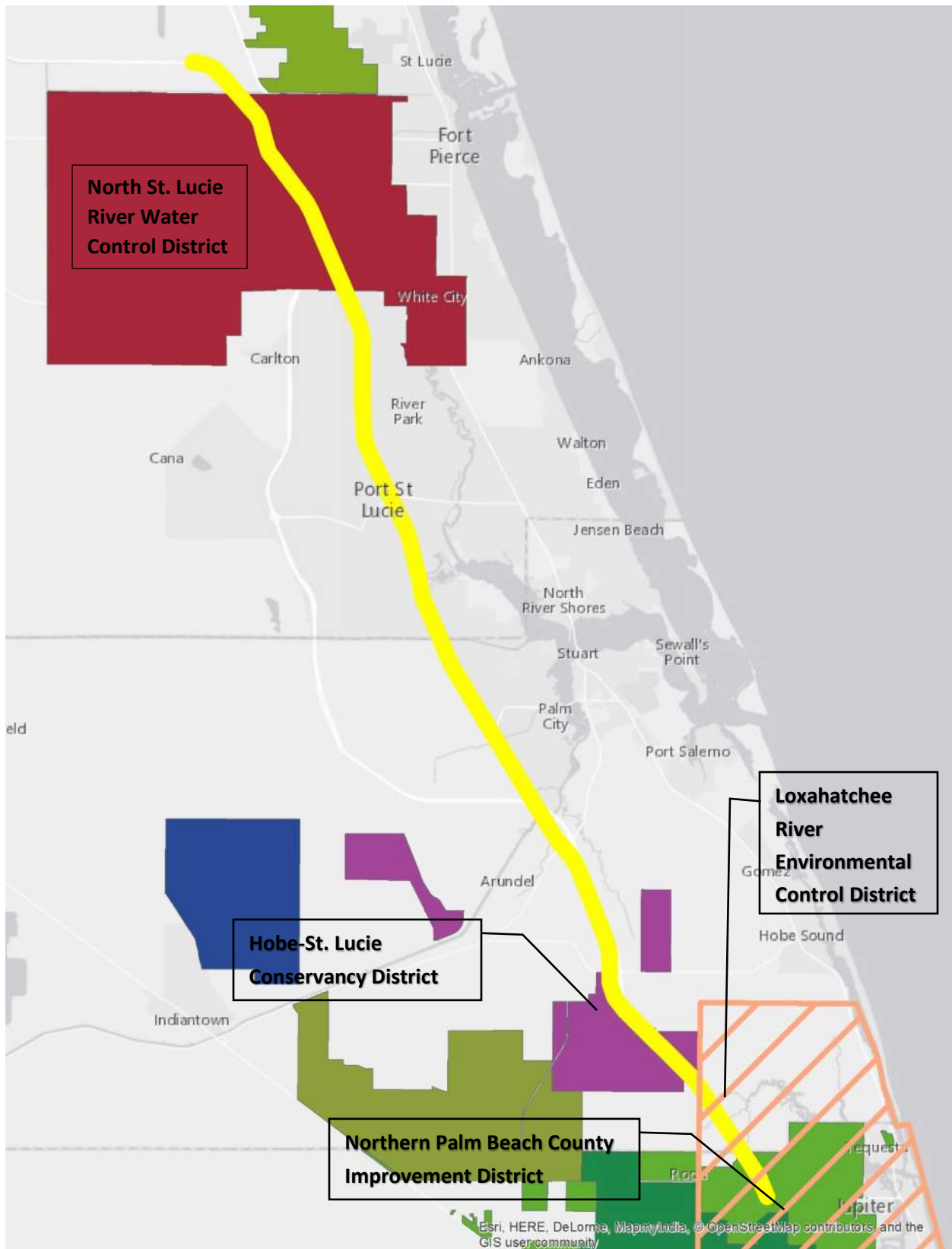
## Exhibit 2: Impaired Water Body and BMAP



*Florida's Turnpike Mainline (SR 91) from Jupiter (Indiantown Rd) to Okeechobee Road (SR 70)  
Palm Beach, Martin, and St. Lucie Counties  
FPID 423374-1-22-01*



### Exhibit 3: Special Water Management Districts



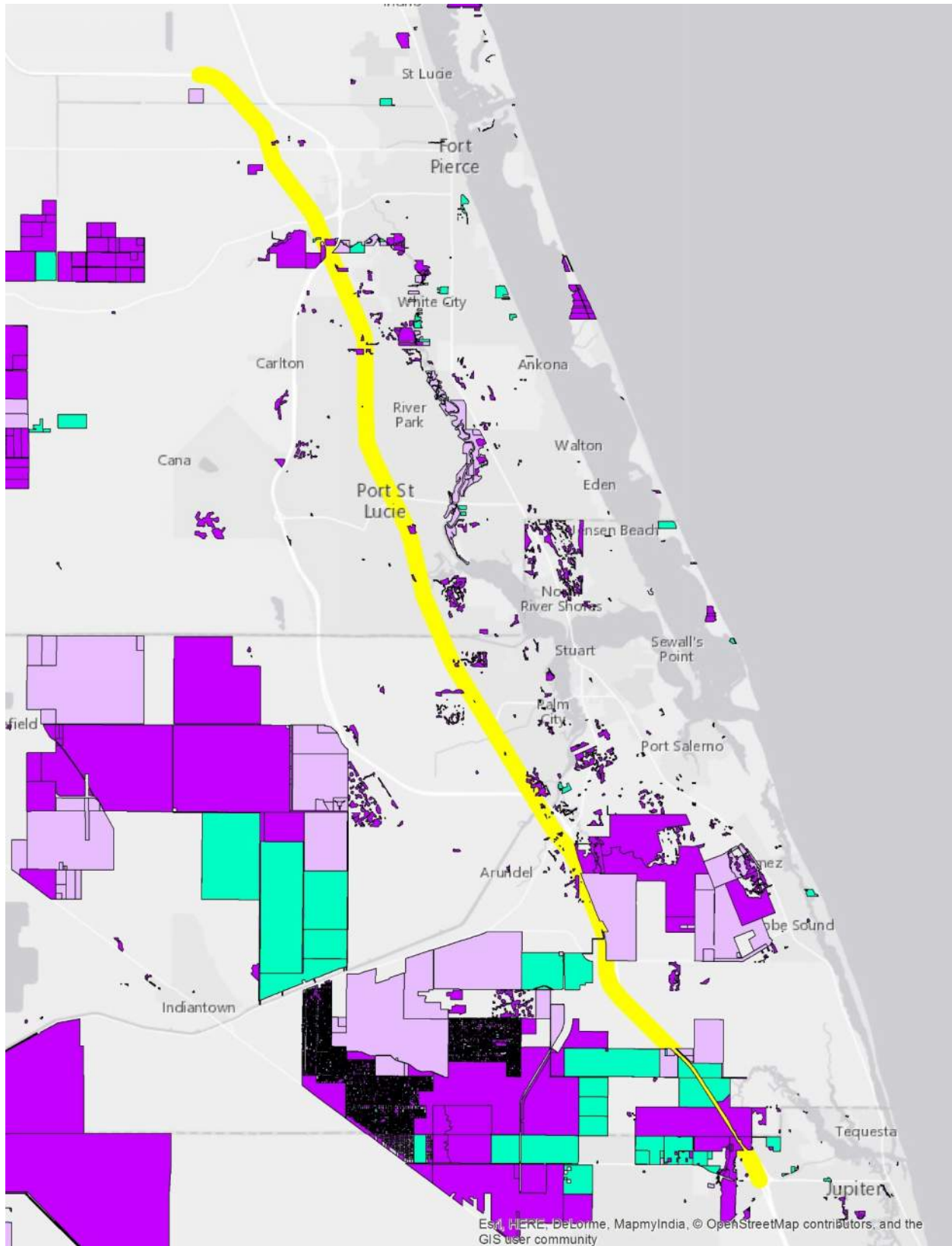
Florida's Turnpike Mainline (SR 91) from Jupiter (Indiantown Rd) to Okeechobee Road (SR 70)  
Palm Beach, Martin, and St. Lucie Counties  
FPID 423374-1-22-01







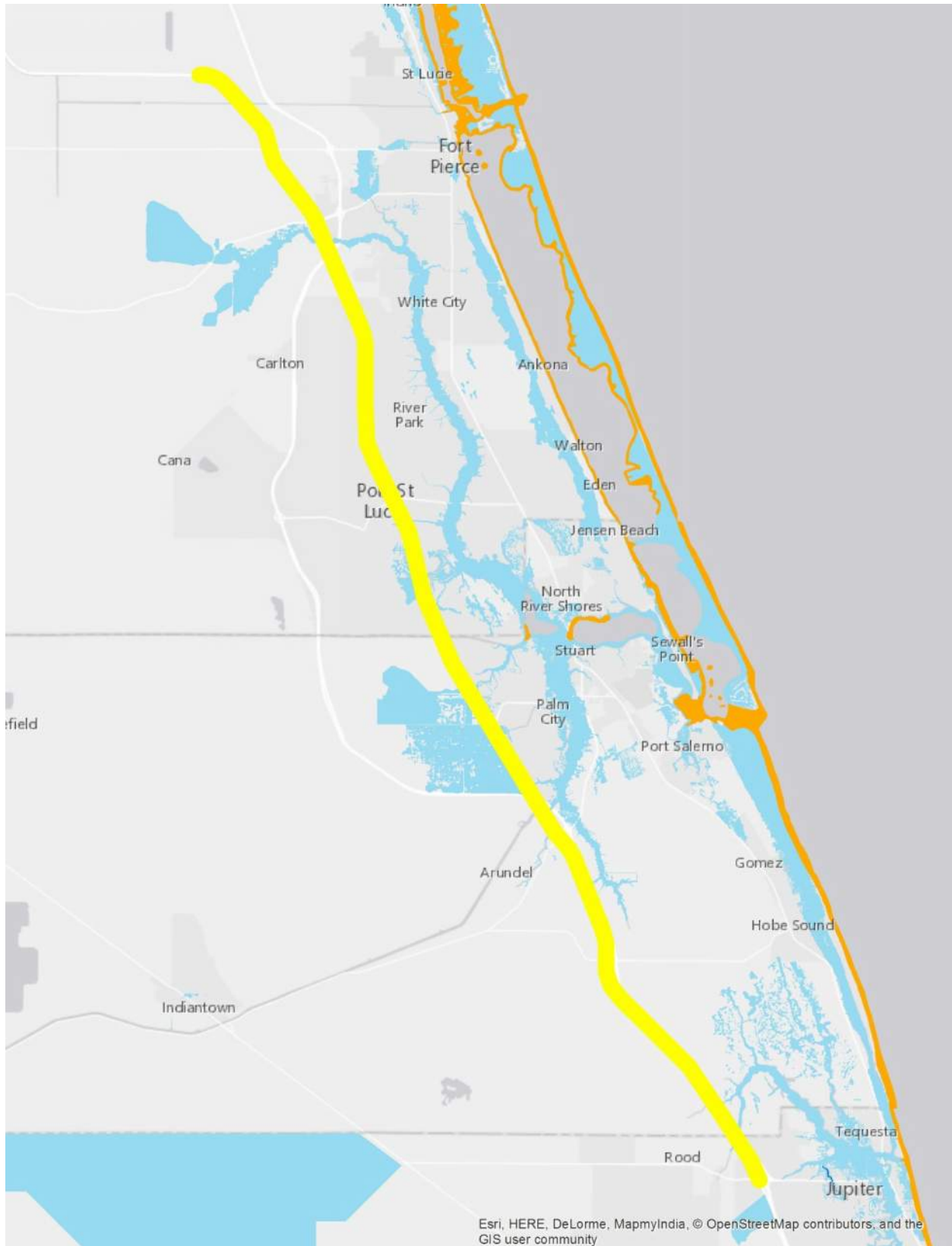
## Exhibit 5: Florida Forever Acquisitions



Florida's Turnpike Mainline (SR 91) from Jupiter (Indiantown Rd) to Okeechobee Road (SR 70)  
Palm Beach, Martin, and St. Lucie Counties  
FPID 423374-1-22-01



## Exhibit 6: FEMA Floodplains



*Florida's Turnpike Mainline (SR 91) from Jupiter (Indiantown Rd) to Okeechobee Road (SR 70)  
Palm Beach, Martin, and St. Lucie Counties  
FPID 423374-1-22-01*



**From:** Overton, Randall D CIV <Randall.D.Overton@uscg.mil>  
**Sent:** Monday, December 4, 2017 2:57 PM  
**To:** Mark Easley  
**Subject:** RE: Turnpike PD&E from Jupiter to Ft. Pierce (FPID 423374-1) - Loxahatchee River Bridge Crossing

Mark,

There will be no Coast Guard Bridge Permit required for the Turnpike crossing of the Loxahatchee River (Lat/Long 26.954317, -80.165520).

Please let me know if you have questions regarding this determination.

Thank you,  
Randy

-----Original Message-----

From: Mark Easley [<mailto:Mark.Easley@kisingercampo.com>]

Sent: Friday, December 01, 2017 1:50 PM

To: Overton, Randall D CIV

Subject: [Non-DoD Source] Turnpike PD&E from Jupiter to Ft. Pierce (FPID 423374-1) - Loxahatchee River Bridge Crossing

Randy,

Some time ago we discussed the Florida's Turnpike Crossing of the Loxahatchee River and USCG permitting requirements. During that discussion, I stated that the crossing was within the area of the Wild and Scenic River section of the Loxahatchee River. Based on your discussions, you stated that the crossing would probably be either "no permit needed" or an "advanced authorization" relative to the USCG permit requirements.

I was wondering if you ever had a chance to look into that?

Please give me a call if you would like to discuss.

Thanks,

ME



KCA Logo

Mark Easley  
Senior Project Manager - Environmental Services

Email: [Mark.Easley@kisingercampo.com](mailto:Mark.Easley@kisingercampo.com) <<mailto:Mark.Easley@kisingercampo.com>>

Work: 813.871.5331 ext 4144

201 N. Franklin St., Suite 400, Tampa, FL 33602

CONFIDENTIALITY NOTE: This communication may be privileged and confidential. It should not be disseminated to others. If received in error, please immediately reply that you have received this communication in error and then delete it. Thank you.



# Loxahatchee River Environmental Control District Coordination Mtg

Turnpike Mainline (SR 91)  
Widening PD&E Study from  
Jupiter to Fort Pierce

## Florida's Turnpike (SR 91) Widening PD&E Study from Jupiter (Indiantown Road) to Okeechobee Road (SR 70) (FPID#: 423374-1-22-01)

Palm Beach, Martin and St. Lucie Counties

July 2, 2020

Brian Ribaric *BR*

### 1. Introductions

- a. Loxahatchee River Environmental Control District (LRECD)  
Albrey Arrington, PhD  
Kris Dean, PE
- b. Florida's Turnpike Enterprise (FTE) and GEC  
Henry Pinzon, PE – FTE  
Rax Jung, PhD, PE – FTE  
Philip Stein – FTE  
Annemarie Hammond – FTE  
Brian Ribaric, PE – Atkins  
Doug Zang, AICP – Atkins  
Adriana Kirwan, PE – HNTB  
Fred Gaines, PWS – Atkins
- c. Lochner and PGA  
Bill Howell, PE – Lochner  
Liz Bartell, PE - PGA

Note: Items in Green are Notes in addition to the agenda topics.

### 2. Project Overview

- a. Evaluating the potential widening of the Turnpike Mainline (SR 91) from four to eight lanes from Indiantown Rd (SR 706) to Okeechobee Rd (SR 70)
- b. Evaluating potential interchange reconfigurations
- c. Identifying stormwater management and ROW needs to meet FDOT and permitting agency requirements
- d. Loxahatchee River is an OFW and Wild and Scenic River
- e. Conducting Environmental Look Around efforts to identify joint-use and nonconventional stormwater opportunities
  - i. Florida Forever Lands
    - 1. Pepper Farms and Flow-Through Marsh
  - ii. Martin County Septic-to-Sewer Conversions

### 3. Stormwater Requirements

- a. Will meet SFWMD treatment and attenuation requirements
- b. Loxahatchee River is classified as an Outstanding Florida Water (OFW)
  - i. No anticipated direct discharge to Lox River
  - ii. Direct discharge to OFWs will require an additional 50% treatment
- c. No anticipated direct discharge to WBID 3230, which is impaired for Nutrients (algal mats)
  - i. Direct discharge may require nutrient removal

### 4. Reclaimed Water Supply Opportunities

- a. LRECD is interested in storage and supply
  - i. There are opportunities for new stormwater management facilities or retrofits to provide reclaimed water
- b. There is reclaimed infrastructure west of I-95
- c. There is a pump station near I-95/SR 91 interchange
  - i. Limited to 12 million gallons per day
- d. There is significant demand for reclaimed water in the area, particularly the golf courses

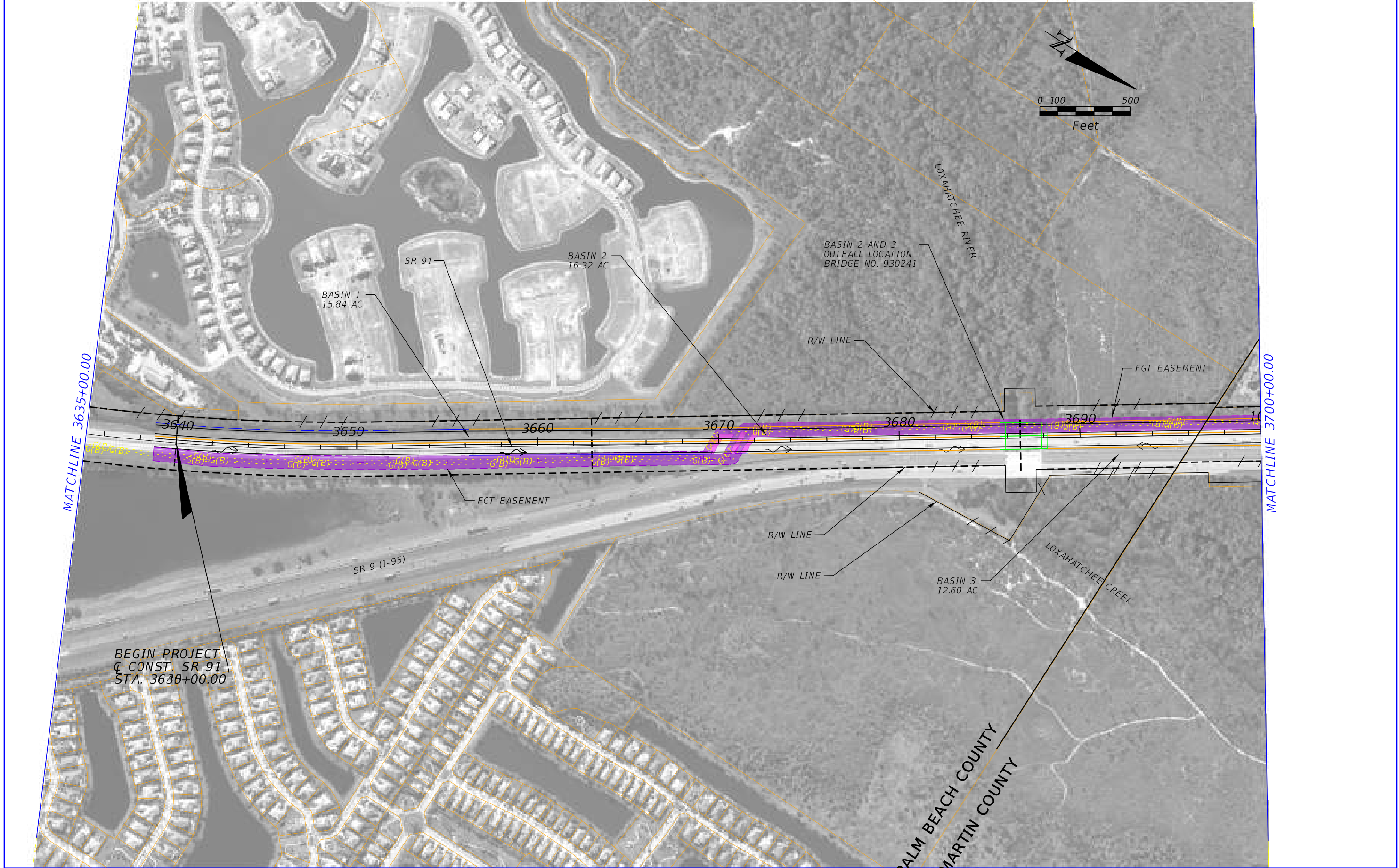


- i. LRECD is the regional provider of reclaimed water
  - e. Stormwater harvesting is another alternative for providing reclaimed water. LRECD suggested looking into staging the interchange lakes at the highest level possible to promote stormwater harvesting options.
- 5. Additional Discussion/Questions
  - a. LRECD is concerned with the following and believes that highway stormwater runoff can potentially address some of these concerns:
    - i. eutrophication of the river and would like to prevent nutrients from discharging to river, ii. the alternative of collocating aquifer storage and recovery (ASR) wells with storage or retention areas, LRECD willing to participate in the cost of ASR.
    - iii. saltwater intrusion due to wellfield pumping
    - iv. highway runoff may not need to be treated if blending with reclaimed water or other sources depending on storage and use.
  - b. FTE stated that currently there is no funding for future projects associated with the PD&E, which could be a good thing as it provides an opportunity to continue discussions with the various stakeholders.
  - c. LRECD offered to meet as future projects are determined to continue general discussion or get into more specific details.

#### ACTION ITEMS

- *Schedule future coordination meetings for future projects as necessary.*
- *Meeting Notes.*





REVISIONS	
DATE	DESCRIPTION

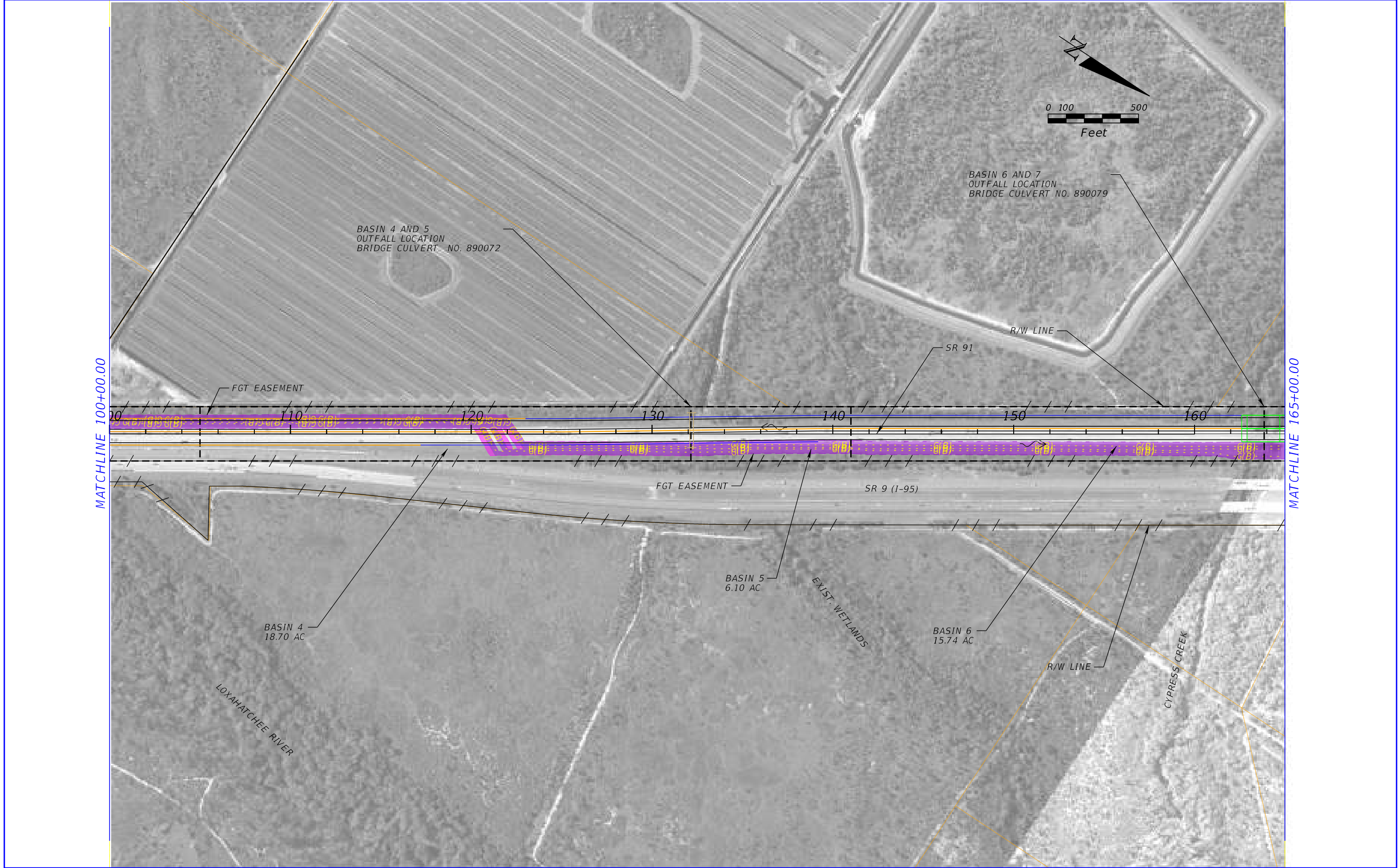
PATEL, GREENE AND ASSOCIATES, LLC  
 280 W. Canton Ave. Suite 400  
 Winter Park, Florida 32789  
 Elizabeth M. Bartell, PE #69125

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 91	PALM BEACH	423374-1-22-01

**DRAINAGE MAP**  
**POST-DEVELOPMENT**

SHEET NO.  
**E-18**





REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

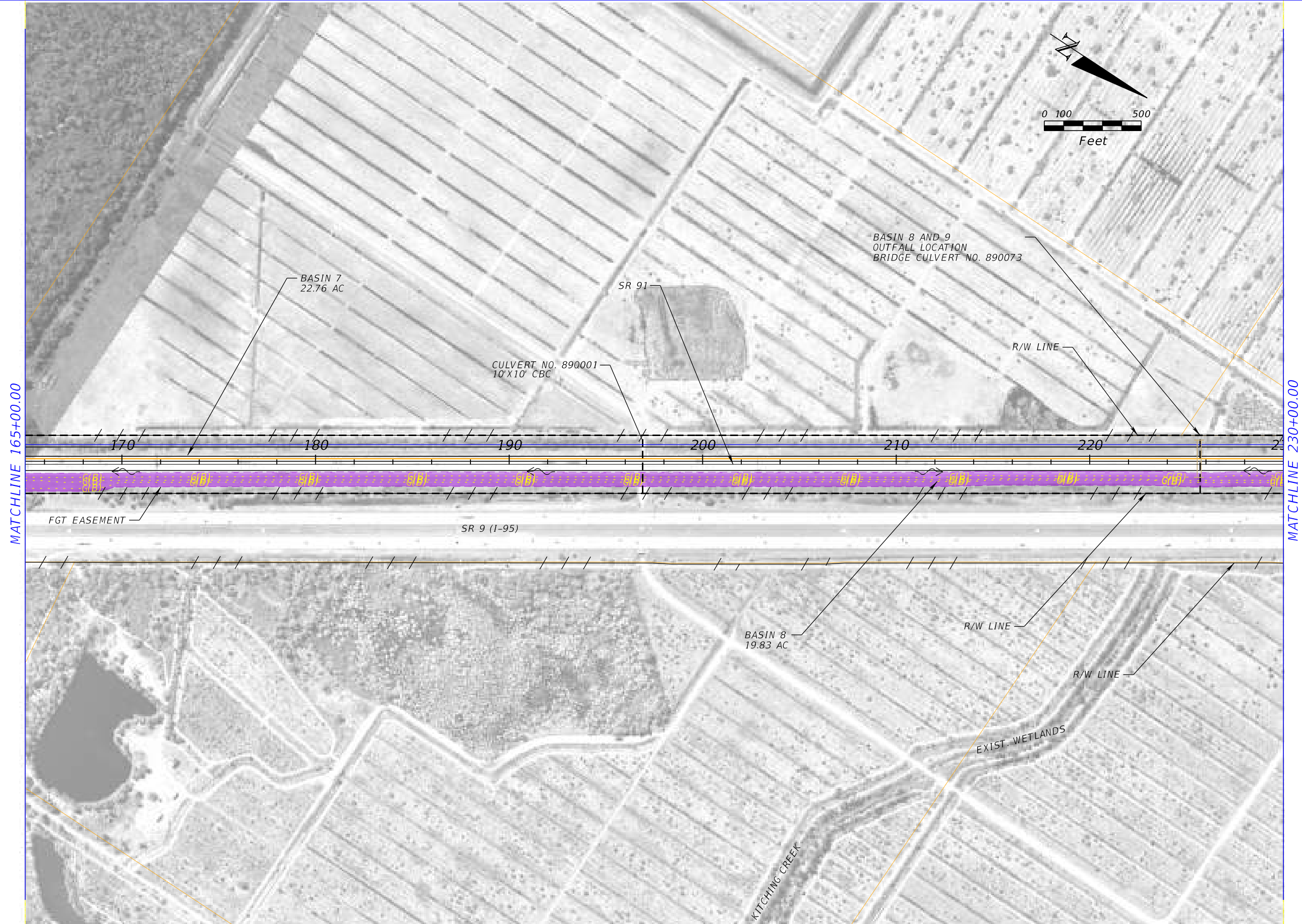
PATEL, GREENE AND ASSOCIATES, LLC  
 280 W. Canton Ave. Suite 400  
 Winter Park, Florida 32789  
 Elizabeth M. Bartell, PE #69125

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 91	MARTIN	423374-1-22-01

**DRAINAGE MAP**  
**POST-DEVELOPMENT**

SHEET NO.  
**E-19**





REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

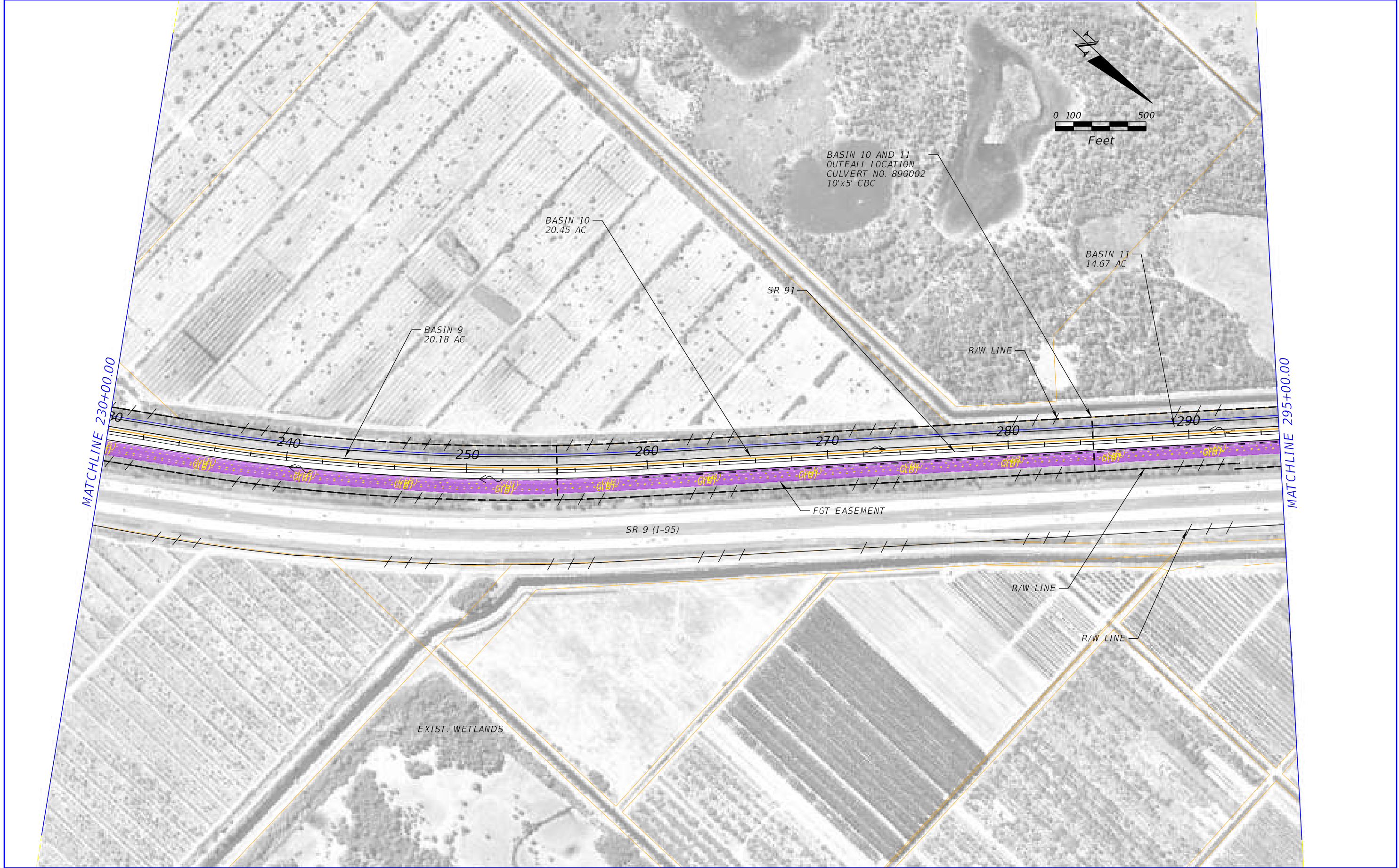
PATEL, GREENE AND ASSOCIATES, LLC  
 280 W. Canton Ave. Suite 400  
 Winter Park, Florida 32789  
 Elizabeth M. Bartell, PE #69125

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 91	MARTIN	423374-1-22-01

**DRAINAGE MAP**  
**POST-DEVELOPMENT**

SHEET NO.  
**E-20**





REVISIONS	
DATE	DESCRIPTION

PATEL, GREENE AND ASSOCIATES, LLC  
 280 W. Canton Ave. Suite 400  
 Winter Park, Florida 32789  
 Elizabeth M. Bartell, PE #69125

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 91	MARTIN	423374-1-22-01

**DRAINAGE MAP**  
**POST-DEVELOPMENT**

SHEET NO.  
**E-21**



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**FPID Number:** 423374-1-22-01

**Project Description:** Turnpike Mainline (SR 91) Widening PD&E from Jupiter to Ft. Pierce

**Meeting Name:** Loxahatchee River Environmental Control District Coordination Meeting

**Date:** Click or tap to enter a date.

**Location:** Go-To Meeting Link:

Call-In Number:

Access Code:

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## 1. Introductions

- a. Loxahatchee River Environmental Control District
- b. Florida's Turnpike Enterprise (FTE) and GEC
- c. Lochner and PGA

## 2. Project Overview

- a. Evaluating the potential widening of the Turnpike Mainline (SR 91) from four to eight lanes from Indiantown Rd (SR 706) to Okeechobee Rd (SR 70)
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- b. Loxahatchee River is classified as an Outstanding Florida Water (OFW)
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  - ii. Direct discharge to OFWs will require an additional 50% treatment
- c. No anticipated direct discharge to WBID 3230, which is impaired for Nutrients (algal mats)
  - i. Direct discharge may require nutrient removal

## 4. Reclaimed Water Supply Opportunities

## 5. Additional Discussion/Questions



MEETING NOTES

# North St. Lucie River Water Control District Coordination Meeting

Turnpike Mainline (SR 91)  
 Widening PD&E Study from  
 Jupiter to Fort Pierce

## Florida's Turnpike (SR 91) Widening PD&E Study from Jupiter (Indiantown Road) to Okeechobee Road (SR 70) (FPID#: 423374-1-22-01)

Palm Beach, Martin and St. Lucie Counties

July 2, 2020

Brian Ribaric *BR*

### 1. Introductions

- a. North St. Lucie River Water Control District (NSLRWCD)
  - Patrick Helms, PE – AECOM
  - Katherine Caricchio, PE – AECOM
- b. Florida's Turnpike Enterprise (FTE) and GEC
  - Henry Pinzon, PE - FTE
  - Brian Ribaric, PE – Atkins
  - Rax Jung, PhD, PE - FTE
  - Doug Zang, AICP – Atkins
  - Philip Stein – FTE
  - Adriana Kirwan, PE – HNTB
  - Annemarie Hammond – FTE
  - Fred Gaines, PWS - Atkins
- c. Lochner and PGA
  - Bill Howell, PE – Lochner
  - Liz Bartell, PE - PG

**Note: Items in Green are Notes in addition to the agenda topics.**

### 2. Project Overview provided by FTE

- a. Evaluating the potential widening of the Turnpike Mainline (SR 91) from four to eight lanes from Indiantown Rd (SR 706) to Okeechobee Rd (SR 70)
- b. Evaluating potential interchange reconfigurations
- c. Identifying stormwater management and ROW needs to meet FDOT and permitting agency requirements
- d. Conducting Environmental Look Around efforts to identify joint-use and nonconventional stormwater opportunities
  - i. Florida Forever Lands
    - 1. Pepper Farms and Flow-Through Marsh
  - ii. Martin County Septic-to-Sewer Conversions
- e. **Project is not currently funded for design, ROW, or construction.**

### 3. Proposed Design at Ten Mile Creek

- a. Proposed widening of bridge over Ten Mile Creek
  - i. Ten Mile Creek is a FEMA regulatory floodway and will require a FEMA No-Rise Certification
  - ii. Anticipate 6.8 acres of encroachment into the Ten Mile Creek FEMA floodplain
    - 1. **FTE proposed floodplain compensation provided within NSLRWCD canal system/Ten Mile Creek. NSLRWCD stated this approach has been done before. The example provided was the Okeechobee Portofino Landings, in which the top of berm or littoral shelf of the channel was expanded to provide floodplain compensation.**
    - 2. **FTE will address FEMA no-rise and CLOMR as required.**
    - 3. **Ten Mile Creek is a sovereign submerged land (SSL).**



- iii. NSLRWCD stated that there is a volumetric discharge requirement (2 inches per acre per day for the 10-year, 3-day storm event) and a head loss requirement (0.3 foot) that is provided in the Permit Information and Criteria Manual.
  1. FTE clarified that FDOT projects are exempt from local requirements under Florida Statutes. FTE will permit through SFWMD utilizing SFWMD and FDOT stormwater design criteria.

4. History of Erosion and Shoaling

- a. FTE Bridge Embankment Protection (FPID 409327-1) in 2003
- b. NSLRWCD does not know of any current issues but requested inspection of the gabions and condition of the channel at Ten Mile Creek during design.

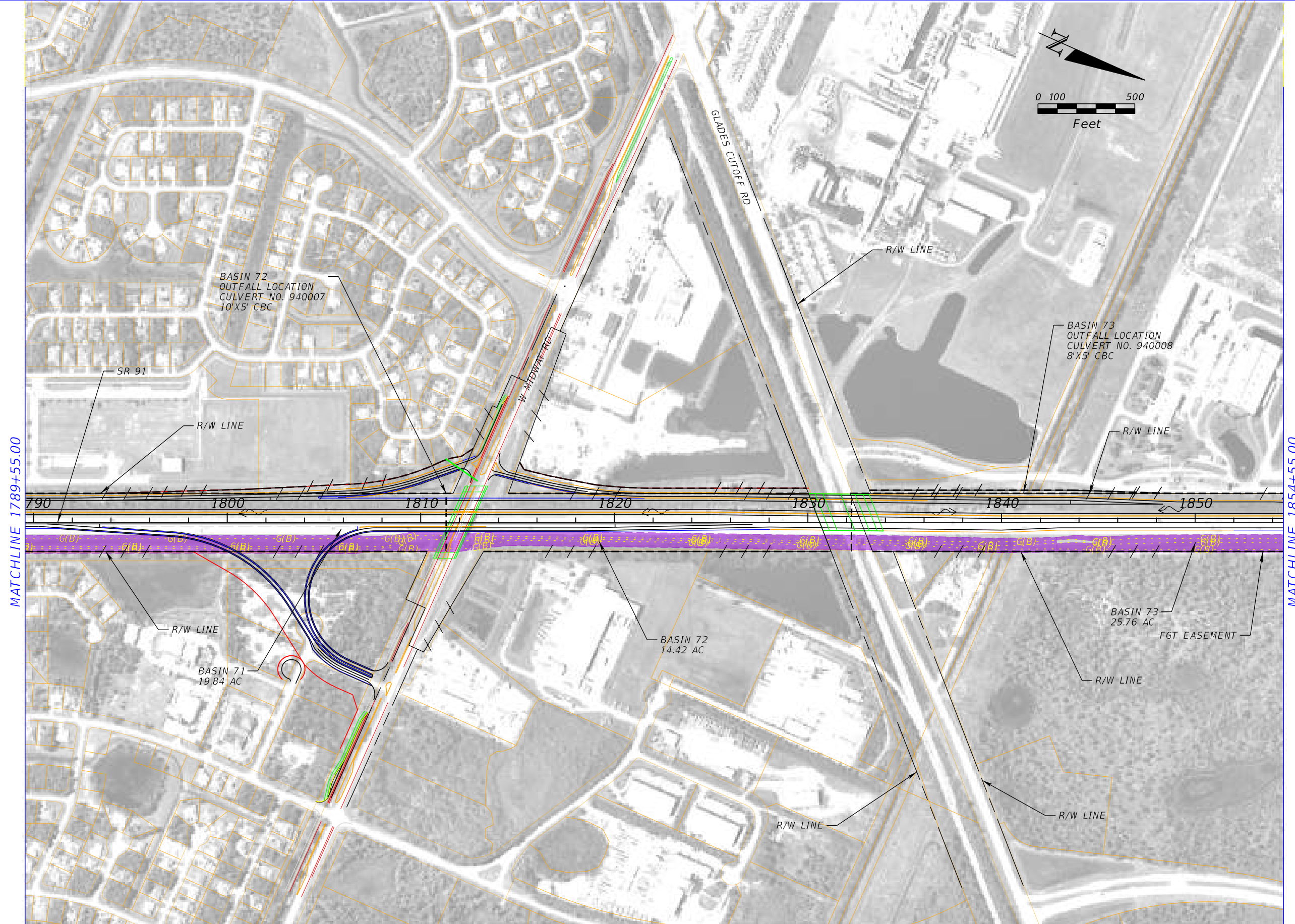
5. Additional Discussion/Questions

- a. NSLRWCD stated there is a DBHydro monitoring site at Gordy Road (East) that shows the flow is tidal.
- b. The control structures are not managed or dictated by SFWMD permit.
- c. NSLRWCD provided right of way (ROW) history on the west side of the Turnpike at Ten Mile Creek vicinity. NSLRWCD indicated that Midway Road/Canal 103 ROW has been conveyed to St. Lucie County. NSLRWCD Canal 102 culvert crossing flows west to east to NSLRWCD Canal 101 remnant at the FTE ROW line. NSLRWCD's Canal 96 at Ten Mile Creek/Gordy Road Structure stops at FTE ROW line and flows across FTE ROW to Ten Mile Creek. SFWMD is relying on the NSLRWCD Canal 96 outfall for the Ten Mile Creek Reservoir. NSLRWCD suggested a meeting with FTE ROW to clear up confusion over ROW limits.
- d. NSLRWCD asked if the widening south of SR 70 Interchange will impact the NSLRWCD's Canal 40 access berm to Ten Mile Creek. FTE responded that widening is proposed to the west in this location and that no impact to the NSLRWCD's Canal 40 or maintenance berm is anticipated.
- e. NSLRWCD inquired about widening at the Canal 49 bridge culvert. FTE responded that the culvert will either be extended or replaced.
- f. NSLRWCD referenced the canal crossing head loss criteria. FTE responded that reference will be added to the PD&E documentation.
- g. NSLRWCD indicated that approx.. 60% of NSLRWCD's 6500 sq. mi. district drains to Ten Mile Creek. Ten Mile Creek maintenance dredging is a challenge since it is Sovereign Submerged Lands. NSLRWCD is coordinating future Ten Mile Creek dredging with FDEP, SFWMD and COE. New bridges and bridge replacements will have to meet current criteria. NSLRWCD has been fined previously for doing unauthorized work within Ten Mile Creek. FTE indicated that the current concept indicates the mainline bridge over Ten Mile Creek will be widened and not replaced.
- h. FTE mentioned that a future PD&E project from SR 70 north will also potentially involve some NSLRWCD crossings. FTE will coordinate with NSLRWCD during that PD&E and future design projects as required.
- i. NSLRWCD indicated that there are maintenance challenges of NSLRWCD canals and culverts within FTE ROW. NSLRWCD has met with Turnpike's maintenance contractor in the past to discuss but challenges remain. FTE indicated that it would pass along the information directly to FTE Maintenance.
- j. NSLRWCD indicated that they don't have any water needs that could be provided by the project as part of the Environmental Look Around aspect.

ACTION ITEMS:

- a. Inform FTE ROW of NSLRWCD's request for a meeting regarding NSLRWCD canal flow across FTE ROW.
- b. Inform FTE Maintenance of NSLRWCD's request for a meeting regarding NSLRWCD maintenance challenges within FTE ROW.
- c. Meeting Notes





MATCHLINE 1789+55.00

MATCHLINE 1854+55.00

REVISIONS	
DATE	DESCRIPTION

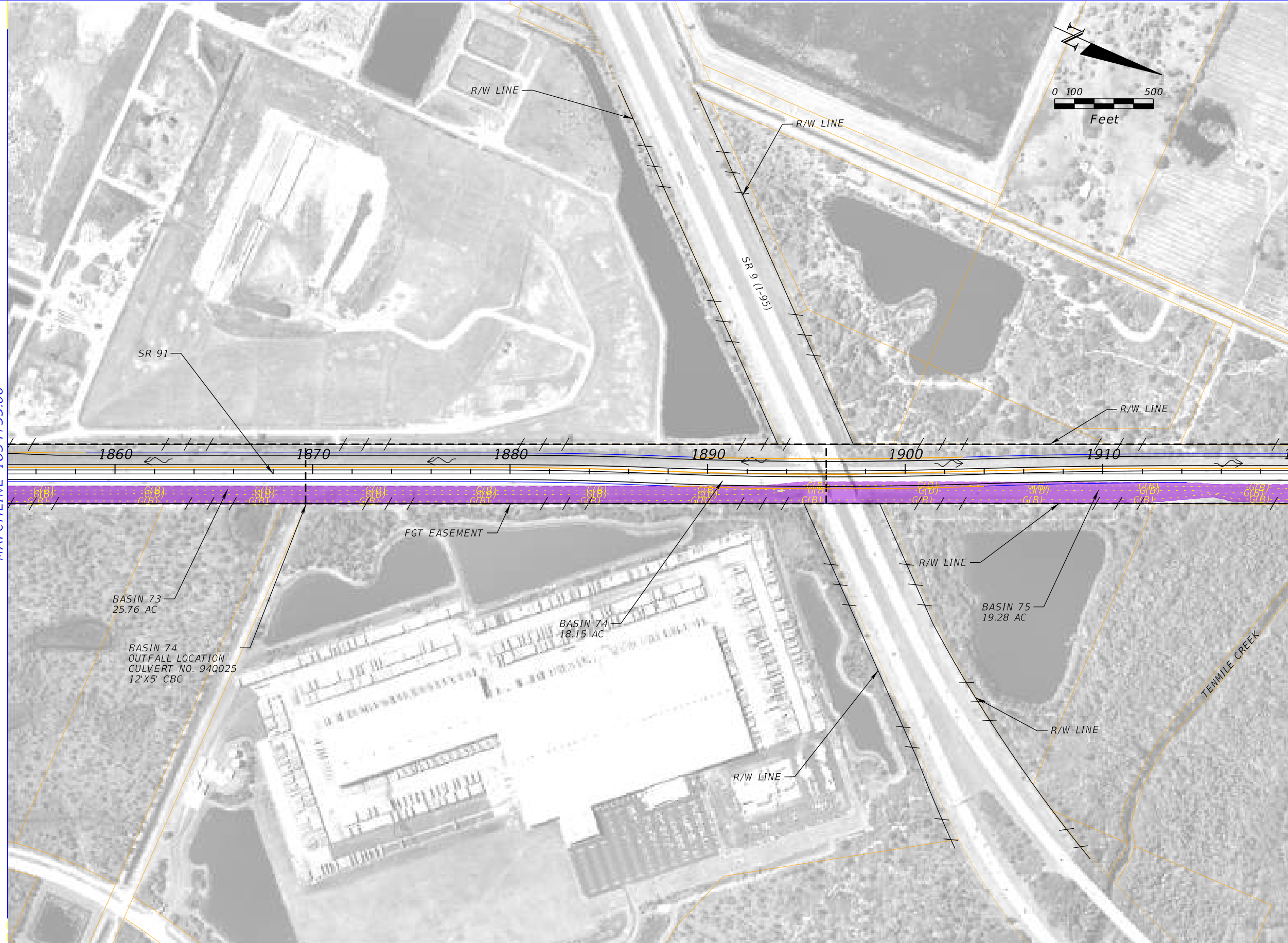
PATEL, GREENE AND ASSOCIATES, LLC  
 280 W. Canton Ave. Suite 400  
 Winter Park, Florida 32789  
 Elizabeth M. Bartell, PE #69125

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 91	ST. LUCIE	423374-1-22-01

**DRAINAGE MAP**  
**POST-DEVELOPMENT**

SHEET NO.  
**E-25**





MATCHLINE 1854+55.00

MATCHLINE 1919+55.00

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

PATEL, GREENE AND ASSOCIATES, LLC  
 280 W. Canton Ave. Suite 400  
 Winter Park, Florida 32789  
 Elizabeth M. Bartell, PE #69125

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 91	ST. LUCIE	423374-1-22-01

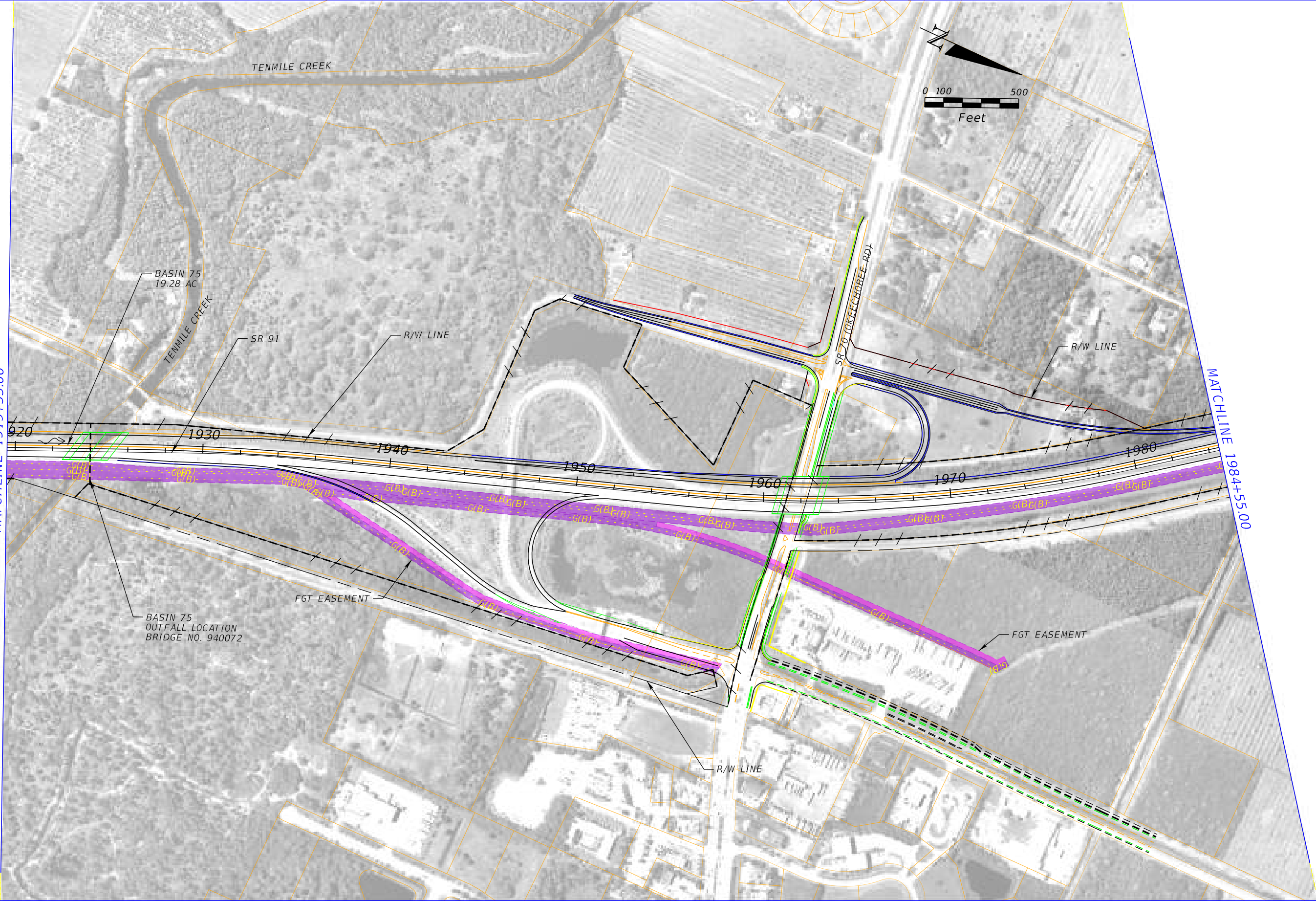
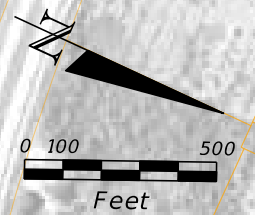
**DRAINAGE MAP**  
**POST-DEVELOPMENT**

SHEET NO.  
**E-26**



MATCHLINE 1919+55.00

MATCHLINE 1984+55.00



REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

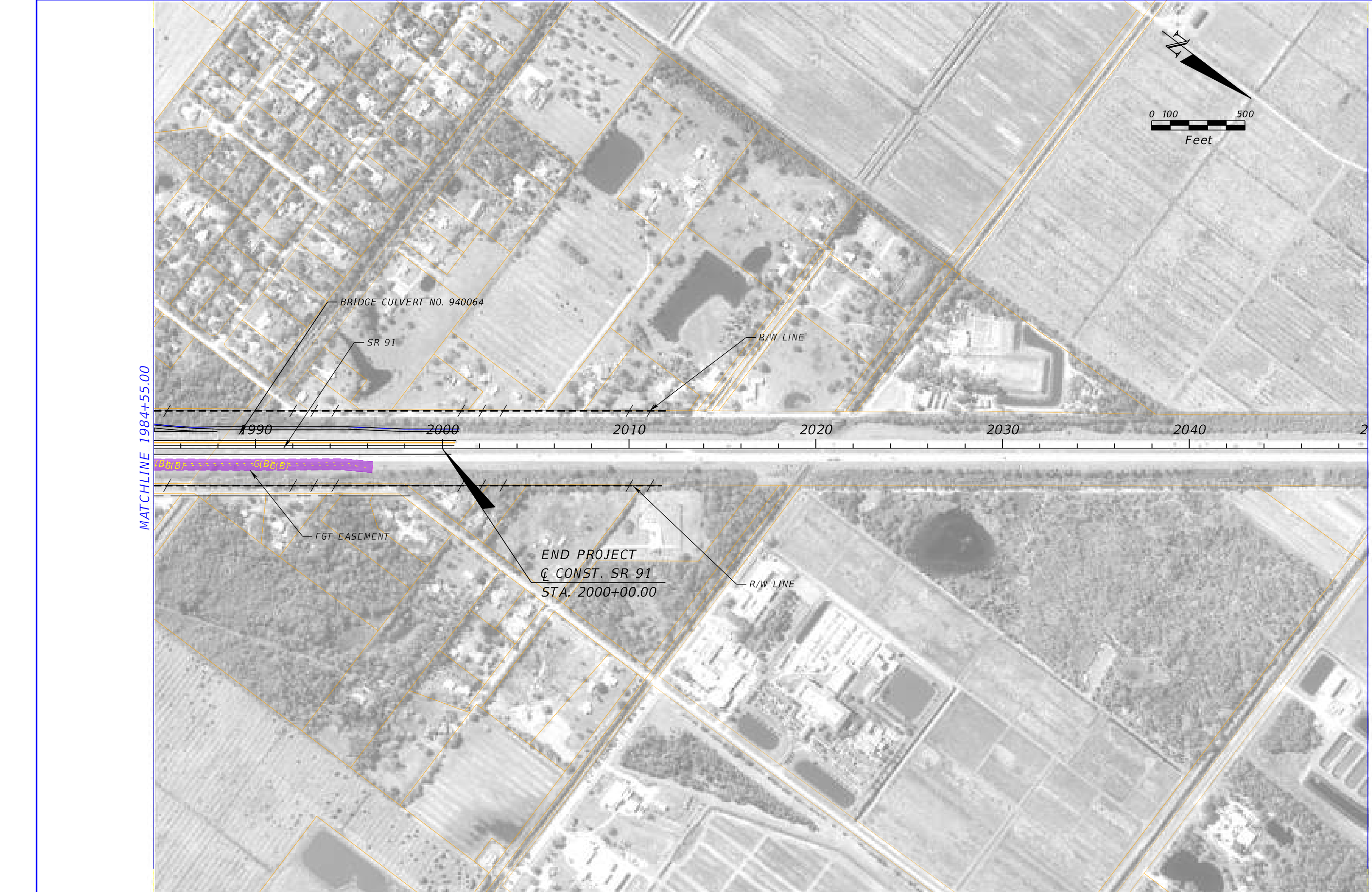
PATEL, GREENE AND ASSOCIATES, LLC  
 280 W. Canton Ave. Suite 400  
 Winter Park, Florida 32789  
 Elizabeth M. Bartell, PE #69125

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 91	ST. LUCIE	423374-1-22-01

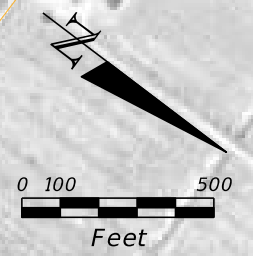
**DRAINAGE MAP**  
**POST-DEVELOPMENT**

SHEET NO.  
**E-27**





MATCHLINE 1984+55.00



BRIDGE CULVERT NO. 940064

SR 91

R/W LINE

1990

2000

2010

2020

2030

2040

(B)(B) (B)(B) (B)(B)

FGT EASEMENT

END PROJECT  
 Q CONST. SR 91  
 STA. 2000+00.00

R/W LINE

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

PATEL, GREENE AND ASSOCIATES, LLC  
 280 W. Canton Ave. Suite 400  
 Winter Park, Florida 32789  
 Elizabeth M. Bartell, PE #69125

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 91	ST. LUCIE	423374-1-22-01

**DRAINAGE MAP**  
**POST-DEVELOPMENT**

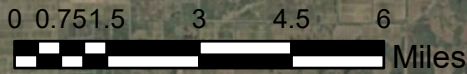
SHEET NO.  
**E-28**



# ST. LUCIE COUNTY

**END PROJECT**

**SR 91 Florida's Turnpike**



**Legend**

- A
- AE
- AH
- AO
- OPEN WATER
- VE

Matchline See Exhibit 2 of 3

...reetMap contributors, and the GIS user community. Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, Stuart CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community



Florida's Turnpike Mainline (SR 91)  
Turkey Lake Service Plaza  
Ocoee, Florida 34761

# FEMA MAP

E-29

FPID 423374-1-22-01

Date: 1/17/2019

Exhibit 3 of 3



**NOTES TO USERS**

This map is for use in administering the National Flood Insurance Program. It does not necessarily identify all areas subject to flooding, particularly from local drainage sources of small size. The community map repository should be consulted for possible updated or additional flood hazard information.

To obtain more detailed information in areas where Base Flood Elevations (BFEs) and/or floodways have been determined, users are encouraged to consult the Flood Profiles and Floodway Data and/or Summary of Stillwater Elevations tables contained within the Flood Insurance Study (FIS) report that accompanies this FIRM. Users should be aware that BFEs shown on the FIRM represent rounded whole-foot elevations. These BFEs are intended for flood insurance rating purposes only and should not be used as the sole source of flood elevation information. Accordingly, flood elevation data presented in the FIS report should be utilized in conjunction with the FIRM for purposes of construction and/or floodplain management.

Coastal Base Flood Elevations (BFEs) shown on this map apply only landward of 0.0' North American Vertical Datum of 1988 (NAVD 88). Users of this FIRM should be aware that coastal flood elevations are also provided in the Summary of Stillwater Elevations table in the Flood Insurance Study report for this jurisdiction. Elevations shown in the Summary of Stillwater Elevations table should be used for construction and/or floodplain management purposes when they are higher than the elevations shown on this FIRM.

Boundaries of the floodways were computed at cross sections and interpolated between cross sections. The floodways were based on hydraulic considerations with regard to requirements of the National Flood Insurance Program. Floodway widths and other pertinent floodway data are provided in the Flood Insurance Study report for this jurisdiction.

Certain areas not in Special Flood Hazard Areas may be protected by flood control structures. Refer to Section 2.4 "Flood Protection Measures" of the Flood Insurance Study report for information on flood control structures for this jurisdiction.

The projection used in the preparation of this map was Transverse Mercator State Plane Florida East FIPS 0901. The horizontal datum was NAD83 HARN, GRS1980 spheroid. Differences in datum, spheroid, projection or State Plane zones used in the production of FIRMs for adjacent jurisdictions may result in slight positional differences in map features across jurisdiction boundaries. These differences do not affect the accuracy of this FIRM.

Flood elevations on this map are referenced to the North American Vertical Datum of 1988. These flood elevations must be compared to structure and ground elevations referenced to the same vertical datum. For information regarding conversion between the National Geodetic Vertical Datum of 1929 and the North American Vertical Datum of 1988, visit the National Geodetic Survey website at <http://www.ngs.noaa.gov/> or contact the National Geodetic Survey at the following address:

NGS Information Services  
NOAA, NNGS12  
National Geodetic Survey  
SSMC-3, #9202  
1315 East-West Highway  
Silver Spring, Maryland 20910-3282  
(301) 713-3242

To obtain current elevation, description, and/or location information for bench marks shown on this map, please contact the Information Services Branch of the National Geodetic Survey at (301) 713-3242 or visit its website at <http://www.ngs.noaa.gov/>.

Base map information shown on this FIRM was provided in digital format by St. Lucie County and the Florida Geographic Data Library.

This map reflects more detailed and up-to-date stream channel configurations than those shown on the previous FIRM for this jurisdiction. The floodplains and floodways that were transferred from the previous FIRM may have been adjusted to conform to these new stream channel configurations. As a result, the Flood Profiles and Floodway Data tables in the Flood Insurance Study report (which contains authoritative hydraulic data) may reflect stream channel distances that differ from what is shown on this map.

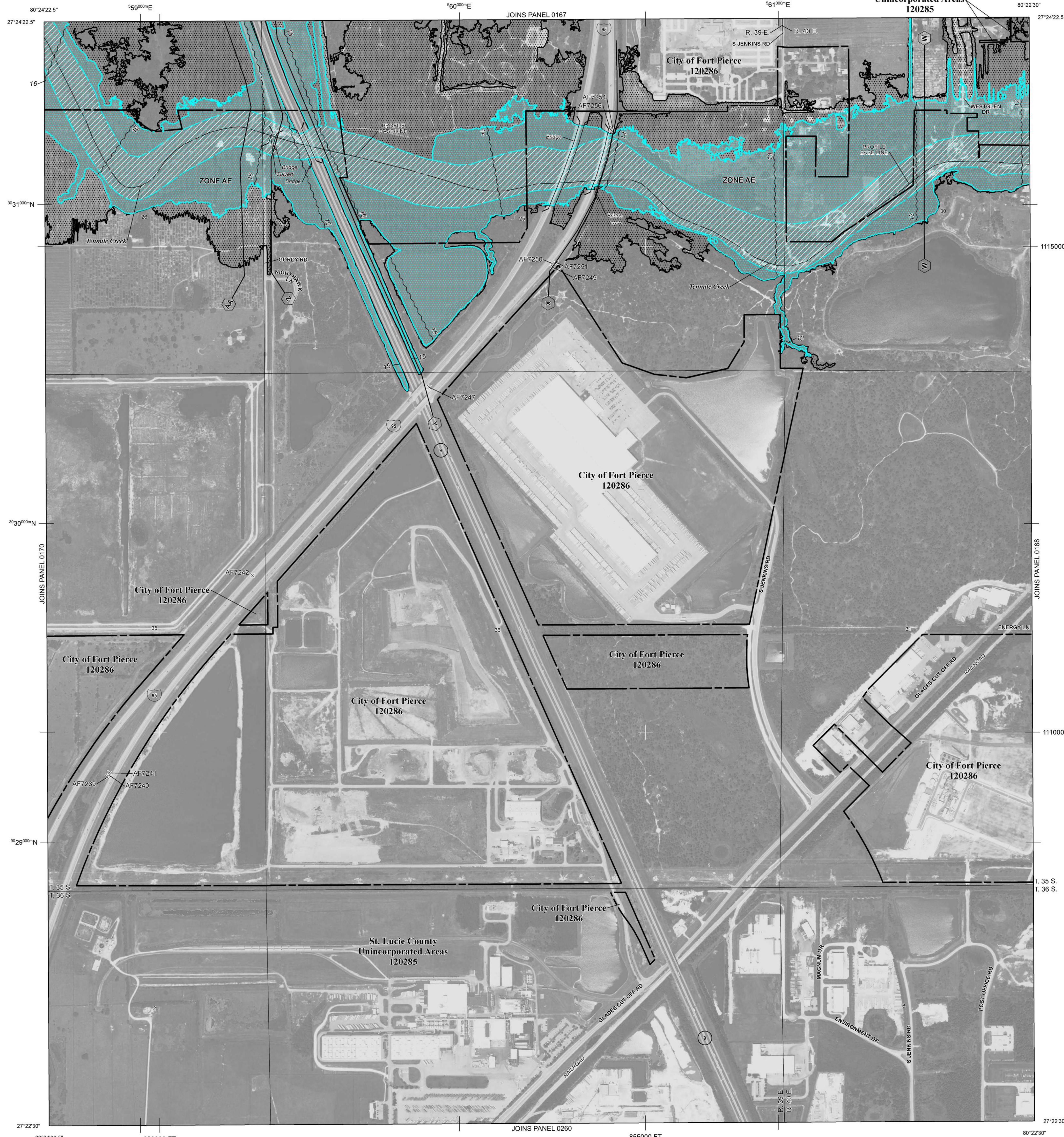
Corporate limits shown on this map are based on the best data available at the time of publication. Because changes due to annexations or de-annexations may have occurred after this map was published, map users should contact appropriate community officials to verify current corporate limit locations.

Please refer to the separately printed Map Index for an overview map of the county showing the layout of map panels, community map repository addresses; and a Listing of Communities table containing National Flood Insurance Program dates for each community as well as a listing of the panels on which each community is located.

Contact the FEMA Map Information eXchange (FMIX) at 1-877-336-2627 for information on available products associated with this FIRM. Available products may include previously issued Letters of Map Change, a Flood Insurance Study report, and/or digital versions of this map. The FMIX may also be reached at its website at <http://fmix.fema.gov/>.

If you have questions about this map or questions concerning the National Flood Insurance Program in general, please call 1-877-FEMA MAP (1-877-336-2627) or visit the FEMA website at <http://www.fema.gov/business/firm/>.

The "profile base lines" depicted on this map represent the hydraulic modeling baselines that match the flood profiles in the FIS report. As a result of improved topographic data, the "profile base line", in some cases, may deviate significantly from the channel centerline or appear outside the SFHA.



**LEGEND**

**SPECIAL FLOOD HAZARD AREAS SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD**

The 1% annual chance flood (100-year flood), also known as the base flood, is the flood that has a 1% chance of being equal or exceeded in any given year. The Special Flood Hazard Area is the area subject to flooding by the 1% annual chance flood. Areas of Special Flood Hazard include Zones A, AE, AH, AO, AR, A99, V, and VE. The Base Flood Elevation is the water-surface elevation of the 1% annual chance flood.

**ZONE A** No Base Flood Elevations determined.

**ZONE AE** Base Flood Elevations determined.

**ZONE AH** Flood depths of 1 to 3 feet (usually areas of ponding); Base Flood Elevations determined.

**ZONE AO** Flood depths of 1 to 3 feet (usually sheet flow on sloping terrain); average depths determined. For areas of alluvial fan flooding, velocities also determined.

**ZONE AR** Special Flood Hazard Area formerly protected from the 1% annual chance flood by a flood control system that was subsequently decertified. Zone AR indicates that the former flood control system is being restored to provide protection from the 1% annual chance or greater flood.

**ZONE A99** Areas to be protected from 1% annual chance flood event by a Federal flood protection system under construction; no Base Flood Elevations determined.

**ZONE V** Coastal flood zone with velocity hazard (wave action); no Base Flood Elevations determined.

**ZONE VE** Coastal flood zone with velocity hazard (wave action); Base Flood Elevations determined.

**FLOODWAY AREAS IN ZONE AE**

The floodway is the channel of a stream plus any adjacent floodplain areas that must be kept free of encroachment so that the 1% annual chance flood can be carried without substantial increases in flood heights.

**OTHER FLOOD AREAS**

**ZONE X** Areas of 0.2% annual chance flood; areas of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood.

**OTHER AREAS**

**ZONE X** Areas determined to be outside the 0.2% annual chance floodplain.

**ZONE D** Areas in which flood hazards are undetermined, but possible.

**COASTAL BARRIER RESOURCES SYSTEM (CBRS) AREAS**

**OTHERWISE PROTECTED AREAS (OPAs)**

CBRS areas and OPAs are normally located within or adjacent to Special Flood Hazard Areas.

1% annual chance floodplain boundary  
0.2% annual chance floodplain boundary  
Floodway boundary  
Zone D boundary  
CBRS and OPA boundary  
Boundary dividing Special Flood Hazard Area Zones and boundary dividing Special Flood Hazard Areas of different Base Flood Elevations, flood depths, or flood velocities  
Base Flood Elevation line and value; elevation in feet\*  
Base Flood Elevation value where uniform within zone; elevation in feet\*  
\* Referenced to the North American Vertical Datum of 1988

**Cross section line**  
Transsect line  
Geographic coordinates referenced to the North American Datum of 1983 (NAD 83), Western Hemisphere  
1000-meter Universal Transverse Mercator grid ticks, zone 17  
5000-foot grid values: Florida State Plane coordinate system, East Zone (FIPSZONE = 0901), Transverse Mercator projection  
Bench mark (see explanation in Notes to Users section of this FIRM panel)  
● M1.5 River Mile  
MAP REPOSITORIES  
Refer to Map Repositories List on Map Index  
EFFECTIVE DATE OF COUNTYWIDE FLOOD INSURANCE RATE MAP  
August 19, 1991  
EFFECTIVE DATE(S) OF REVISION(S) TO THIS PANEL  
November 4, 1992  
June 30, 1999

February 16, 2012 - to update corporate limits, to add Base Flood Elevations, to add Special Flood Hazard Areas, to change Special Flood Hazard Areas, to add roads and road names, to update the effects of wave action, and to incorporate previously issued Letters of Map Revision.

For community map revision history prior to countywide mapping, refer to the Community Map History table located in the Flood Insurance Study report for this jurisdiction.

To determine if flood insurance is available in this community, contact your insurance agent or call the National Flood Insurance Program at 1-800-638-6620.

**MAP SCALE 1" = 500'**

250 0 250 500 750 1,000 FEET  
150 0 150 300 METERS

**NATIONAL FLOOD INSURANCE PROGRAM**

**PANEL 0169J**

**FIRM**

**FLOOD INSURANCE RATE MAP**

**ST. LUCIE COUNTY, FLORIDA**

**AND INCORPORATED AREAS**

**PANEL 169 OF 420**

(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

**CONTAINS:**

COMMUNITY	NUMBER	PANEL	SUFFIX
FORT PIERCE, CITY OF	120286	0169	J
ST. LUCIE COUNTY	120285	0169	J

Notice to User: The Map Number shown below should be used when placing map orders; the Community Number shown above should be used on insurance applications for the subject community.

**MAP NUMBER 12111C0169J**

**MAP REVISED FEBRUARY 16, 2012**

**Federal Emergency Management Agency**





An employee-owned company

General Consultant  
Florida Department of Transportation, Turnpike District



February 27, 2002

Mr. Don Loving  
Senior Regulatory Supervisor  
Martin/St. Lucie Service Center  
South Florida Water Management District  
210 Atlanta Avenue  
Stuart, Florida 34994

RECEIVED  
FEB 28 2002

BY: **MSL**

Re: Bridge Embankment Protection at Ten Mile Creek (Bridge # 940072)  
Financial Project ID No.: 409327-1  
St. Lucie County  
**Request for approval of channel maintenance**

Dear Mr. Loving:

The Florida Department of Transportation, Florida's Turnpike (Turnpike) is proposing the rehabilitation of the Ten Mile Creek (Canal No. 71) shoreline downstream of the Gordy Road Structure and abutment protection at the Turnpike bridge. Due to high flows that periodically exceed the structure's capacity, bank and shoreline erosion has occurred and has resulted in sediment being deposited in the center of the channel.

The Turnpike proposes to remove the sedimentation and restore the channel to its original condition. Additionally, the current bridge abutment protection requires improvement, as it is substandard. The Turnpike's proposed project includes the restoration work originally to be conducted by the North St. Lucie River Water Control District (NSLRWCD), as a maintenance activity, under SFWMD Permit No. 56-00658-S (see attached).

Pursuant to our telephone conversation on November 20, 2001, the Turnpike is requesting approval to conduct the described work as a maintenance activity as well. Construction is scheduled to begin on October 30, 2002. Attached please find a set of construction plans and copies of correspondence between the NSLRWCD and the SFWMD.

If you have any questions or require additional information, please do not hesitate to contact me at (407) 532-3999 ext. 3410.

Sincerely

Jay C. Burtell  
District Permit Coordinator

Attachments

- Cc: John Post (TPK)
- Mike Sasser (TPK)
- Alexandru Tomacinschi (PHA)
- Stuart McGahee (LBFH)

x 3410

E-31

SCANNED





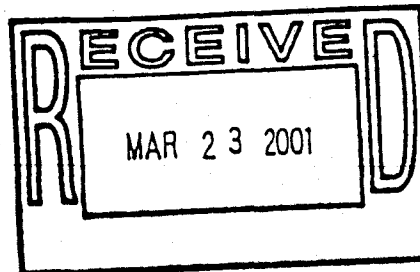
# SOUTH FLORIDA WATER MANAGEMENT DISTRICT

MARTIN/ST. LUCIE SERVICE CENTER 210 Atlanta Avenue, Stuart, FL 34994  
(561) 223-2600 • FL WATS 1-800-250-4100 • Suncom 269-2600 • Fax (561) 223-2608 • www.sfwmd.gov/org/exo/mslsc/

CON 24-06

March 20, 2001

Mr. Marty Sanders, P.E., District Engineer  
North St. Lucie River Water Control District  
2721 South Jenkins Road  
Ft. Pierce, FL 34981



Dear Mr. Sanders:

**SUBJECT: NSLRWCD Structure S-71-1 Channel Maintenance  
(a.k.a. Gordy Road Structure)  
SFWMD Permit No. 56-00658-S  
St. Lucie County  
Secs. 26/Twp. 35 S/ Rge. 39 E**

The purpose of this correspondence is to approve your request of March 8, 2001, to conduct stream channel maintenance and sedimentation removal downstream of NSLRWCD Structure S-71-1 (Gordy Road Structure). The subject drawing and description of the scope of work will be added to our files. With this approval I am requesting the following items prior to commencement of construction:

- Date of commencement.
- An on-site meeting with myself, the contractor and NSLRWCD staff seventy-two (72) hours prior to construction to confirm the proposed turbidity control plan.

Be advised that the maintenance activity authorized by this correspondence does not eliminate the necessity for NSLRWCD to obtain other Federal or local authorizations prior to the commencement of the subject activity.

Should you have questions or need additional information regarding this authorization or the requested items noted above, please contact me (800-250-4100, ext. 3607) at this office.

Sincerely,  
*Donald R. Loving*  
Donald R. Loving  
Senior Regulatory Supervisor  
Martin/St. Lucie Service Center

C: Mr. Jay Burrell, Turnpike District

**GOVERNING BOARD**

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Michael D. Norton, Vice Chairman  
Mitchell W. Berger

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Gerardo B. Fernandez  
Patrick J. Gleason

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Harkley R. Thornton  
Trudi K. Williams

**EXECUTIVE OFFICE**

Frank R. Finch, P.E., Executive Director  
James E. Blount, Chief of Staff

**SERVICE CENTER**

Paul S. Millar, Director

E-32

SCANNED







JOHN SCOTTO

**BOARD OF SUPERVISORS**  
MYRON (MAC) VARN, JR.  
PRESIDENT

STEVEN D. CASSENS  
PRESIDENT PRO TEM

**NORTH ST. LUCIE RIVER WATER CONTROL DISTRICT**

2721 SOUTH JENKINS ROAD  
FORT PIERCE, FLORIDA 34981  
TELEPHONE (561) 461-5050

MARTY E. SANDERS, P.E.  
DISTRICT ENGINEER

CAROL A. DONAHUE  
SECRETARY-TREASURER

FRANK H. FEE, III  
ATTORNEY

*Bob Gifford*

→ *461-2450*

*Fed Funds*  
*\$30K +/-*

**FAXED**  
*3/29*

*thinks its ok to transfer \$ to TPK*

March 29, 2001

Andres E. Aquino  
Assistant Permits Engineer  
Sverdrup Corporation  
P.O. Box 9828  
Ft. Lauderdale, FL 33310-9828

Re: **NSLRWCD Structure S-71-1 / Gordy Road**  
**Maintenance of Channel**

Dear Mr. Aquino:

Per your request, please find the following information regarding the maintenance of Canal No. 71 (Ten-mile Creek) at the Gordy Road Structure. The plans hereby referenced were forwarded to you on March 20, 2001.

**The History:**

The Gordy Road Structure was constructed in the early 1960's as part of a water control plan designed by the Soil Conservation Service.

The water control structure was originally designed with retention dikes tying the structure back into natural ground. For events exceeding the structure capacity, stormwater discharges around the structure and downstream into Canal No. 71 (Ten-Mile Creek). As a result of high flows that exceed the structure's capacity, erosion occurs to the channel banks. Most recently during a 1995 event, approximately 8.5 inches of rainfall caused the structure to overflow and exceed its capacity resulting in the depositing of a shoal downstream of the water control structure.

**The Proposed Plan:**

We propose to remove the sedimentation and restore the stream channel to its original design section as shown on the Turnpike plans dated 1955 and the Gordy Road plans dated 1962. To help prevent this erosion problem in the future, large diameter (1 - 2 foot) riprap boulders will be placed along the north bank of the channel where the erosion has occurred in the past. Behind these boulders filter fabric will be placed so that material does not migrate between the



boulders. Additionally, a rock rubble emergency discharge channel will be constructed over this embankment so that in the event that the discharge exceeds the structure capacity the excess stormwater will be routed over the rock rubble and will not cause further erosion and sedimentation.

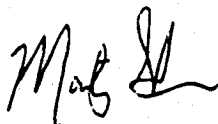
**The Work:**

The work will be accomplished utilizing backhoe and front-end loader to move material. Floating turbidity booms will be placed at the east and west edge of the Florida Turnpike bridge to prevent any downstream migration of sedimentation during the removal of the material. No work will be done during discharge from Structure S-71-1.

Because of the current low flow conditions, we are requesting that this work be authorized under maintenance exemption. It is imperative that we begin work immediately. We believe the work can be accomplished in 3 – 4 weeks. Given the current dry conditions and no anticipated rainfall, it is highly likely that this work can be accomplished before any significant rainfall.

Please feel to call if you have any questions.

Sincerely,



Marty Sanders, P.E.  
District Engineer

Enclosures

Cc: Wade C. Fleming

P:\74-0901\S-71-1\MAINTENANCE-SFWMD-AQUINO-L01.doc



**Subject: Ten Mile Creek Right-of-Way map**

**Date: Tue, 26 Mar 2002 09:54:20 -0500**

**From: jay.burrell@dot.state.fl.us**

**To: dloving@sfwmd.gov**

Per our conversation on 3/25. Please let me know if you require any further information.

Jay C. Burrell  
District Permit Coordinator  
Florida's Turnpike Headquarters  
P.O. Box 613069  
Ocoee, FL 34761  
(407) 532-3999 ext. 3410  
jay.burrell@dot.state.fl.us

----- Forwarded by Jay Burrell/TP/FDOT on 03/26/2002 09:48 AM -----

Stephanie  
Pulsifer

To: Jay Burrell/TP/FDOT@FDOT  
cc: Mike Joiner/TP/FDOT@FDOT  
Subject: Ten Mile Creek Right-of-Way map

03/26/2002  
08:47 AM


Good morning.

Enclosed please find the zipped Right-of-Way map in .tif format for the Ten Mile Creek Bridge.

(See attached file: 10 Mile Cr.zip)

If I can be of further assistance, please do not hesitate to contact me.  
Thanks!

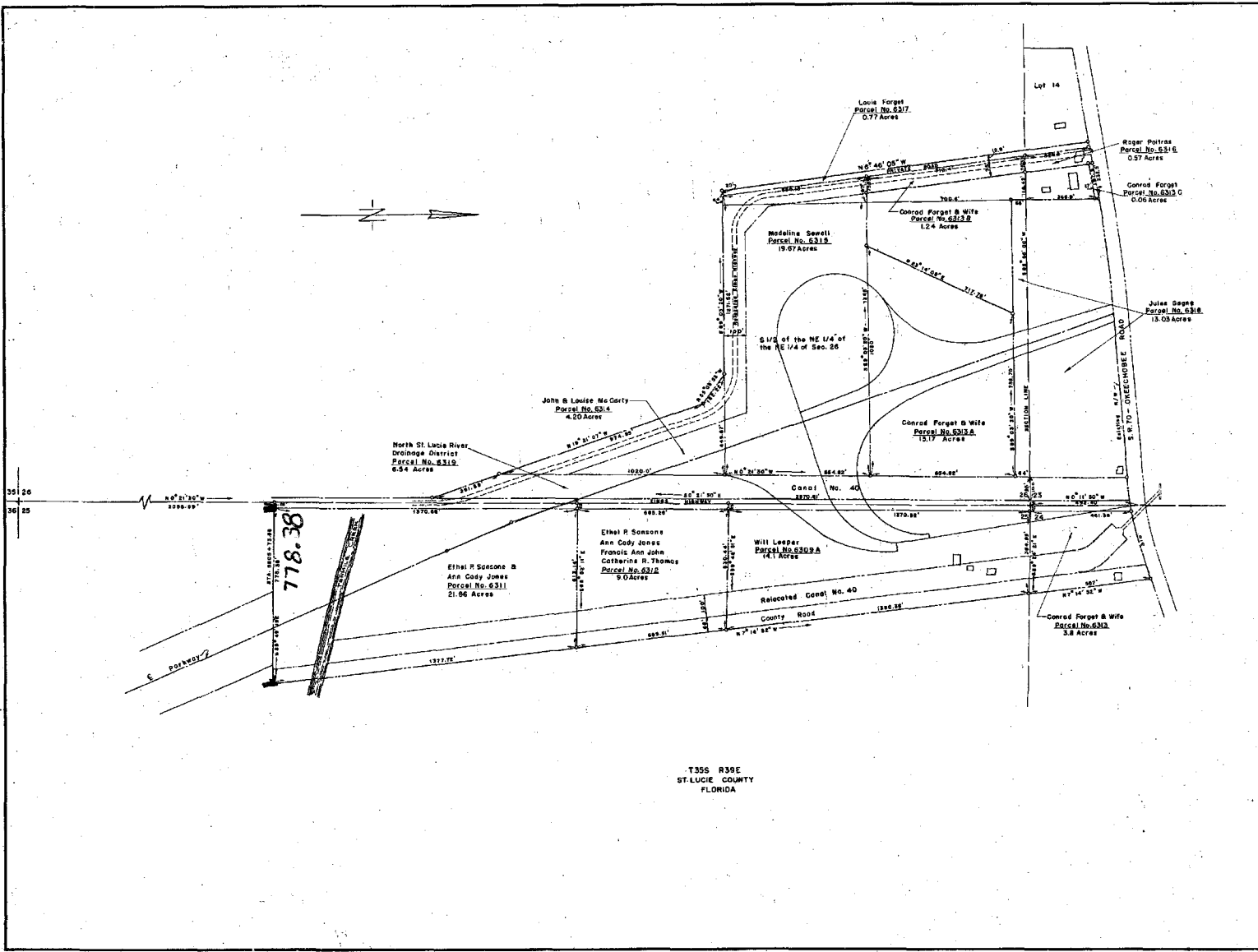
Stephanie Pulsifer  
Survey and Mapping  
Department of Transportation  
Turnpike Headquarters  
Turnpike Mile Post 263, Florida's Turnpike  
(Building 5315, Turkey Lake Service Plaza)  
Ocoee, Florida 34761  
(PH) 407-532-3999 Ext. 3427  
(FAX) 407-822-6538  
stephanie.pulsifer@dot.state.fl.us

 10 Mile Cr.zip	<b>Name:</b> 10 Mile Cr.zip <b>Type:</b> Zip Compressed Data (application/x-zip-compressed) <b>Encoding:</b> base64
--	---

E-36



QUANTITIES				
NO.	ITEM DESCRIPTION	UNIT	ESTIMATED	FINAL
			QUANTITY	QUANTITY



T355 R39E  
ST. LUCIE COUNTY  
FLORIDA

I certify that this map and the surveys on which it is based were made under my supervision and direction and are true and correct to the best of my knowledge.

*J. H. Horton*  
Section Engineer  
Land Surveyor Florida Reg. No. 275

REVISION		DATE	BY

FLORIDA STATE TURNPIKE AUTHORITY  
**SUNSHINE STATE PARKWAY**  
MIAMI TO FORT PIERCE SECTION

SECTION 6  
RIGHT-OF-WAY MAP  
ST. LUCIE COUNTY

BILL HORTON & ASSOCIATES  
SECTION ENGINEER SECTION

MADE BY: J. H. HORTON  
CHECKED BY: J. H. HORTON  
DATE: 12/27/52  
SCALE: 1" = 500'

CONTRACT NO. 6-3 SHEET 12 OF 12

SCANNED









# SOUTH FLORIDA WATER MANAGEMENT DISTRICT

MARTIN/ST. LUCIE SERVICE CENTER 210 Atlanta Avenue, Stuart, FL 34994  
(772) 223-2600 • FL WATS 1-800-250-4100 • Suncom 269-2000 • Fax (772) 223-2608 • www.sfwmd.gov/org/exo/mslsc/

CON 24-06  
Environmental Resource Regulation Department  
Application Number: 020326-18

April 26, 2002

Mr. Jay C. Burrell, District Permit Coordinator  
Florida Department of Transportation, Turnpike District  
P.O. Box 613069  
Ocoee, FL 34761

Dear Mr. Burrell:

**Subject: Exemption Number: 56-01594-P**  
**Channel Maintenance & Bridge Embankment Protection @ Ten Mile Creek**  
**St. Lucie County, S26/T35S/R39E**

This is to acknowledge receipt of your request to conduct channel maintenance, sedimentation removal and abutment repairs to the Turnpike Bridge over Ten Mile Creek in St. Lucie County. The work will consist of the removal of existing sediments from the stream channel, straighten the existing channel and stabilization of the Ten-Mile Creek shoreline, as well as the northern and southern turnpike bridge abutments as shown on the attached exhibits. No other work is authorized under this exemption.

The South Florida Water Management District (District) has reviewed the information submitted and has determined that the proposed activity will have only minimal or insignificant individual or cumulative adverse impacts on the water resources of the District. Therefore, based solely on the documents submitted to the District on February 28, 2002 and March 26, 2002, the project qualifies for an exemption pursuant to subsection 373.406(6), Florida Statutes. Activities which qualify for an exemption must be conducted and operated using appropriate best management practices and in a manner which does not cause a water quality violation pursuant to Florida Administrative Code 62-302.

This letter does not relieve you from the responsibility of obtaining other permits (federal, state or local) which may be required for this project.

The determination that this project qualifies as an exempt activity may be revoked if the installation is substantially modified, if the basis for the exemption is determined to be materially incorrect, or if the installation results in a violation of state water quality standards. Any changes made in the construction plans or location of the project may necessitate a permit from the District. Therefore, you are advised to contact the District before beginning the project and before beginning any work in wetlands which is not specifically described in the submittal.

E-39

GOVERNING BOARD

Trudi K. Williams, P.E., *Chair*  
Lennart E. Lindahl, P.E., *Vice-Chair*  
Pamela Brooks-Thomas

Michael Collins  
Hugh M. English  
Gerardo B. Fernández

Patrick J. Gleason, Ph.D., P.G.  
Nicolas J. Gutiérrez, Jr., Esq.  
Harkley R. Thornton

EXECUTIVE OFFICE

Henry Dean, *Executive Director*



Mr. Jay C. Burreli  
Florida Department of Transportation  
Ten-Mile Creek Improvements  
April 26, 2002  
Page 2 of 3

The notice of determination that the project qualifies as an exempt activity constitutes final agency action by the District unless a petition for administrative hearing is filed. Upon timely filing of a petition, this Notice will not be effective until further order by the District. Please be advised that if the District has not published a notice in the newspaper advising the public that it is determining that this activity is exempt pursuant to subsection 373.406, Florida Statutes. Enclosed is a sample package used for newspaper noticing by the District. Publication, using the District form, notifies the members of the public (third parties) of their rights to challenge the determination. If proper notice is given by publication, third parties have a 21-day time limit on the time to file a petition opposing the determination. If you do not publish a notice, a party's right to challenge the determination extends for an indefinite period of time. If you wish to have certainty that the period of filing such a challenge is closed, then you may publish, at your own expense, such a notice in a newspaper of general circulation. A copy of the form of the notice and list of newspapers of general circulation is attached for you use. If you choose to publish this notice, please provide us with an affidavit of publication when it becomes available.

This package includes:

- Attachment 1 The name address and telephone number for the newspaper of general circulation typically used by the District to advertise permits that are issued by the Governing Board is provided.
- Attachment 2 A copy of a sample notice that the District staff used is included to provide guidance on the language to be used in the notice. This language satisfies the legal requirements for noticing.
- Attachment 3 A copy of a sample affidavit of publication is also included. The affidavit is provided to you by the newspaper to provide proof of publication. The original affidavit should be submitted to:

Ms. Loretta Scragg  
South Florida Water Management District  
P.O. Box 24680  
West Palm Beach, FL 33416-4680

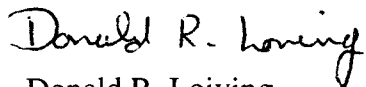
- Attachment 4 Location Map and Plans



Mr. Jay C. Burrell  
Florida Department of Transportation  
Ten-Mile Creek Improvements  
April 26, 2002  
Page 3 of 3

At least seventy-two (72) hours prior to the commencement of construction of the proposed works, the District is requesting a pre-construction meeting with the contractors to review methods or work and construction timing. If you have any questions, please contact me at (800) 250-4100, extension 3607.

Sincerely,



Donald R. Loiving  
Senior Regulatory Supervisor  
Environmental Resource Compliance Division  
Martin/St. Lucie Service Center  
South Florida Water Management District

DRL/sbm

Attachments

c: Don West, St. Lucie County Engineer  
Florida Department of Environmental Protection (Port St. Lucie office)  
U.S. Army Corps of Engineers (Stuart)  
North St. Lucie River Water Control District





## Legal Advertisement Newspaper Addresses and Deadlines

Each newspaper has a different schedule for the advertisement of legal ads. The following table outlines the newspaper, the address, deadline the publishing day, phone numbers and contacts for the legal ads.

County	Newspaper	Contact	Deadline	Publish Day	Fax	Phone
Broward	Ft. Lauderdale News/Sun Sentinel 333 Southwest 12 <sup>th</sup> Avenue Deerfield Beach, FL 33442	Dedrie Floyd	Monday 2:30	Following Thursday	(941) 435-1006	(941) 435-1033
Charlotte	Charlotte Sun Herald 23170 Harborview Road Charlotte Harbor, FL 33980	Mary Skaggs	Monday 5:00	Following Thursday	(941) 629-2085	(941) 629-2655
Collier	Naples Daily News 1075 Central Avenue Naples, FL 33490	Pam Perrell	Monday 4:00	Following Thursday	(941) 263-4703	(941) 263-4632
Glades	Independent Newspapers Inc. C/o Glades County Democrat © P.O. Box 7013 Dover, DE 19903	No Contact	Thursday 5:00	Following Thursday	(863) 983-7537	(863) 945-0511
Handy	Independent Newspapers Inc. C/o Clewiston News(S) P.O. Box 7013 Dover, DE 19903	No Contact	Thursday 5:00	Following Thursday	(863) 983-7537	(863) 933-9145
Highlands	Lake Placid Journal 232 North Main Street Lake Placid, FL 33852	Sharon Jones	Monday 5:00	Following Thursday	(941) 699-0331	(941) 455-2622
Lee	Fort Myers News Press 2442 Dr. Martin Luther King, Jr. Hwy Fort Myers, FL 33901	Brenda Leighton	Monday 12:00	Following Thursday	(941) 337-1335	(941) 335-0211
Martin	Stuart News 1939 Southeast Federal Highway Stuart, FL 34994	Mary Byrne	Tuesday 12:00	Following Thursday	(861) 221-4125	(861) 237-1550
Miami-Dade	Miami Herald 1 Herald Plaza Miami, FL 33101 Attn: Ms. Cooper	Ms. Cooper	Tuesday 3:00	Following Thursday	(305) 995-8121	(305) 375-2584
Miami-Dade	South Dade News Ledger 15 Northeast 1 <sup>st</sup> Road Homestead, FL 33090 Attn Carol (Legal)	Carol Raymond	Tuesday 5:00	Following Friday	(305) 248-0596	(305) 245-2311
Monroe	Key West Citizen 3420 Northside Drive Key West, FL 33040	Cristal Vrabo	Tuesday 12:00	Following Thursday	(305) 294-0768	(305) 294-8641
Okeechobee	Independent Newspapers Inc. C/o Okeechobee News (A) P.O. Box 7013 Dover, DE 19903	No Contact	Wednesday 10:00	Following Friday	(877) 354-2424	(863) 753-3131
Orange	Orlando Sentinel Star 633 North Orange Avenue Orlando, FL 32802	Julia Nichols	Tuesday 5:00	Following Thursday	(407) 420-5011	(407) 420-5150
Osceola	Osceola Shopper/News Gazette 108 Church Street Kissimmee, FL 34742	Barbara Pollard	Friday 5:00	Following Thursday	(407) 846-8516	(407) 846-7600
Palm Beach	Palm Beach Post 2751 South Dixie Highway West Palm Beach, FL 33405	Claudia Dunn	Monday 3:00	Following Thursday	(561) 320-4340	(561) 320-3100
Polk	Lakeland Ledger 300 West Lima Street Lakeland, FL 33815	Jennifer Clendenning	Friday 3:00	Following Thursday	(941) 687-7975	(941) 686-7000
St. Lucie	Fort Pierce News Tribune 600 Edwards Road Fort Pierce, FL 34954	Pat Shammon	Wednesday 12:00	Following Thursday	(861) 486-5997	(861) 481-2000



**SOUTH FLORIDA WATER MANAGEMENT DISTRICT  
PUBLIC NOTICE OF APPLICATION**

Notice is hereby given that pursuant to Chapter 373, Florida Statutes, the following application(s) for permit has been received for a project(s) in Broward County:

---

Deerfield Beach Energy LLC (Deerfield Beach Energy Center) 1400 Smith St, Houston, TX 77002, has submitted Application 010112-7 for a Water Use Permit for 30 acres of industrial lands. The water will be withdrawn from the Biscayne Aquifer and the project is located in Section 9, Township 48 South, Range 42 East.

Minto Communities Inc (Lido Isles) 4400 W Sample Rd, Suite 200, Coconut Creek, FL 33073, has submitted Application 001215-2 for modification of Surface Water Management Permit 06-01835-S for 153.68 acres of residential lands. The water will be discharged via an existing system and the project is located in Section 20, Township 51 South, Range 40 East.

Shamrock of Broward Ltd (Shamrock Condominium) 12615 SW 91<sup>st</sup> Street, Miami, FL 33186, has submitted Application 010111-18 for an Environmental Resource Permit for 2.3 acres of residential lands. The water will be discharged to an onsite retention system and the project is located in Section 2, Township 51 South, Range 42 East.

Watermark Communities Inc (Heron Bay East) 11575 Heron Bay Blvd, 2<sup>nd</sup> Floor, Coral Springs, FL 33076, has submitted Application 010116-7 for modification of Surface Water Management Permit 06-00073-S for 250.63 acres of residential lands. The water will be discharged to the LWDD L-36 Canal via the NSID Waterways and the project is located in Sections 31,32/5,6, Townships 47/48 South, Range 41 East.

Broward County Board of County Commissioners (N Central County Neighborhood Improvement Project) 115 S Andrews Ave, Room 421, Ft Lauderdale, FL 33301, has submitted Application 010119-10 for an Environmental Resource Permit for 945 acres of residential lands. The water will be discharged to the C-12 and C-13 Canals and the project is located in Sections 28,29,31,32, Township 49 South, Range 42 East.

Broward County Parks and Recreation Division (Offsite Mitigation at Long Key Natural Area) 1000 NW 38<sup>th</sup> Street, Oakland Park, FL 33308, has submitted Application 010124-12 for modification of Environmental Resource Permit 06-02416-P for 29.4 acres of offsite mitigation lands. The water will be discharged via an existing system and the project is located in Section 23, Township 50 South, Range 40 East.

---

Interested persons may comment upon the application or submit a written request for a copy of the staff report containing proposed agency action regarding the application by writing to the South Florida Water Management District, Attn: Environmental Resource Regulation, PO Box

**ATTACHMENT 2**  
**sheet 1 of 2**



24680, West Palm Beach, FL 33416-4680, but such comments or requests must be received by 5:00 PM within 21 days from the date of publication.

No further public notice will be provided regarding this application. A copy of the staff report must be requested in order to remain advised of further proceedings. Substantially affected persons are entitled to request an administrative hearing regarding the proposed agency action by submitting a written request therefor after reviewing the staff report.

PUBLISH: February 15, 2001

Sun Sentinel  
333 SW 12<sup>th</sup> Avenue  
Deerfield Beach, FL 33442  
Attn: Legal Ads

ATTACHMENT 2  
sheet 2 of 2

E-44

SCANNED



4240  
R100609  
# 787.46

# The Miami Herald

www.herald.com  
www.e herald.com

PUBLISHED DAILY  
MIAMI-DADE-FLORIDA

STATE OF FLORIDA  
COUNTY OF DADE

Before the undersigned authority personally  
appeared:

**JEANNETTE MARTINEZ**

who on oath says that he/she is

**CUSTODIAN OF RECORDS**

of The Miami Herald, a daily newspaper published at  
Miami in Dade County, Florida; that the attached  
copy of advertisement was published in said  
newspaper in the issues of:

December 21, 2000

Affiant further says that the said The Miami Herald  
is a newspaper published at Miami, in the said Dade  
County, Florida and that the said newspaper has  
heretofore been continuously published in said Dade  
County, Florida each day and has been entered as  
second class mail matter at the post office in Miami,  
in said Dade County, Florida, for a period of one  
year next preceding the first publication of the  
attached copy of advertisement; and affiant further  
says that he has neither paid nor promised any  
person, firm or corporation any discount, rebate,  
commission or refund for the purpose of securing  
this advertisement for publication in the said  
newspapers(s).

*Jeannette Martinez*  
Sworn to and subscribed before me this  
26th day of December, 2000

My Commission  
Expires: May 12, 2002  
Silvia Acosta

*Silvia Acosta*  
Notary

OFFICIAL NOTARY SEAL  
SILVIA ACOSTA  
NOTARY PUBLIC STATE OF FLORIDA  
COMMISSION NO. CC717118

## SOUTH FLORIDA WATER MANAGEMENT PUBLIC NOTICE OF APPLICATION

Notice is hereby given  
that pursuant to Chapter  
373, Florida Statutes, the  
following application(s)  
for permit have been  
received for project (s) in  
Miami-Dade County:

U.S. Air Force  
(Homestead Air Reserve  
Station) 29050 Coral Sea  
Blvd., Box 68, Homestead,  
FL 33039-1299, has  
submitted Application  
for a permit to install  
Public Water Supply to  
service 3500 acres. The  
project is located in  
Sections  
1, 2, 11-14/36/6, 7,  
Townships 57/56 South,  
Ranges 39/40 East.

Homestead Miami  
Speedway LLC  
(Homestead Motorsports  
Complex Overflow  
Parking Lot) One  
Speedway Blvd.

Homestead, FL 33033,  
has submitted Application  
for a permit to install  
an Environmental  
Resource Permit for 120  
acres of recreational  
lands. The water will be  
removed via an  
irrigation system and the  
project is located in  
Section 22, Township 57  
South, Range 39 East.

M & H Homestead Ltd.  
(Keys Gate) 13 SW 7th  
Street, Miami, FL 33130,  
has submitted Application  
for a permit to install  
surface water

management of 865.63  
acres of residential  
lands. The water will be  
removed via an  
irrigation system and the  
project is located in  
Sections  
15 17, 19-21, 23, 29,  
Township 57 South,  
Range 39 East.

Interested persons may  
comment upon the  
application or submit a  
written request for a  
copy of the staff report  
containing proposed  
agency action regarding  
the application by writing  
to the South Florida  
Water Management  
District, Attn:  
Environmental Resource  
Regulation, P.O. Box  
24580, West Palm  
Beach, FL 33418-4580,  
but such comments or  
requests must be  
received by 5:00 PM  
within 21 days from the  
date of publication.

No further public notice  
will be provided  
regarding this  
application. A copy of  
the staff report must be  
requested in order to  
remain advised of further  
proceedings.  
Substantially affected  
persons are entitled to  
request an  
administrative hearing  
regarding the proposed  
agency action by  
submitting a written  
request therefor after  
reviewing the staff  
report.

PUBLISH:  
December 21, 2000

# ATTACHMENT 3

E-45

SCANNED



**STATE OF FLORIDA**  
**DEPARTMENT OF TRANSPORTATION**

**CONTRACT PLANS**

FINANCIAL PROJECT ID 409327-1-52-01

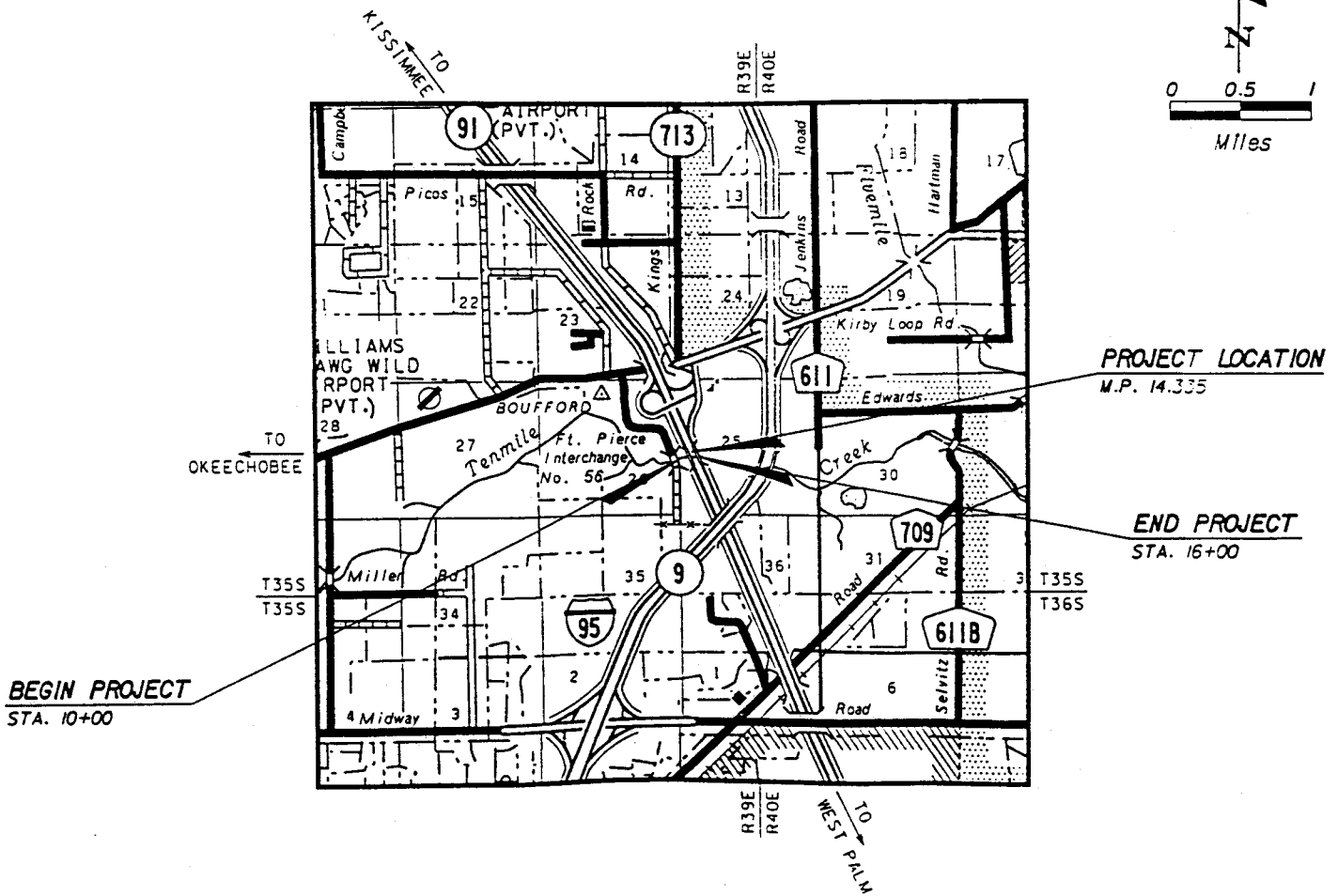
FLORIDA'S TURNPIKE

ST. LUCIE COUNTY (94470)

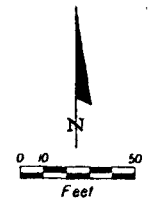
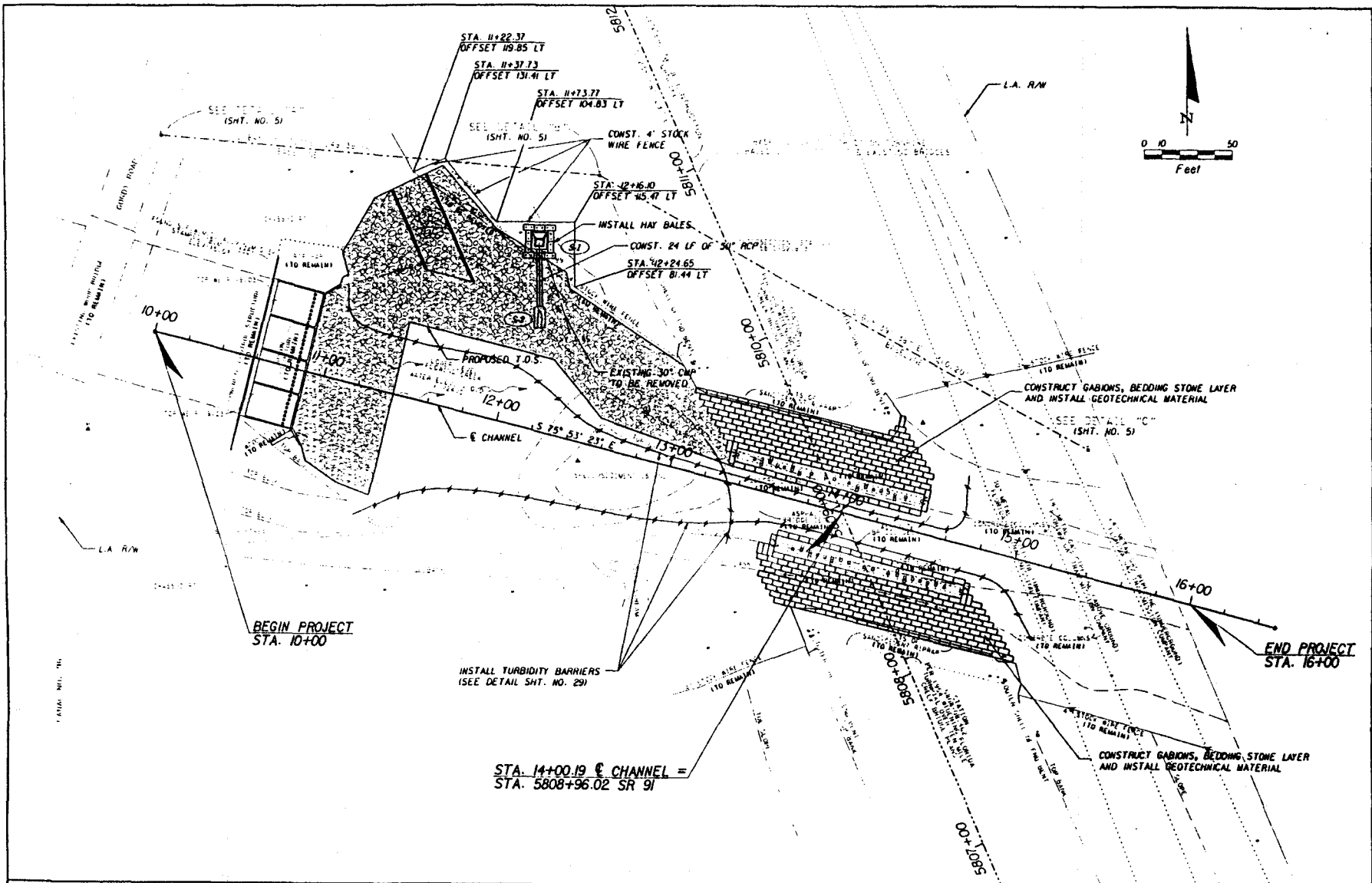
BRIDGE EMBANKMENT PROTECTION

AT

TEN MILE CREEK - BRIDGE # 940072







ATTACHMENT 4B

SCANNED

DATE		REVISIONS		DESCRIPTION	
NO.		DATE	BY		

  
**PITMAN & HARTENSTEIN & ASSOC., INC.**  
**ENGINEERS**  
 JACKSONVILLE • FORT MYERS • TAMPA

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	STA.	FINANCIAL PROJECT ID.
91	ST. LUCIE	409327-1-52-01

PLAN SHEET

SHEET NO.
11



App. No. 020326-18

COMPONENTS OF CONTRACT PLANS SET  
ROADWAY PLANS

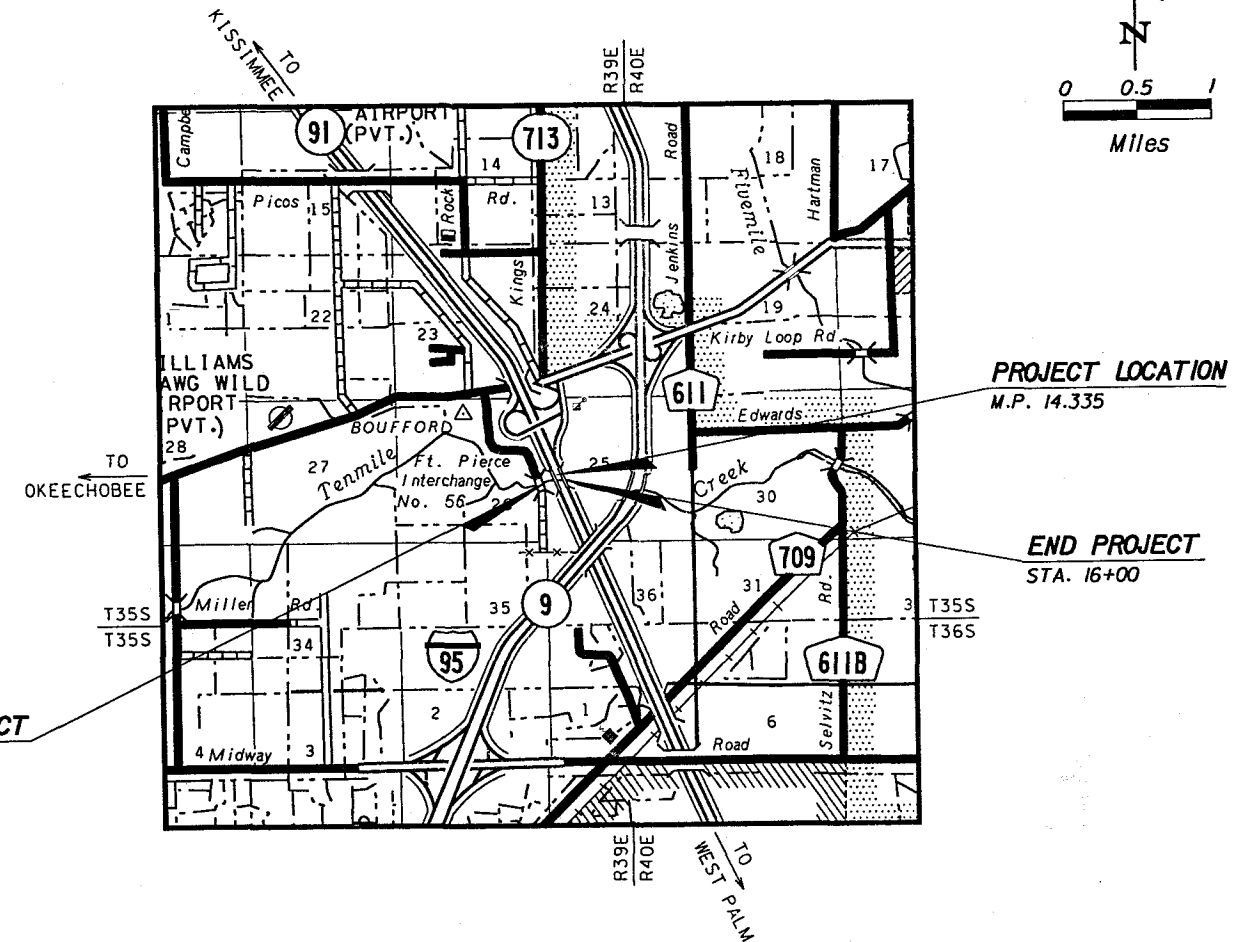
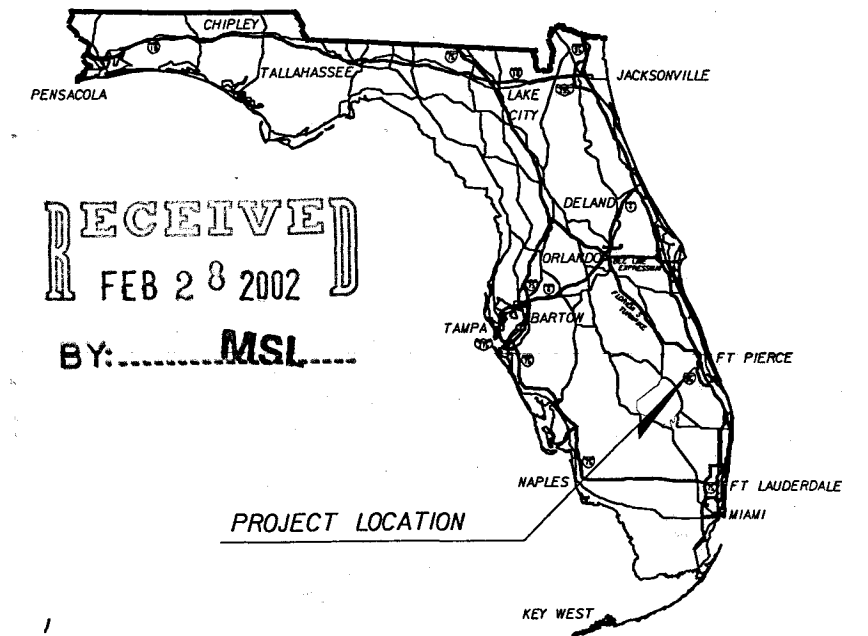
STATE OF FLORIDA  
DEPARTMENT OF TRANSPORTATION

CONTRACT PLANS

FINANCIAL PROJECT ID 409327-1-52-01  
FLORIDA'S TURNPIKE  
ST. LUCIE COUNTY (94470)  
BRIDGE EMBANKMENT PROTECTION  
AT  
TEN MILE CREEK - BRIDGE # 940072

INDEX OF PLANS

SHEET NO.	SHEET DESCRIPTION
1	KEY SHEET
2	SUMMARY OF PAY ITEMS
3	SUMMARY OF QUANTITIES
4	SUMMARY OF DRAINAGE STRUCTURES
5	GENERAL NOTES & REFERENCE POINTS
6 - 9	TYPICAL SECTION AND DETAILS
10	DRAINAGE DETAILS
11	PLAN SHEET
12	DRAINAGE STRUCTURE SHEET
13	PROFILE
14 - 28	CROSS SECTIONS
29 - 31	EROSION CONTROL
32	TRAFFIC CONTROL SHEET



ROADWAY SHOP DRAWINGS  
TO BE SUBMITTED TO:  
  
ROBERT BERKOWITZ, P.E.  
PITMAN HARTENSTEIN AND ASSOC., INC.  
7820 ARLINGTON EXPWY, SUITE 640  
JACKSONVILLE, FLORIDA 32211

PLANS PREPARED BY:  
  
PITMAN HARTENSTEIN AND ASSOC., INC.  
7820 ARLINGTON EXPWY, SUITE 640  
JACKSONVILLE, FLORIDA 32211



VENDOR No. 59-2695553

NOTE: THE SCALE OF THESE PLANS  
MAY HAVE CHANGED BY REPRODUCTION.

CONTRACT No. C-7799

*Handwritten signature and date: 2/28/02*

GOVERNING STANDARDS AND SPECIFICATIONS:  
FLORIDA DEPARTMENT OF TRANSPORTATION,  
ROADWAY AND TRAFFIC DESIGN STANDARDS  
DATED JANUARY 2000, AND  
STANDARDS SPECIFICATIONS FOR ROAD AND BRIDGE  
CONSTRUCTION DATED 2000,  
AS AMENDED BY CONTRACT DOCUMENTS.

REVISIONS

LENGTH OF PROJECT		
	LINEAR FEET	MILES
ROADWAY	N/A	N/A
BRIDGES	N/A	N/A
NET LENGTH OF PROJECT	600	0.11
EXCEPTIONS	N/A	N/A
GROSS LENGTH OF PROJECT	600	0.11

GEC PROJECT MANAGER: MIKE SASSER, PE  
FDOT PROJECT MANAGER: WILLIAM F. SLOUP, PE

KEY SHEET REVISIONS		
DATE	BY	DESCRIPTION

ROADWAY PLANS  
ENGINEER OF RECORD: ROBERT BERKOWITZ, P.E.

P.E. NO. 39216

FISCAL YEAR	SHEET NO.
03	E-48

100% SUBMITTAL



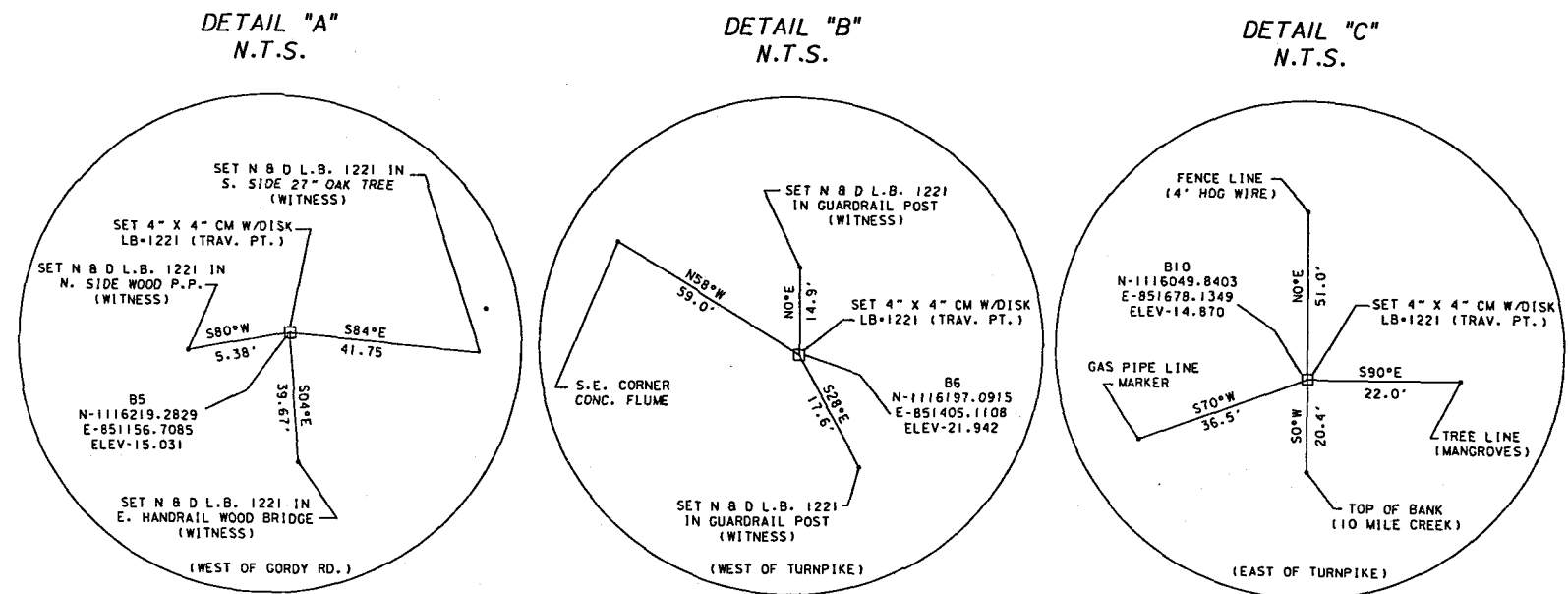




# GENERAL NOTES

1. ALL WORK AND MATERIALS SHALL BE IN COMPLETE ACCORDANCE WITH ALL RELATIVE SECTIONS OF "FDOT STANDARD SPECIFICATIONS" (LATEST REVISION) AND ALL FDOT STANDARD DETAILS (LATEST REVISION)
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND INSURANCE REQUIRED FOR THE PROJECT.
3. ANY PUBLIC LAND CORNER WITHIN THE LIMITS OF CONSTRUCTION IS TO BE PROTECTED. IF A CORNER MONUMENT IS IN DANGER OF BEING DESTROYED AND HAS NOT BEEN PROPERLY REFERENCED, THE PROJECT ENGINEER SHOULD NOTIFY THE SURVEYOR WITHOUT DELAY BY TELEPHONE
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL/DISPOSAL OF ANY UNSUITABLE MATERIAL ENCOUNTERED DURING CONSTRUCTION, AND THE FURNISHING AND COMPACTING OF SUITABLE REPLACEMENT BACKFILL MATERIAL. COST TO BE INCLUDED WITH THE REGULAR EXCAVATION.
5. UTILITIES TO BE ADJUSTED BY OTHERS AS DIRECTED BY THE ENGINEER.
6. ALL EXISTING DRAINAGE STRUCTURES WITHIN THE LIMITS OF CONSTRUCTION SHALL REMAIN UNLESS NOTED "TO REMOVED".
7. THE CONTRACTOR SHALL GIVE 48-HOURS NOTICE TO THE FT. PIERCE OFFICE AT 461-464-2831 PRIOR TO ANY WORK NEAR FGT FACILITIES. THE LOCATION OF FGT'S FACILITIES SHOWN ON THE DRAWINGS IS APPROXIMATE. A FIELD SURVEY MUST BE DONE TO DETERMINE THE ACTUAL LOCATION OF FGT'S FACILITIES. A FGT REPRESENTATIVE MUST BE PRESENT WHEN ANY WORK IS DONE WITHIN 10" OF PHYSICALLY VERIFIED FGT FACILITY. MACHINE EXCAVATION MAY NOT BE PERFORMED WITHIN 5" OF A PHYSICALLY VERIFIED FGT FACILITY. A FGT REPRESENTATIVE WILL PERFORM THE PHYSICAL VERIFICATION
8. THE CONTRACTOR SHALL INSTALL EROSION CONTROL MEASURES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL MAINTAIN DRAINAGE DURING CONSTRUCTION TO ENSURE SAFE VEHICULAR OPERATIONS AS WELL AS TO PROHIBIT ANY UNDUE IMPACT TO OFF-SITE DRAINAGE OR OTHER ENVIRONMENTAL CONCERNS
9. IN CASE OF RAIN-OUTS AND/OR WASH-OUTS, THE CONTRACTOR SHALL REDO THE WORK AREA WITH FULL OBSERVANCE OF LAIDEN- RUNOFF AND EROSION CONTROL METHODS.
10. THE CONTRACTOR SHALL COVER ALL DISTURBED AREAS, IMMEDIATELY AFTER CONSTRUCTION, WITH THE PERMANENT COVERING TYPE SHOWN IN THE PLANS AT THAT LOCATION.

- |   |  |                     |  |                                |   |
|---|--|---------------------|--|--------------------------------|---|
| <ol style="list-style-type: none"> <li>11. <u>UTILITY OWNERS</u></li> </ol> | <table border="0" style="width: 100%;"> <tr> <td style="text-align: left;"><u>PHONE NUMBER</u></td> </tr> <tr> <td>BELL SOUTH - S.A. ROBERTS (561) 468-5513</td> </tr> <tr> <td>AT&amp;T - BILL HAM (407) 248-3445</td> </tr> <tr> <td>FLORIDA GAS TRANSMISSION - JOE SANCHEZ (407) 838-7171</td> </tr> </table> | <u>PHONE NUMBER</u> | BELL SOUTH - S.A. ROBERTS (561) 468-5513 | AT&T - BILL HAM (407) 248-3445 | FLORIDA GAS TRANSMISSION - JOE SANCHEZ (407) 838-7171 |
| <u>PHONE NUMBER</u>   |  |                     |  |                                |   |
| BELL SOUTH - S.A. ROBERTS (561) 468-5513                                    |  |                     |  |                                |   |
| AT&T - BILL HAM (407) 248-3445  |  |                     |  |                                |   |
| FLORIDA GAS TRANSMISSION - JOE SANCHEZ (407) 838-7171                       |  |                     |  |                                |   |
12. CALL THE SUNSHINE STATE AT 1-800-432-4770, AND THE UTILITIES OWNERS 48 HOURS PRIOR BEGINNEIN WORK
  13. IT IS THE RESPONSIBLTY OF THE CONTRACTOR TO ENSURE THAT THE CONSTRUCTION METHODS USED ON THE PROJECT AREA DOES NOT CAUSE ANY DISCHARGE OF TURBID WATER DOWNSTREAM OF THE PROJECT AREA. THE CONTRACTOR SHALL MAKE ANY PROVISIONS NECESSARY TO ENSURE COMPLIANCE WITH ENVIRONMENTAL AGENCY REQUIREMENTS. NO ADDITIONAL COMPENSATION BEYOND THOSE MEASURES INDICATED IN THE PLANS WILL BE MADE FOR COMPLIANCE WITH THESE REQUIREMENTS.
  14. BURNING OF MATERIALS AND/OR DEBRIS AS MEANS OF DISPOSAL IS PROHIBITED WITHIN THE PROJECT LIMITS. CONTRACTOR SHALL DISPOSE OF ALL CLEARED AND GRUBBED MATERIAL OFF-SITE.
  15. REMOVAL & DISPOSAL OF EXISTING FENCE TO BE INCLUDED IN THE COST OF SITE PREPARATION.
  16. FENCE TO BE REPLACED SHOULD BE EQUAL TO EXISTING PROPERTY FENCE TYPE.
  17. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO PERSONAL PROPERTY DUE TO CONSTRUCTION ACTIVITIES AND SHALL PERFORM ANY AND ALL REPAIRS AT THE CONTRACTORS EXPENSE.
  18. ALL SUITABLE EXCAVATED MATERIAL MAY BE UTILIZED AS BACKFILL WITH THE APPROVAL OF THE PROJECT ENGINEER.



REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

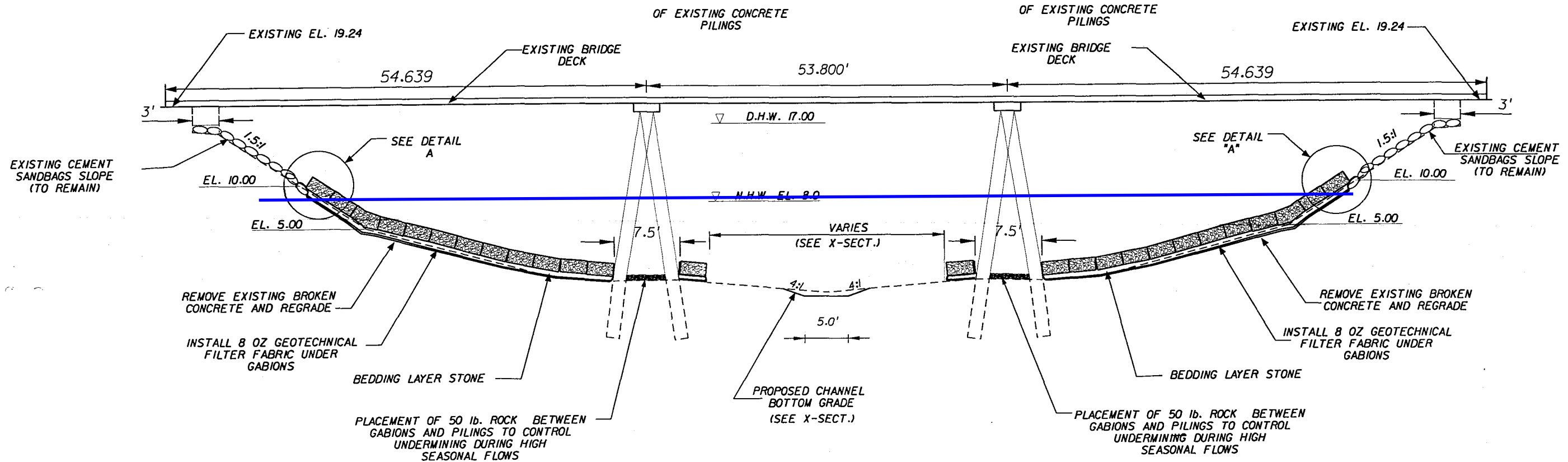


STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
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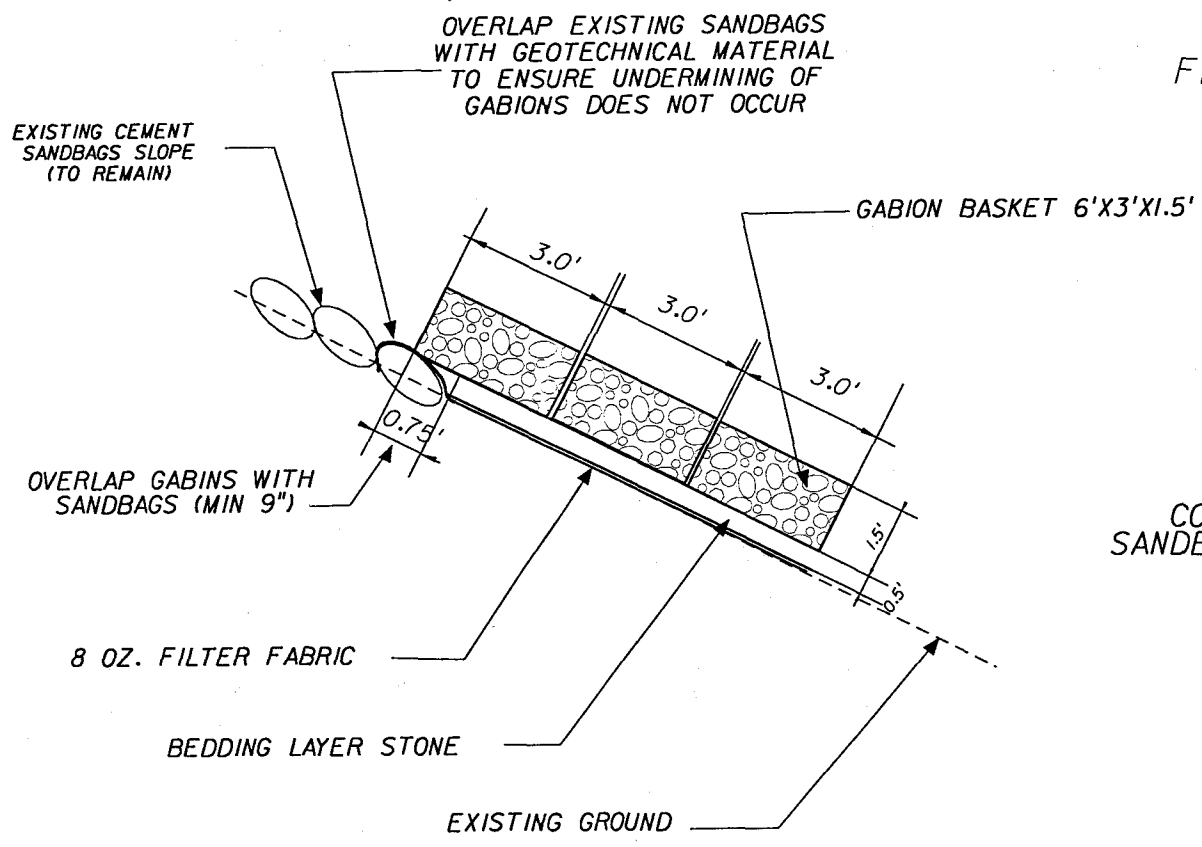
**GENERAL NOTES &  
SURVEY REFERENCE POINTS**

SHEET NO.  
**E-50**





CHANNEL TYPICAL SECTION  
UNDER BRIDGE  
FROM STA. 13+40 TO STA. 15+00  
(N.T.S.)



DETAIL "A"  
CONNECTION OF  
SANDBAGS TO GABIONS  
(N.T.S.)

NOTE: THIS TYPICAL IS TAKEN ALONG  
TURNPIKE BASE LINE

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

**PITMAN HARTENSTEIN & ASSOC., INC.**  
**ENGINEERS**  
JACKSONVILLE ▼ FORT MYERS ▼ TAMPA

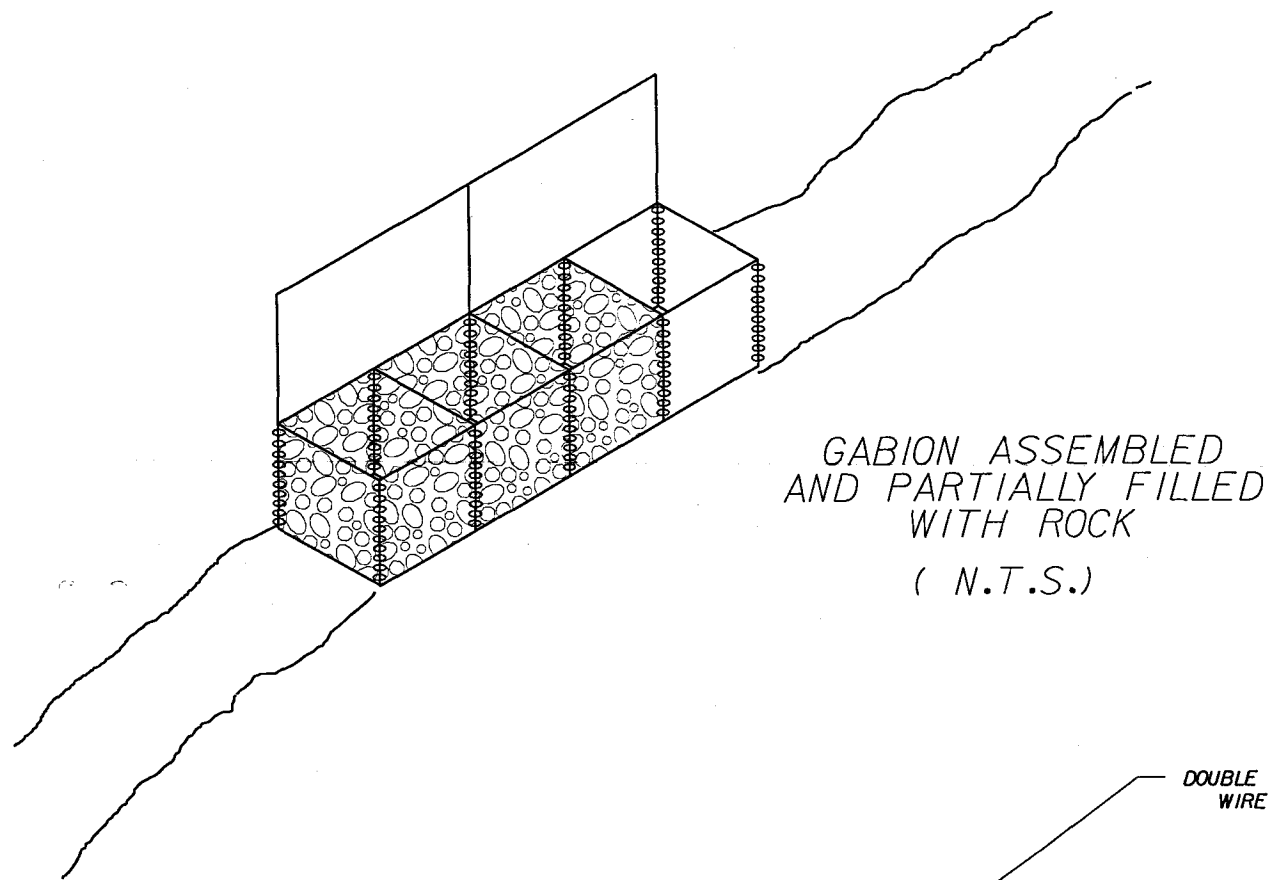
STATE OF FLORIDA  
DEPARTMENT OF TRANSPORTATION

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
91	ST. LUCIE	409327-1-52-01

**TYPICAL BRIDGE SECTION**  
**( EMBANKMENT PROTECTION )**

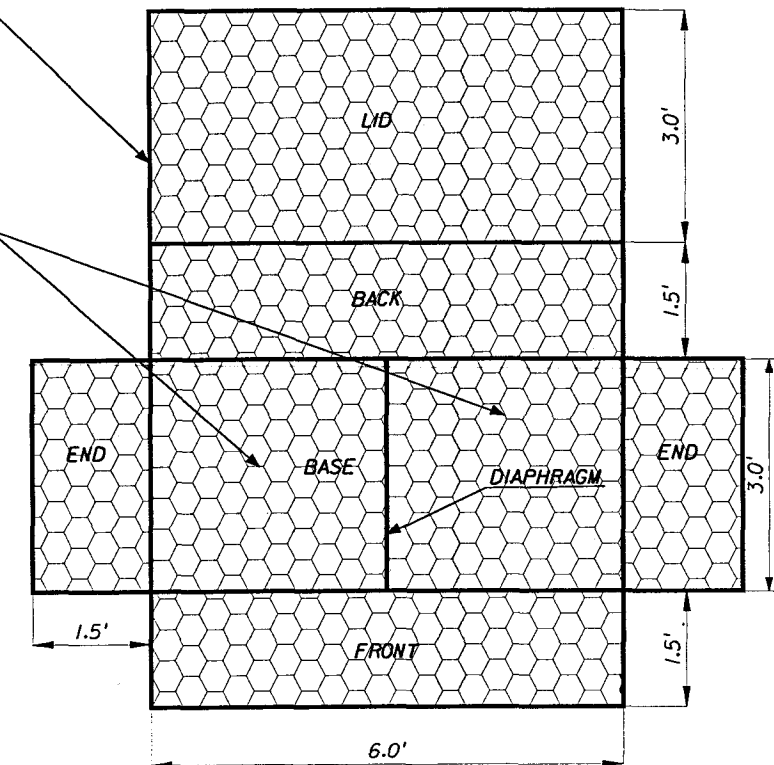
SHEET NO.  
**E-51**





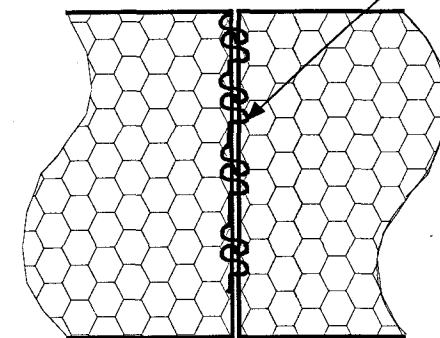
PVC COATED & GALVANIZED STEEL WIRE MESH BASKET

GABIONS ARE 2 CELL CONSTRUCTED

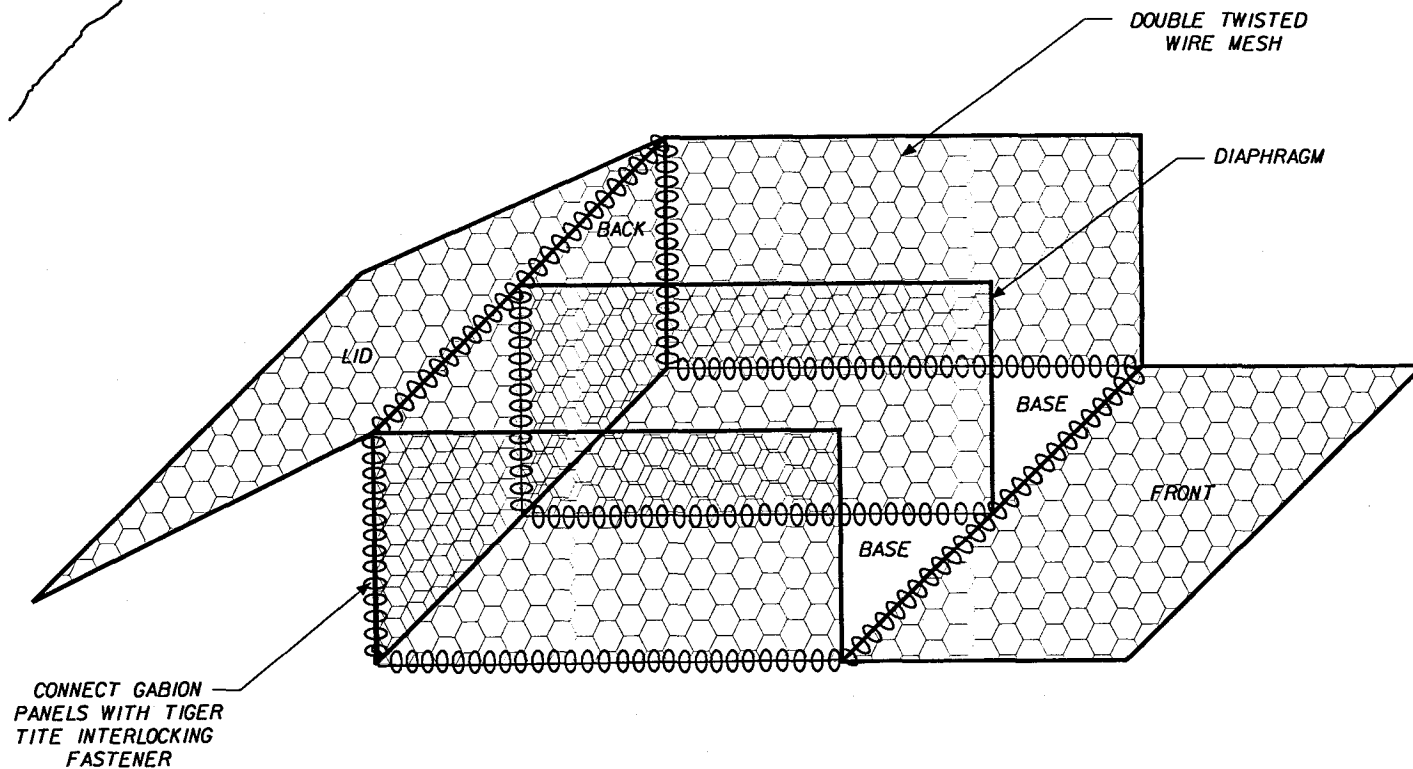


GABION UNASSEMBLED ( N.T.S.)

ALL BASKETS WILL BE CONNECTED WITH DOUBLE LOOP AND CONSTRUCTED OF DOUBLE TWISTED WIRE MESH AND CONNECTED IN ACCORDANCE WITH MANUFACTURERS SPECIFICATIONS



BASKETS CONNECTION DETAILS ( N.T.S.)



6 FT. X 3 FT. X 1.5 FT. GABION PARTIALLY ASSEMBLED ( N.T.S.)

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION



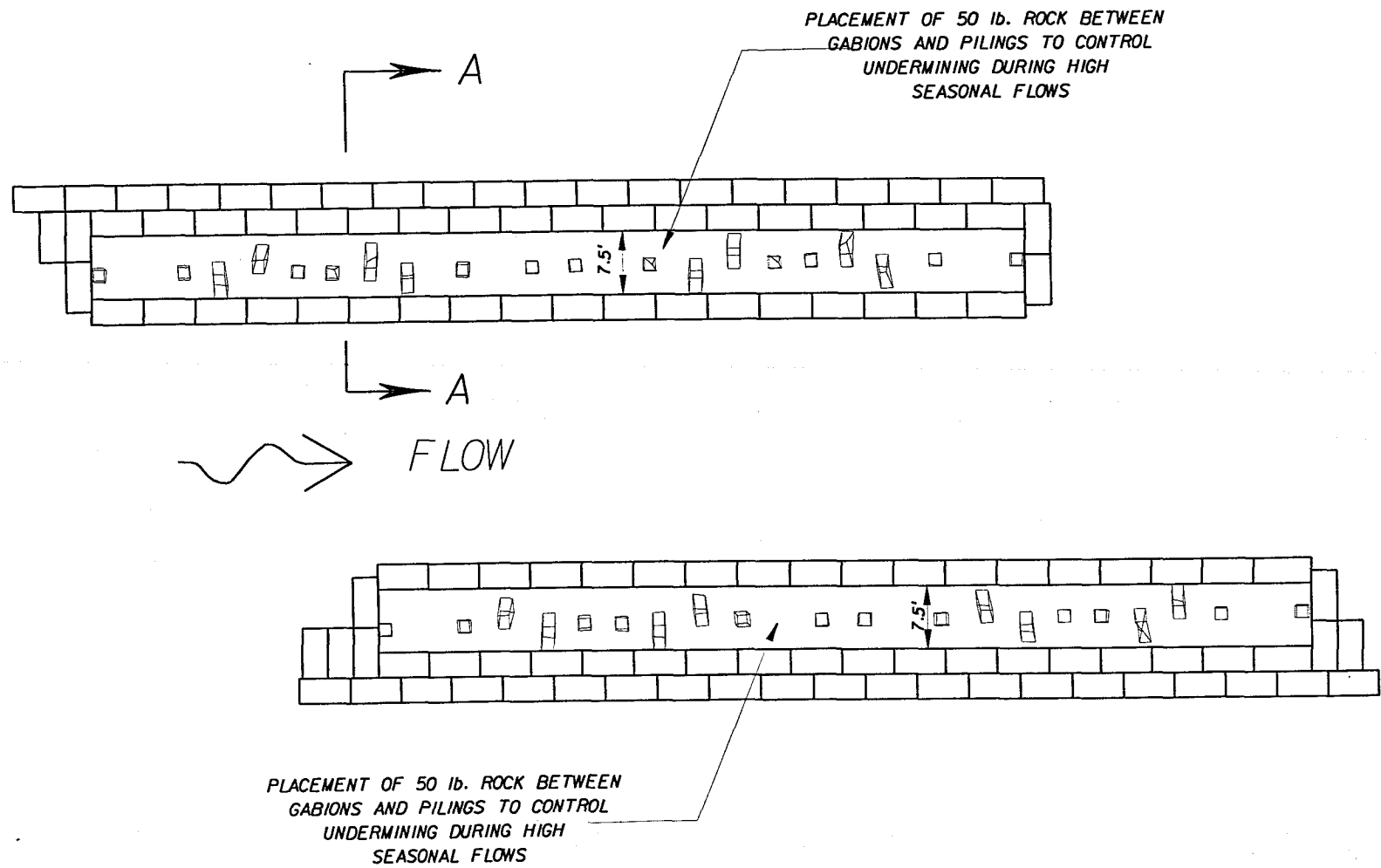
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
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**GABION ASSEMBLY DETAIL**

SHEET NO.

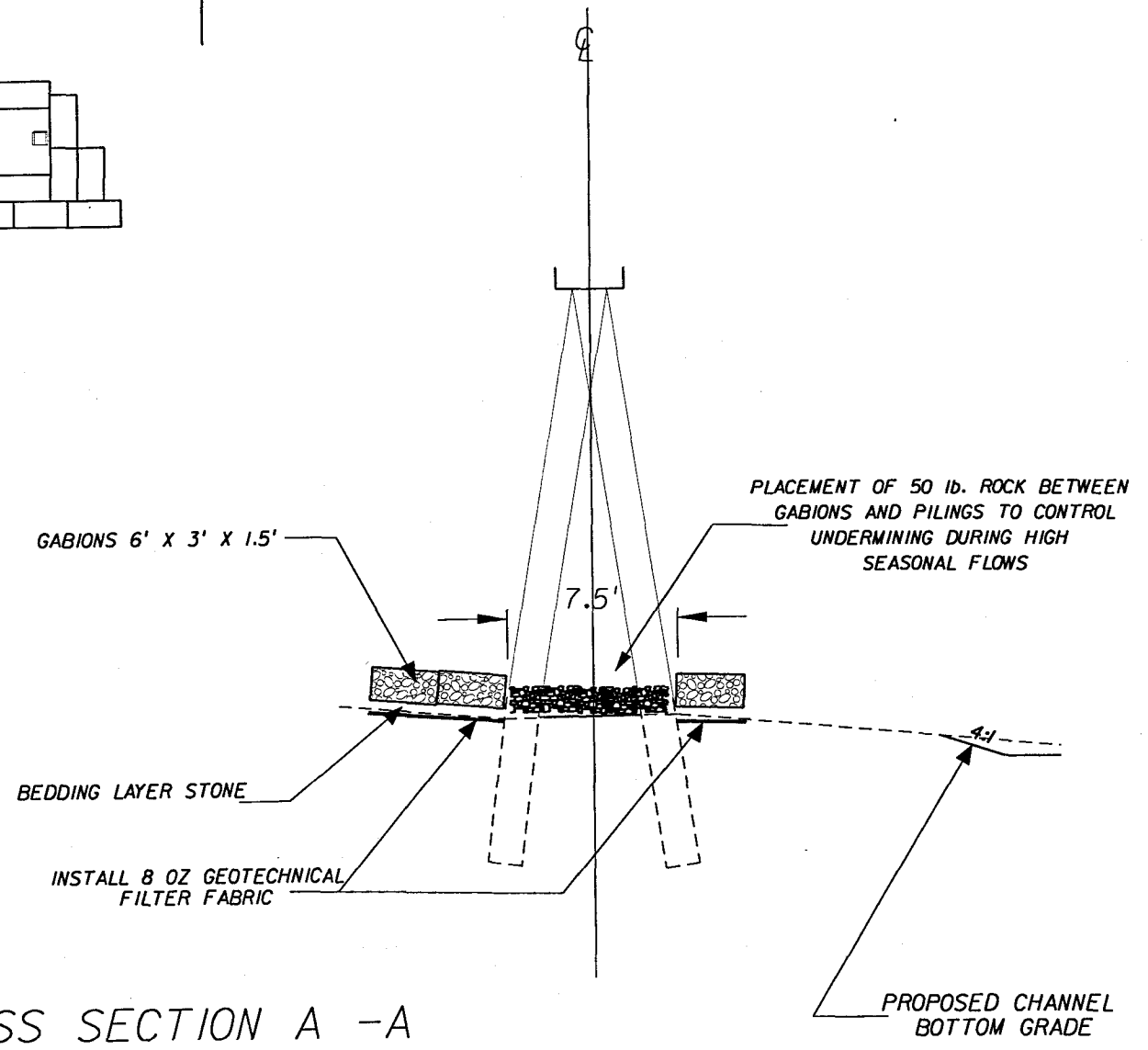
E-52





PLACEMENT OF 50 lb. ROCK BETWEEN GABIONS AND PILINGS TO CONTROL UNDERMINING DURING HIGH SEASONAL FLOWS

PLACEMENT OF 50 lb. ROCK BETWEEN GABIONS AND PILINGS TO CONTROL UNDERMINING DURING HIGH SEASONAL FLOWS



CROSS SECTION A - A

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

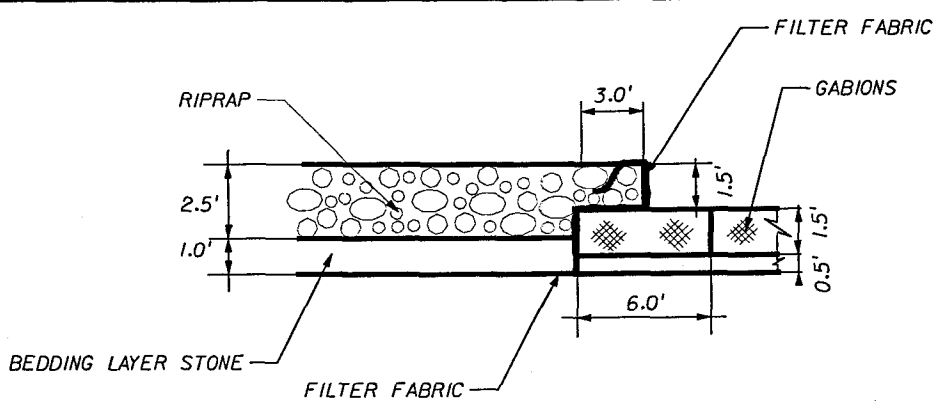
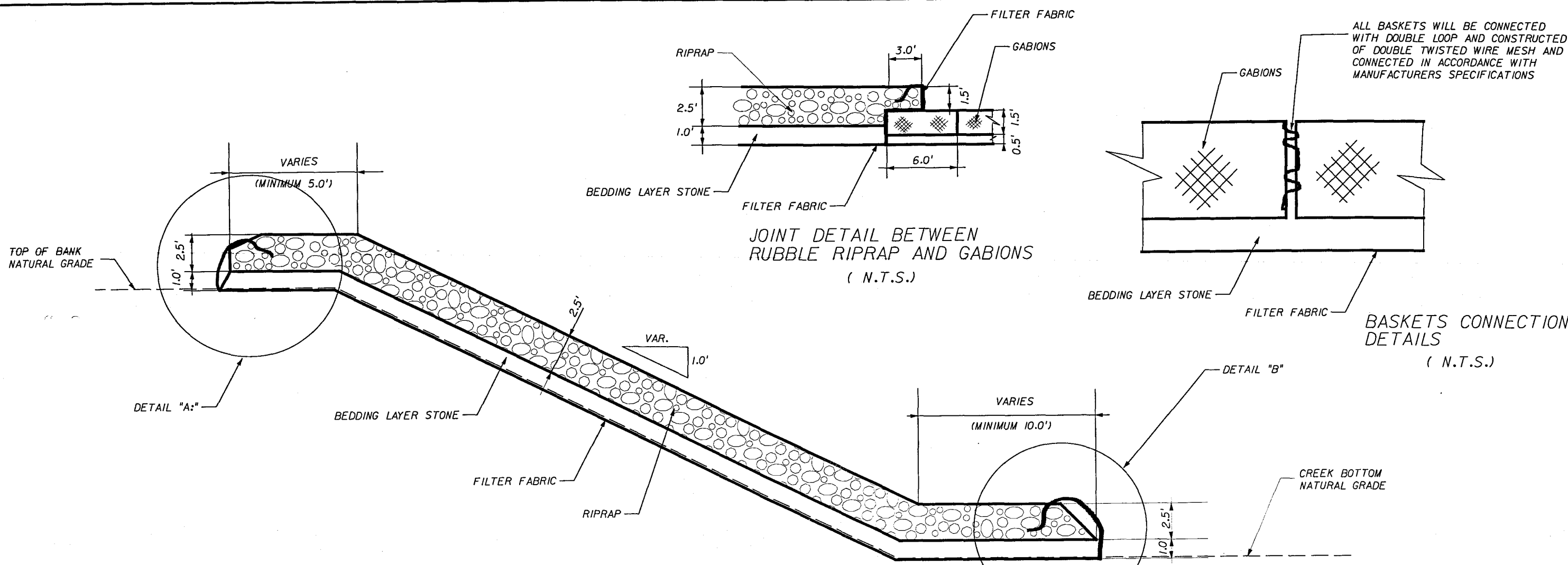
**PITMAN HARTENSTEIN & ASSOC., INC.**  
**ENGINEERS**  
 JACKSONVILLE ▼ FORT MYERS ▼ TAMPA

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
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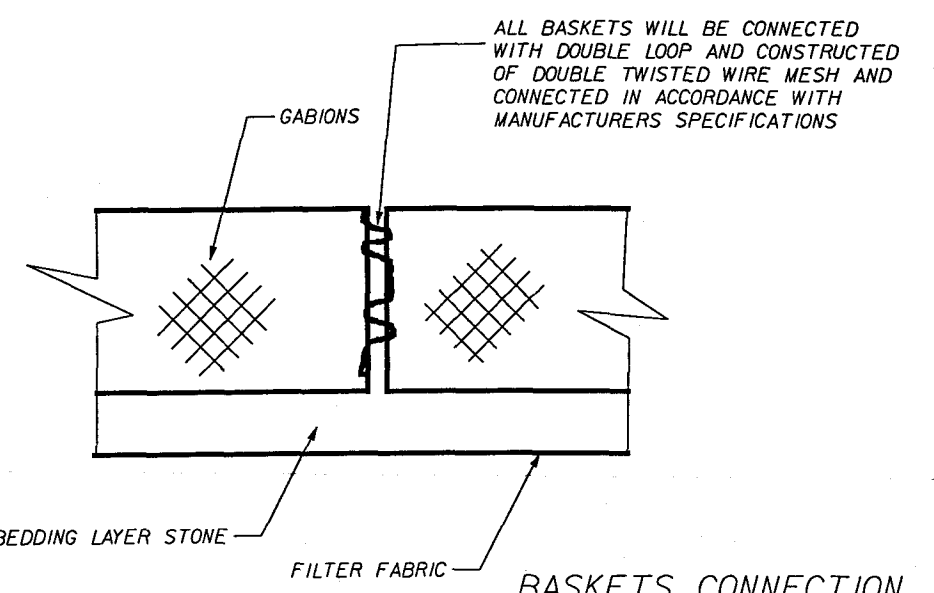
**GABION PLACEMENT WITH PILINGS**

SHEET NO.  
**E-53**



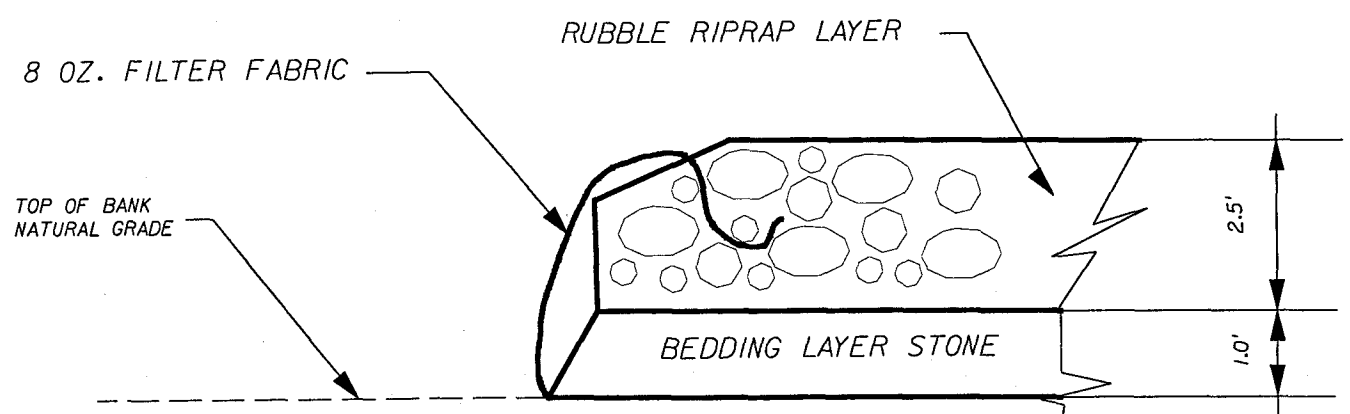


JOINT DETAIL BETWEEN  
RUBBLE RIPRAP AND GABIONS  
( N.T.S.)

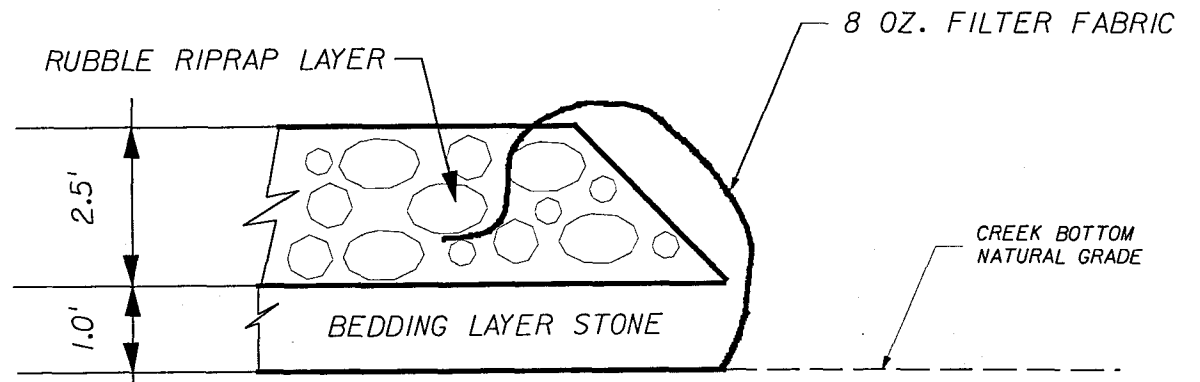


BASKETS CONNECTION  
DETAILS  
( N.T.S.)

RUBBLE RIPRAP SLOPE PROTECTION  
ADJACENT TO STREAM  
(BANK AND SHORE)  
( N.T.S.)



DETAIL "A"  
(N.T.S.)



DETAIL "B"  
(N.T.S.)

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

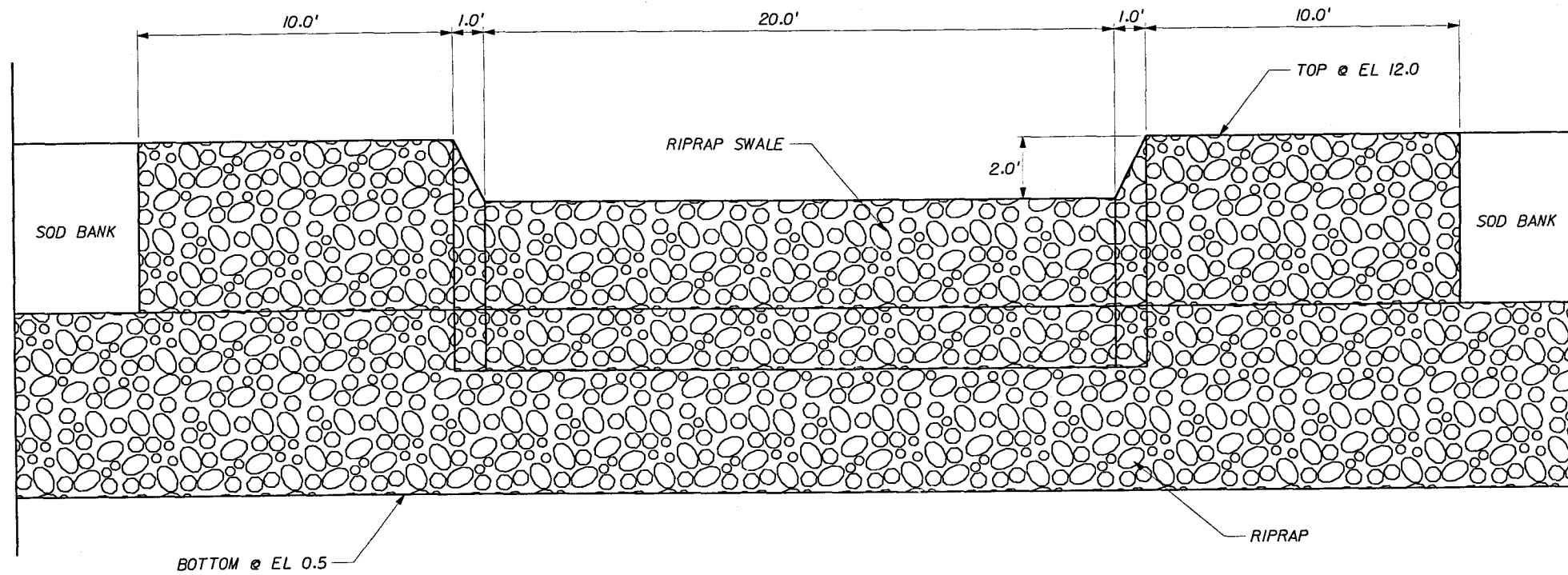


STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
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**RUBBLE RIPRAP - DETAILS**

SHEET NO.  
**E-54**

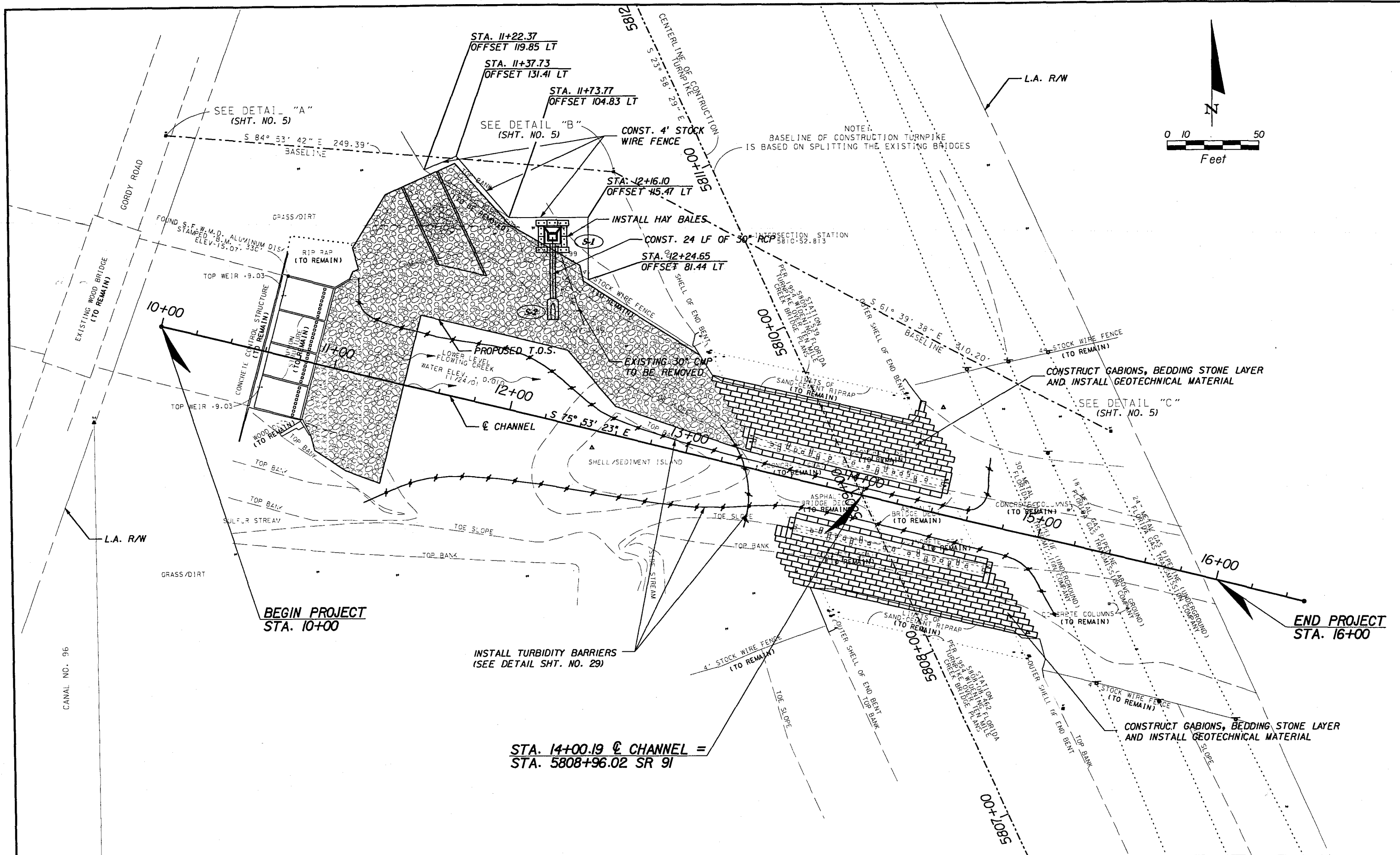




ELEVATION VIEW OF BANK@  
PROPOSED CANAL IMPROVEMENTS  
N.T.S.

REVISIONS							STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			<b>DRAINAGE DETAILS</b>	SHEET NO.
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION		ROAD NO.	COUNTY	FINANCIAL PROJECT ID		
							91	ST. LUCIE	409327-1-52-01		E-55





SEE DETAIL "A"  
(SHT. NO. 5)

SEE DETAIL "B"  
(SHT. NO. 5)

SEE DETAIL "C"  
(SHT. NO. 5)

NOTE:  
BASELINE OF CONSTRUCTION TURNPIKE  
IS BASED ON SPLITTING THE EXISTING BRIDGES

BEGIN PROJECT  
STA. 10+00

END PROJECT  
STA. 16+00

INSTALL TURBIDITY BARRIERS  
(SEE DETAIL SHT. NO. 29)

STA. 14+00.19 @ CHANNEL =  
STA. 5808+96.02 SR 91

DATE		BY	DESCRIPTION	DATE		BY	DESCRIPTION



STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
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PLAN SHEET

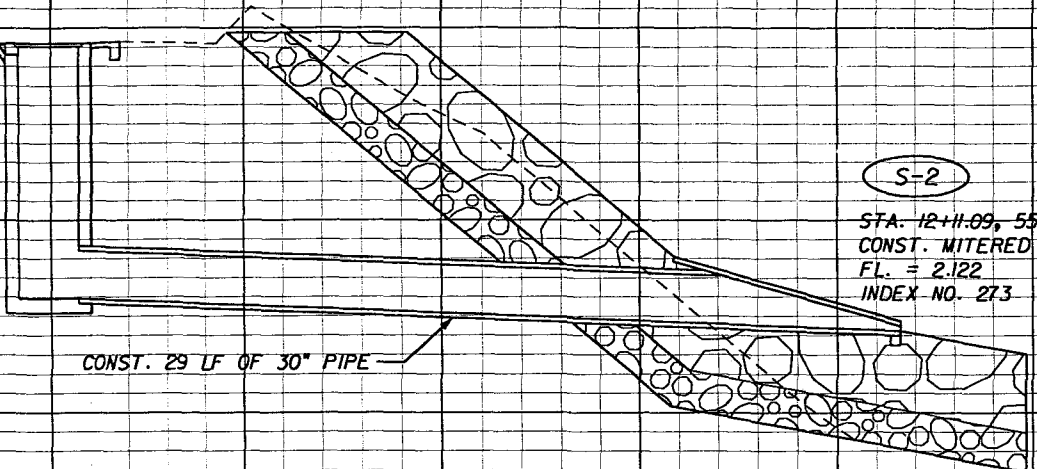
SHEET NO.  
E-56



CHANNEL

15  
10  
5  
0  
5

S-1  
STA. 11+99.80, 99.97 LT.  
CONST. DBI TYPE D W/ TRAV. SLOT  
GRATE EL. = 9.597  
INV. EL. = 2.958  
INDEX NO. 232



S-2  
STA. 12+11.09, 55.05 LT.  
CONST. MITERED END SECTION  
FL. = 2.122  
INDEX NO. 273

S-2  
STA. 12+11.09, 55.05 LT.  
CHANNEL

S-1  
STA. 11+99.80, 99.97 LT.  
CHANNEL

-130 -120 -110 -100 -90 -80 -70 -60 -50 -40 -30 -20 -10 0

DATE		BY	DESCRIPTION	REVISIONS		DATE	BY	DESCRIPTION



STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
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**DRAINAGE STRUCTURE SHEET**

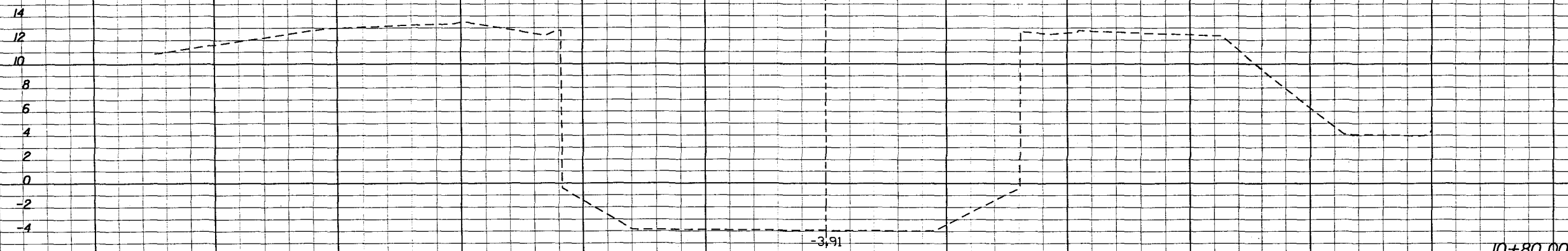
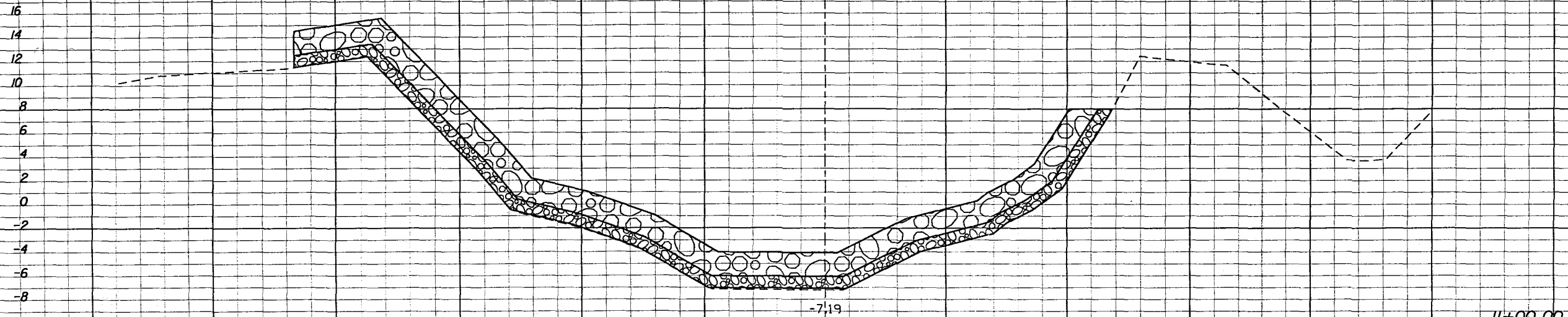
SHEET NO.  
**E-57**







CONSTRUCTION



Regular		Exc.		Embankment	
A	V	A	V	A	V
1				4	
		0			0

SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION



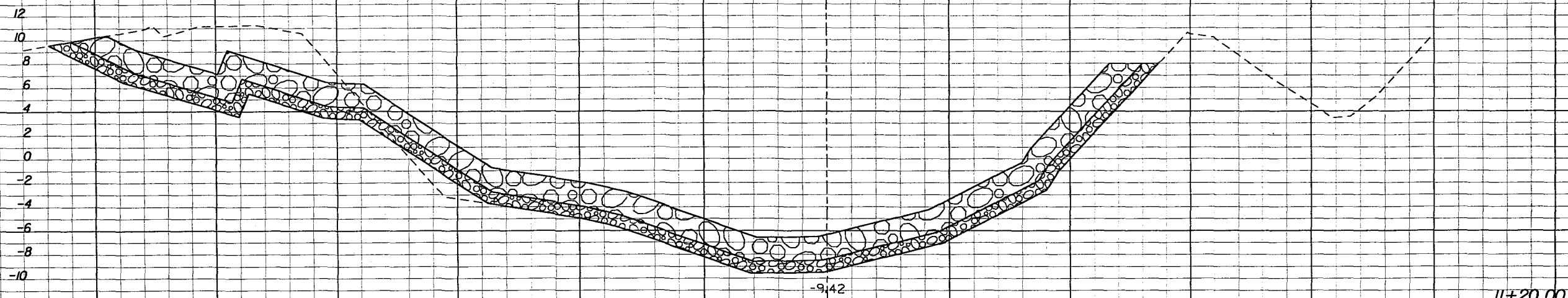
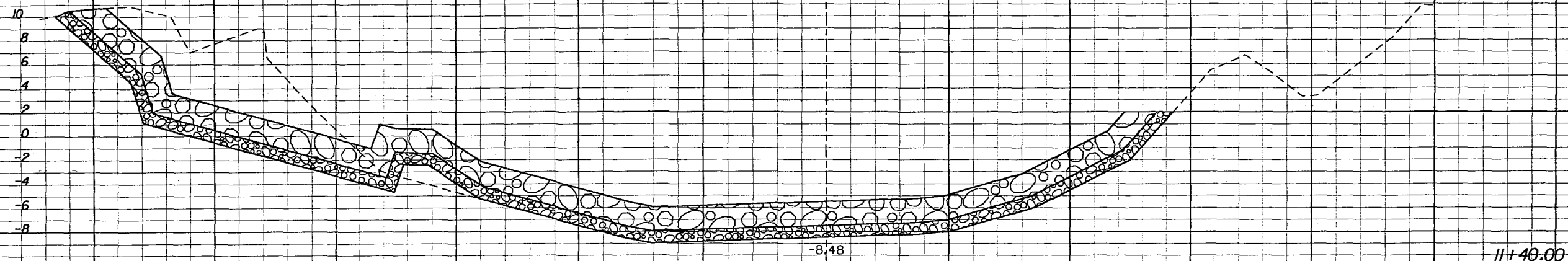
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
91	ST. LUCIE	409327-1-52-01

**CROSS SECTIONS**  
**BRIDGE EMBANKMENT PROTECTION**  
**TEN MILE CREEK**

SHEET NO.  
**E-59**



CONSTRUCTION



Regular		Exc.		Embankment	
A	V	A	V	A	V
332				13	
	216				10
250				13	
	93				7

SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION



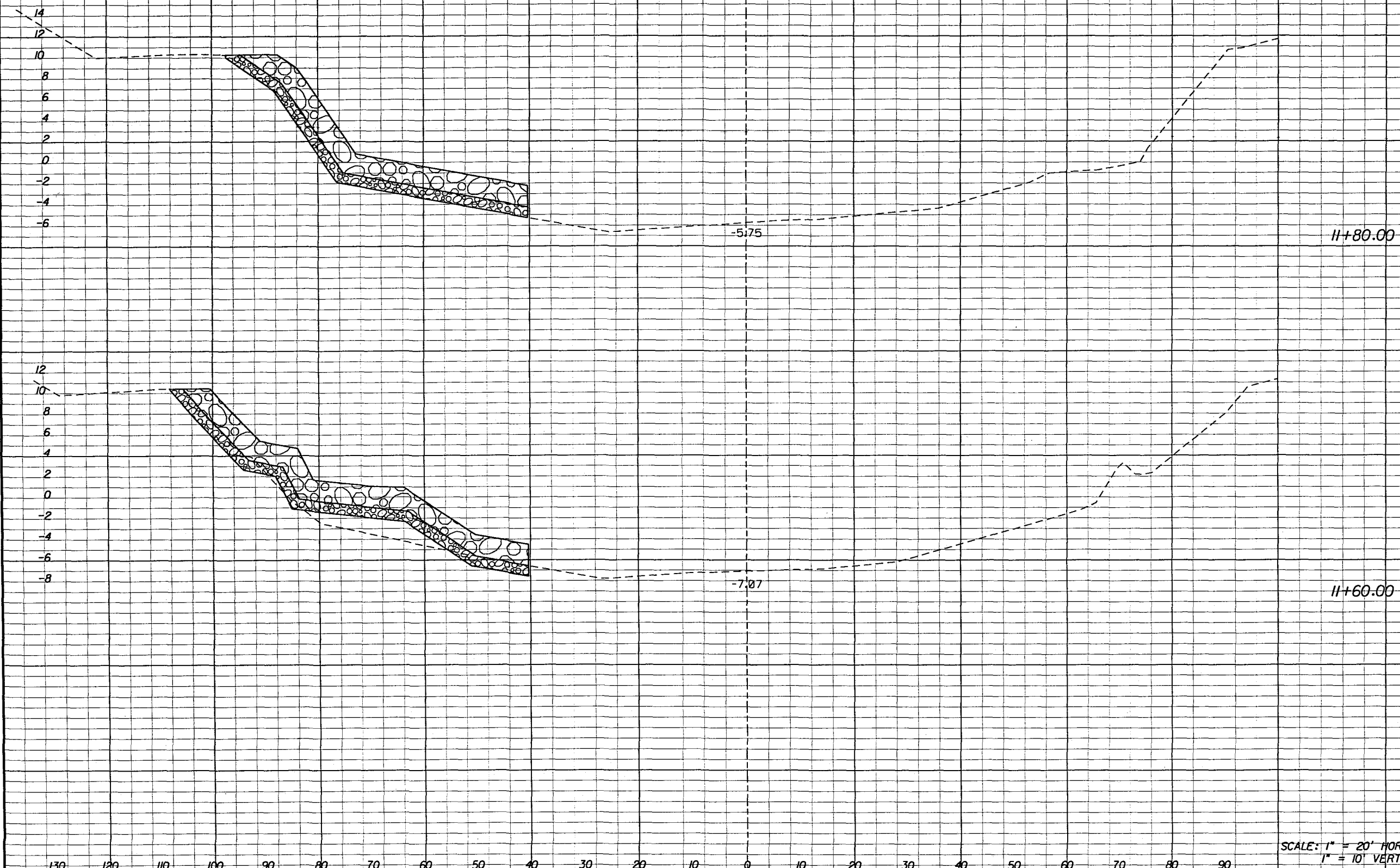
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
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**CROSS SECTIONS**  
**BRIDGE EMBANKMENT PROTECTION**  
**TEN MILE CREEK**

SHEET NO.  
**E-60**



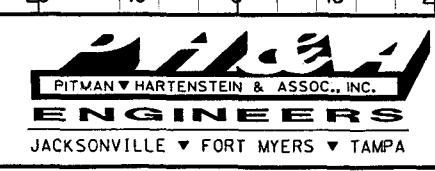
CONSTRUCTION



Regular		Exc.		Embankment	
A	V	A	V	A	V
0				0	
		8			13
21				34	
		131			17

SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION



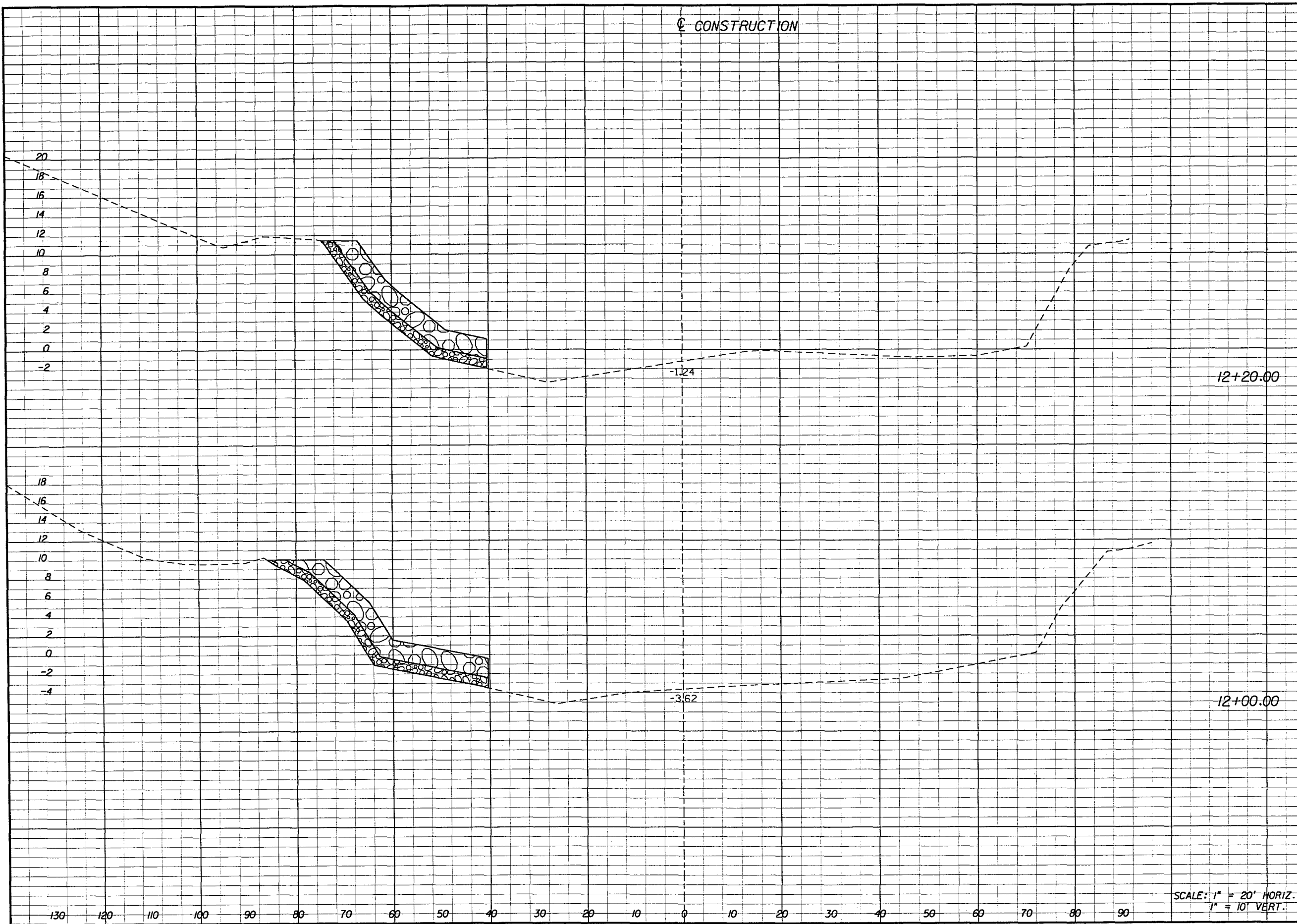
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
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**CROSS SECTIONS**  
**BRIDGE EMBANKMENT PROTECTION**  
**TEN MILE CREEK**

SHEET NO.  
**E-61**



CONSTRUCTION



Regular		Exc.		Embankment	
A	V	A	V	A	V
0				0	
		0			0
0				0	
		0			0

SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION



STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
91	ST. LUCIE	409327-1-52-01

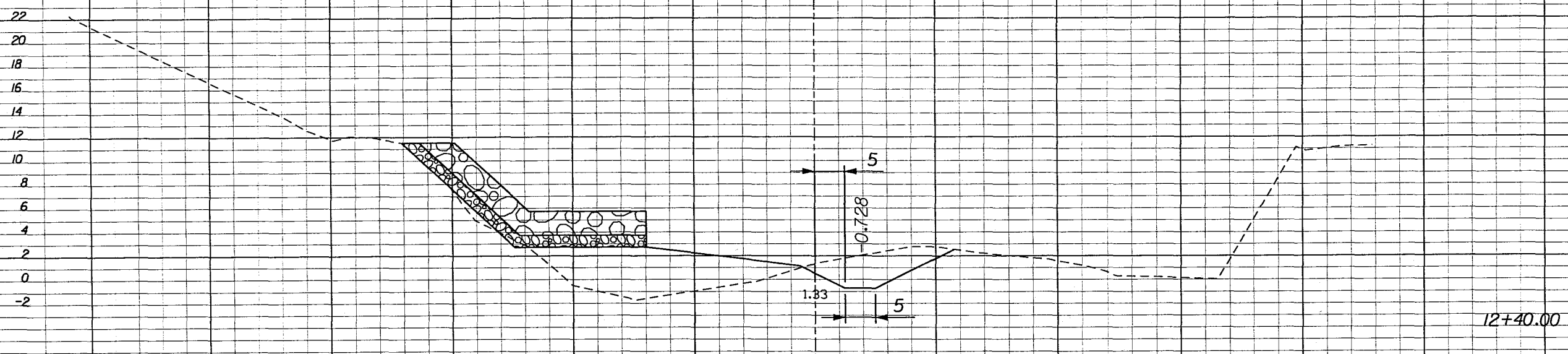
**CROSS SECTIONS**  
**BRIDGE EMBANKMENT PROTECTION**  
**TEN MILE CREEK**

SHEET NO.  
**E-62**



CONSTRUCTION

Regular	Exc.	Embankment	
A	V	A	V



132

112

12+40.00

49

42

SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

REVISIONS			
DATE	BY	DESCRIPTION	DATE



STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
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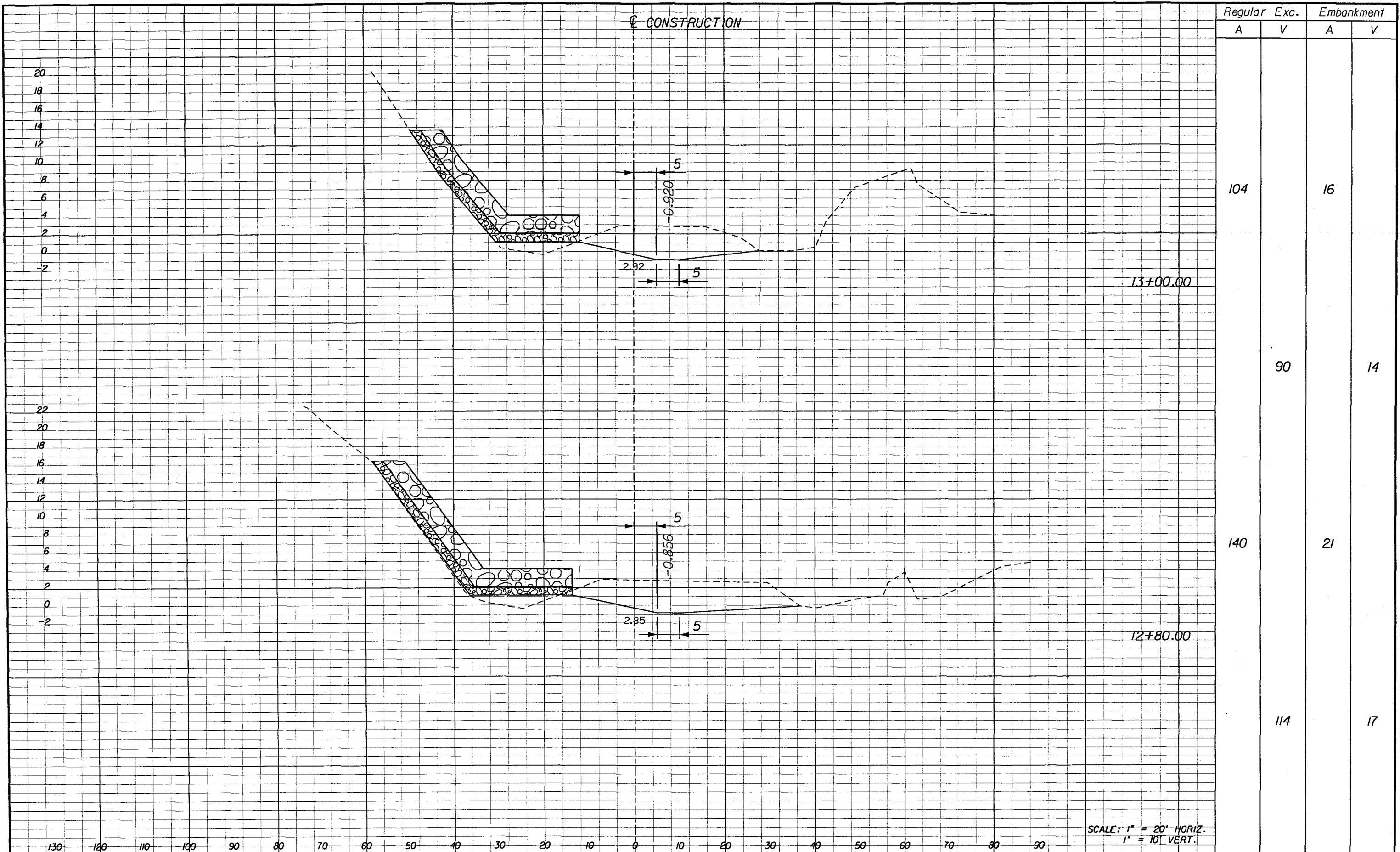
**CROSS SECTIONS**  
**BRIDGE EMBANKMENT PROTECTION**  
**TEN MILE CREEK**

SHEET NO.  
**E-63**









Regular		Exc.		Embankment	
A	V	A	V	A	V
104				16	
	90				14
140				21	
	114				17

SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

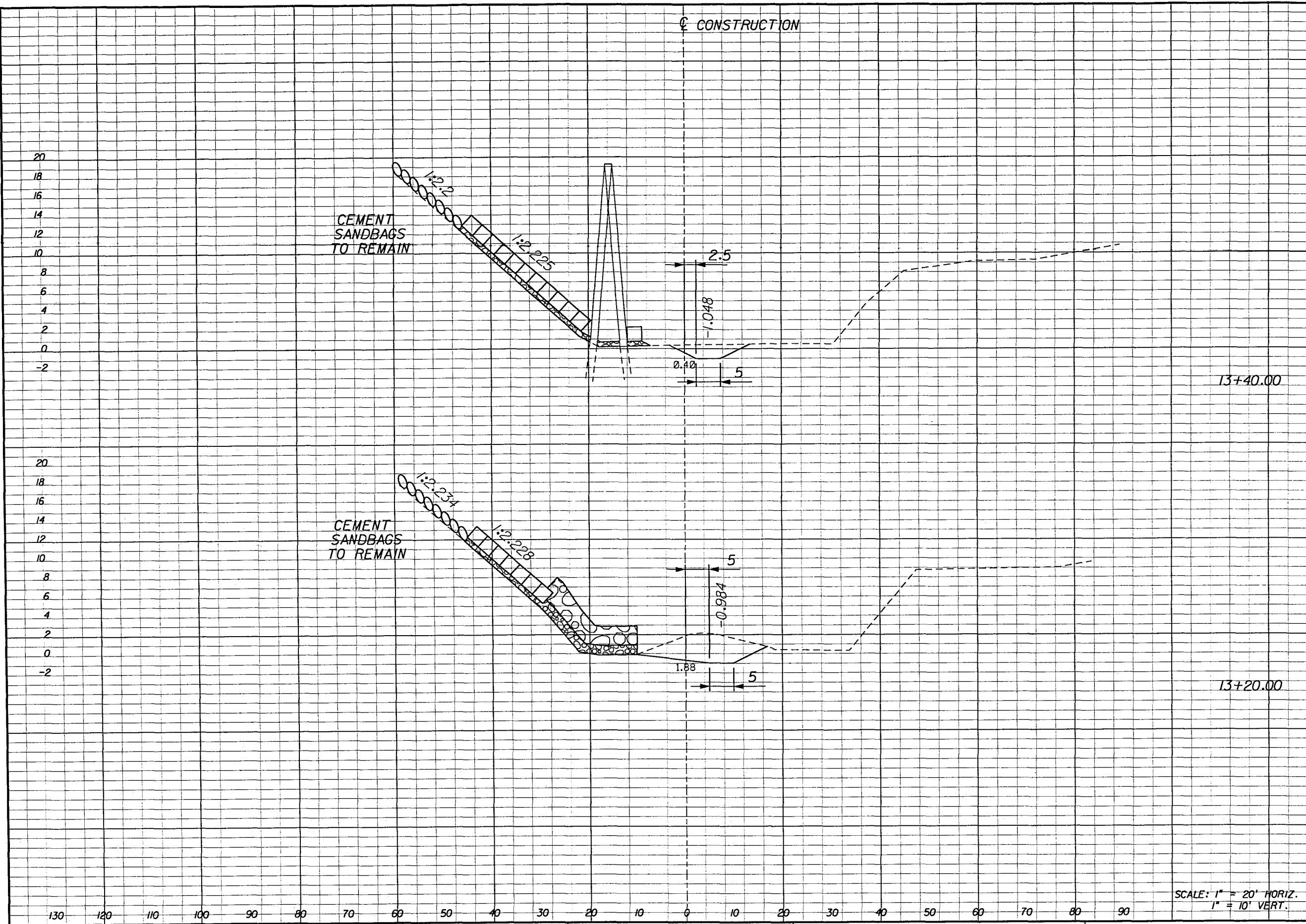


STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
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**CROSS SECTIONS**  
**BRIDGE EMBANKMENT PROTECTION**  
**TEN MILE CREEK**

SHEET NO.  
**E-65**





Regular		Exc.		Embankment	
A	V	A	V	A	V
	16		0		
		25			0
	51		0		
		57			6

SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

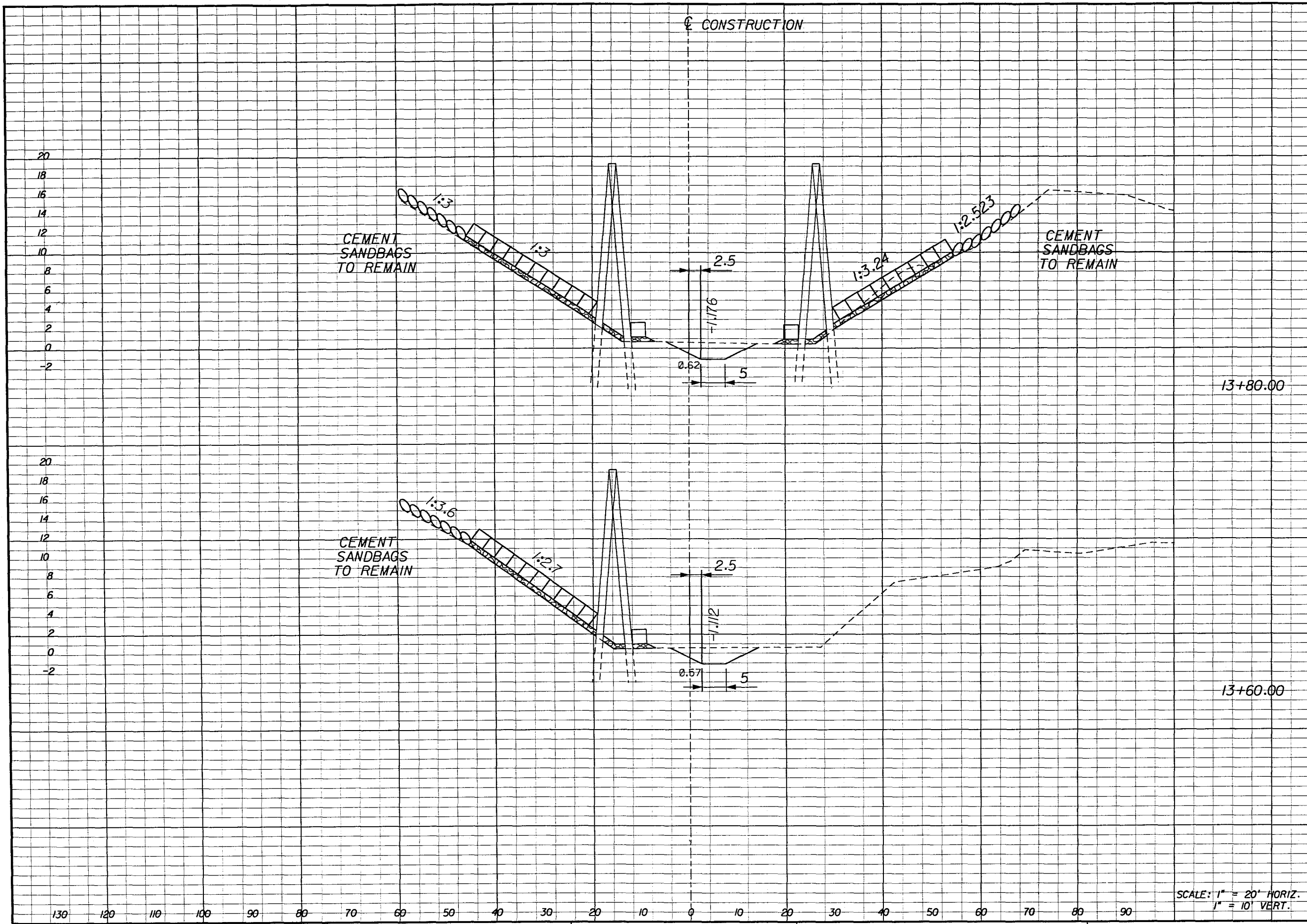


STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
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**CROSS SECTIONS**  
**BRIDGE EMBANKMENT PROTECTION**  
**TEN MILE CREEK**

SHEET NO.  
**E-66**





CONSTRUCTION

Regular		Exc.		Embankment	
A	V	A	V	A	V
42				0	
	23			0	
20				0	
	13			0	

SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION



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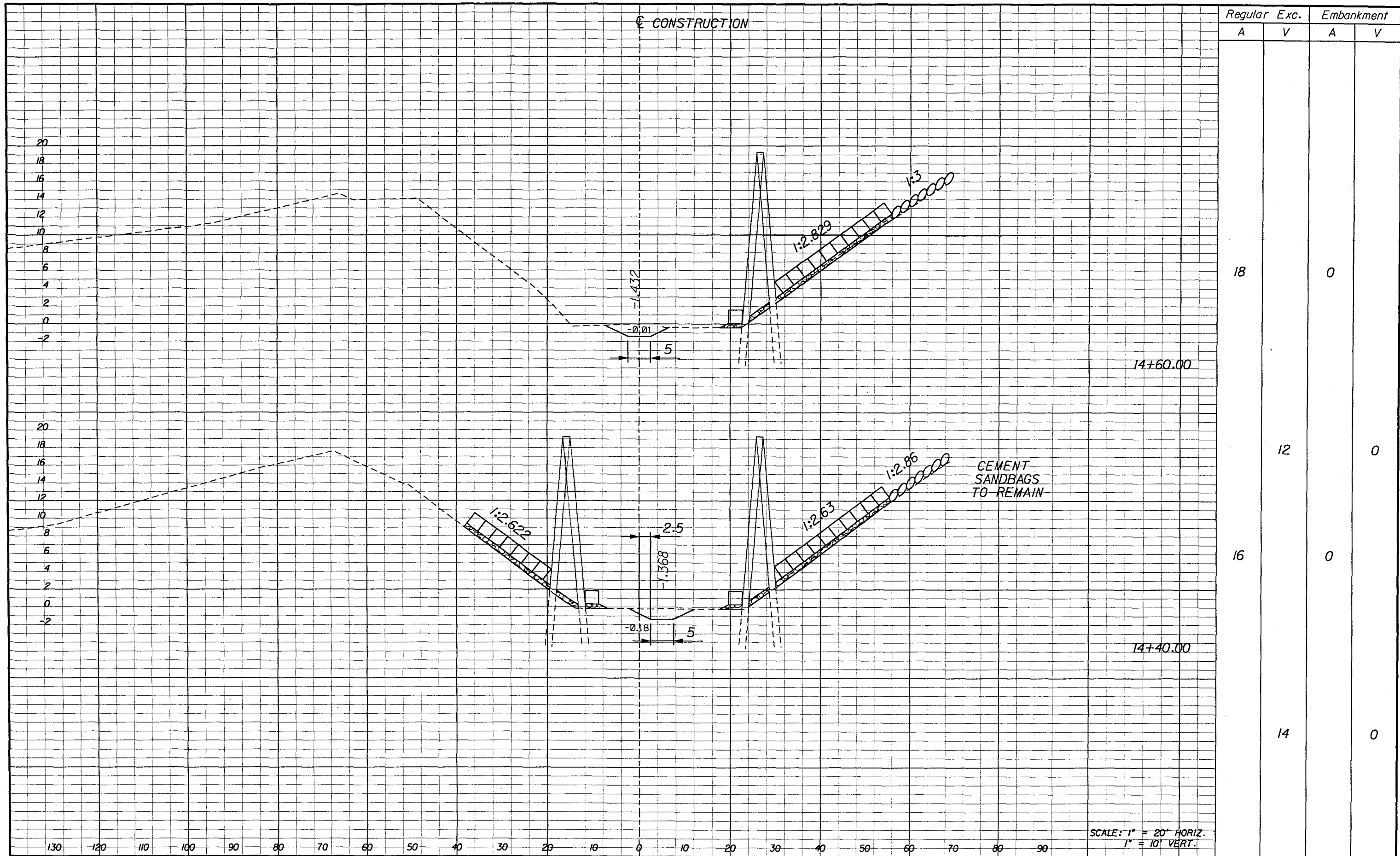
**CROSS SECTIONS**  
**BRIDGE EMBANKMENT PROTECTION**  
**TEN MILE CREEK**

SHEET NO.  
**E-67**









SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

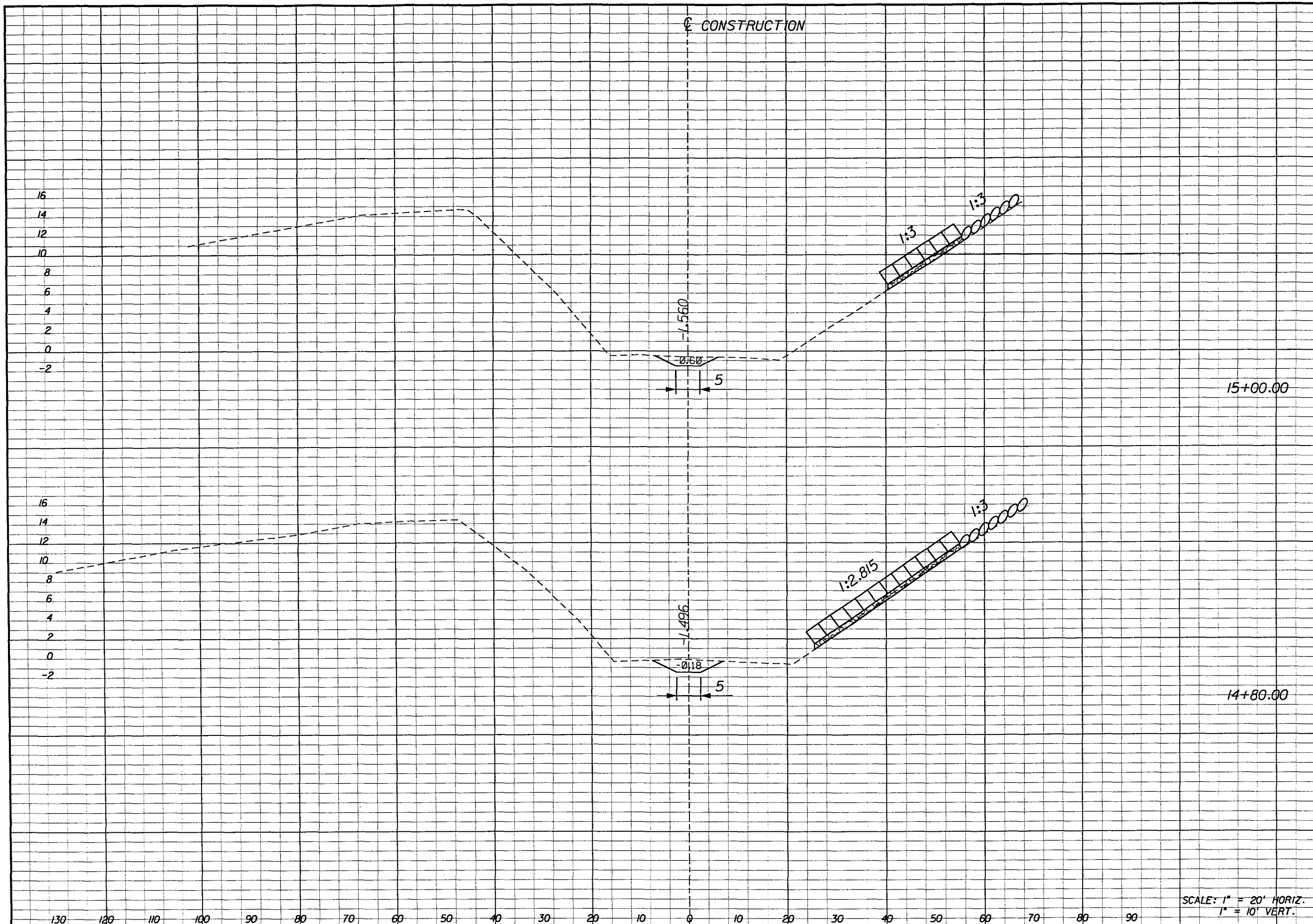


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**CROSS SECTIONS  
BRIDGE EMBANKMENT PROTECTION  
TEN MILE CREEK**

SHEET NO.  
**E-69**





Regular		Exc.		Embankment	
A	V	A	V	A	V
9				0	
	9				0
17				0	
	13				0

SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION



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**CROSS SECTIONS**  
**BRIDGE EMBANKMENT PROTECTION**  
**TEN MILE CREEK**

SHEET NO.  
**E-70**

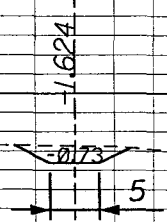


CONSTRUCTION

Regular Exc. Embankment  
A V A V

22  
20  
18  
16  
14  
12  
10  
8  
6  
4  
2  
0  
-2

130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90



15+20.00

8

0

6

0

SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

REVISIONS					
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**CROSS SECTIONS**  
**BRIDGE EMBANKMENT PROTECTION**  
**TEN MILE CREEK**

SHEET NO.  
**E-71**



CONSTRUCTION

44  
42  
40  
38  
36  
34  
32  
30  
28  
26  
24  
22  
20  
18  
16  
14  
12  
10  
8  
6  
4  
2  
0  
-2

L.A. R/W  
(TURNPIKE)

1.688

1.115 5

15+40.00

SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

Regular		Exc.		Embankment	
A	V	A	V	A	V
4				0	
		4			0

130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90

REVISIONS

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CROSS SECTIONS  
BRIDGE EMBANKMENT PROTECTION  
TEN MILE CREEK

SHEET NO.  
E-72



CONSTRUCTION

40  
38  
36  
34  
32  
30  
28  
26  
24  
22  
20  
18  
16  
14  
12  
10  
8  
6  
4  
2  
0  
-2

L.A. R/W  
TURNPIKE

-1.75

15+60.00

Regular		Exc.		Embankment	
A	V	A	V	A	V

0

0

2

0

SCALE: 1" = 20' HORIZ.  
1" = 10' VERT.

130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90

REVISIONS

DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION



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ROAD NO.	COUNTY	FINANCIAL PROJECT ID
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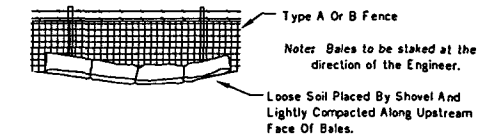
**CROSS SECTIONS**  
**BRIDGE EMBANKMENT PROTECTION**  
**TEN MILE CREEK**

SHEET NO.  
**E-73**

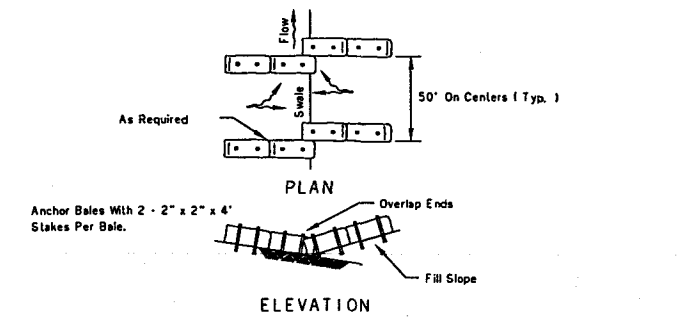


**EROSION AND SEDIMENT CONTROL NOTES**

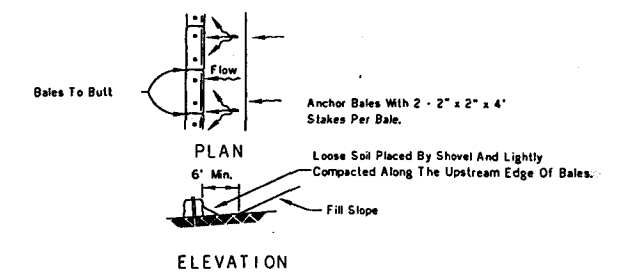
- THE CONTRACTOR IS RESPONSIBLE FOR REMOVING SILT FROM SITE IF NOT REUSABLE ON-SITE AND ASSURING PLAN ALIGNMENT AND GRADE IN ALL DITCHES AND SWALES AT COMPLETION OF CONSTRUCTION.
- THE SITE CONTRACTOR IS RESPONSIBLE FOR REMOVING THE TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES AFTER COMPLETION OF CONSTRUCTION AND ONLY WHEN AREAS HAVE BEEN STABILIZED.
- ADDITIONAL PROTECTION - ON-SITE PROTECTION IN ADDITION TO THE ABOVE MUST BE PROVIDED THAT WILL NOT PERMIT SILT TO LEAVE THE PROJECT CONFINES DUE TO UNSEEN CONDITIONS OR ACCIDENTS.
- CONTRACTOR SHALL INSURE THAT ALL DRAINAGE STRUCTURES, PIPES, ETC. ARE CLEANED OUT AND WORKING PROPERLY AT TIME OF ACCEPTANCE.
- BALES SHALL BE EITHER WIRE-BOUND OR STRING-TIED WITH THE BINDINGS ORIENTED AROUND THE SIDES RATHER THAN OVER AND UNDER THE BALES.
- THE FILTER BARRIER SHALL BE ENTRENCHED AND BACKFILLED. A TRENCH SHALL BE EXCAVATED TO A MINIMUM DEPTH OF 8 INCHES. AFTER THE BALES ARE STAKED, THE EXCAVATED SOIL SHALL BE BACKFILLED AND COMPACTED AGAINST THE FILTER BARRIER.
- EACH BALE SHALL BE SECURELY ANCHORED AND HELD IN PLACE BY AT LEAST TWO STAKES OR REBARS DRIVEN THROUGH THE BALE.
- LOOSE STRAW SHOULD BE WEDGED BETWEEN BALES TO PREVENT WATER FROM ENTERING BETWEEN BALES.
- STRAW BALE BARRIERS SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL.
- CLOSE ATTENTION SHALL BE PAID TO THE REPAIR OF DAMAGED BALES, END RUNS AND UNDERCUTTING BENEATH BALES.
- NECESSARY REPAIRS TO BARRIERS OR REPLACEMENT OF BALES SHALL BE ACCOMPLISHED PROMPTLY.
- ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE STRAW BALE BARRIER IS NO LONGER REQUIRED SHALL BE DRESSED TO CONFORM TO THE EXISTING GRADE, PREPARED AND COVER WITH THE PERMANENT COVERING TYPE SHOWN IN THE PLANS AT THE LOCATION.
- SILT FENCES AND FILTER BARRIERS SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY.
- SHOULD THE FABRIC ON A SILT FENCE OR FILTER BARRIER DECOMPOSE OR BECOME INEFFECTIVE PRIOR TO THE END OF THE EXPECTED USABLE LIFE AND THE BARRIER STILL BE NECESSARY, THE FABRIC SHALL BE REPLACED PROMPTLY.
- SEDIMENT DEPOSITS SHOULD BE REMOVED AFTER EACH STORM EVENT. THEY MUST BE REMOVED WHEN DEPOSITS REACH APPROXIMATELY ONE-THIRD THE HEIGHT OF THE BARRIER.
- ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE SILT FENCE OR FILTER BARRIER IS NO LONGER REQUIRED SHALL BE DRESSED TO CONFORM WITH THE EXISTING GRADE, PREPARED AND COVER WITH THE PERMANENT COVERING TYPE SHOWN IN THE PLANS AT THAT LOCATION.
- THE STRUCTURE SHALL BE INSPECTED AFTER EACH RAIN AND REPAIRS MADE AS NEEDED.
- THE CONTRACTOR IS RESPONSIBLE FOR FOLLOWING THE BEST EROSION AND SEDIMENT CONTROL PRACTICES AS OUTLINED IN THE PLANS, SPECIFICATIONS AND ST. JOHNS RIVER WATER MANAGEMENT DISTRICT SPECIFICATIONS AND CRITERIA.
- FOR ADDITIONAL INFORMATION ON SEDIMENT AND EROSION CONTROL REFER TO "THE FLORIDA DEVELOPMENT MANUAL - A GUIDE TO SOUND LAND AND WATER MANAGEMENT" FROM THE STATE OF FLORIDA DEPARTMENT OF ENVIRONMENTAL REGULATION (F.D.E.R.) CHAPTER 6.
- EROSION AND SEDIMENT CONTROL BARRIERS SHALL BE PLACED ADJACENT TO ALL WETLAND AREAS WHERE THERE IS POTENTIAL FOR DOWNSTREAM WATER QUALITY DEGRADATION. SEE DETAIL SHEET FOR TYPICAL CONSTRUCTION.
- ALL DISTURBED AREAS SHALL BE GRASSED, FERTILIZED, MULCHED AND MAINTAINED UNTIL THE PERMANENT COVERING TYPE SHOWN IN THE PLANS AT THE LOCATION CAN BE COMPLETED.
- SOD SHALL BE PLACED IN AREAS WHICH MAY REQUIRE IMMEDIATE EROSION PROTECTION TO ENSURE WATER QUALITY STANDARDS ARE MAINTAINED.
- ANY DISCHARGE FROM DEWATERING ACTIVITY SHALL BE FILTERED AND CONVEYED TO THE OUTFALL IN A MANNER WHICH PREVENTS EROSION AND TRANSPORTATION OF SUSPENDED SOLIDS TO THE RECEIVING OUTFALL.
- DEWATERING PUMPS SHALL NOT EXCEED THE CAPACITY OF THAT WHICH REQUIRES A CONSUMPTIVE USE PERMIT FROM THE ST. JOHNS RIVER WATER MANAGEMENT DISTRICT.
- ALL DEWATERING, EROSION, AND SEDIMENT CONTROL TO REMAIN IN PLACE AFTER COMPLETION OF CONSTRUCTION AND REMOVED ONLY WHEN AREAS HAVE STABILIZED.
- THIS PLAN INDICATES THE MINIMUM EROSION AND SEDIMENT MEASURES REQUIRED FOR THIS PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR MEETING ALL APPLICABLE RULES, REGULATIONS AND WATER QUALITY GUIDELINES AND MAY NEED TO INSTALL ADDITIONAL CONTROLS.
- THE CONTRACTOR SHALL BE REQUIRED TO RESPOND TO ALL WATER MANAGEMENT DISTRICT INQUIRIES, RELATIVE TO COMPLIANCE OF SJRWMD FOR EROSION AND SEDIMENTATION CONTROL. THE COST OF THIS COMPLIANCE SHALL BE PART OF THE CONTRACT.



**BALES BACKED BY FENCE**

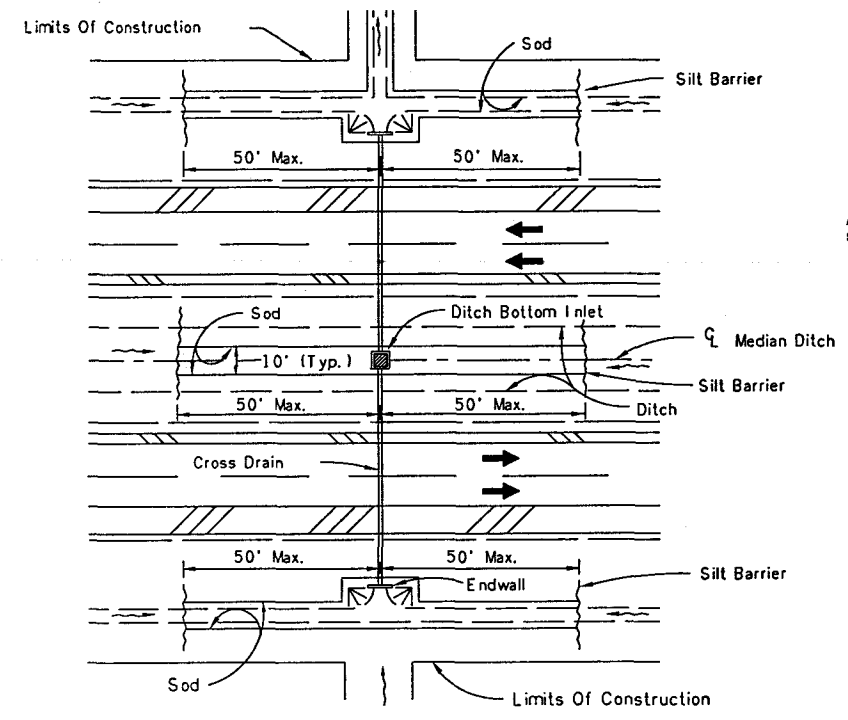


TO BE USED AT SELECTED SITES WHERE THE NATURAL GROUND SLOPES TOWARD THE TOE OF SLOPE

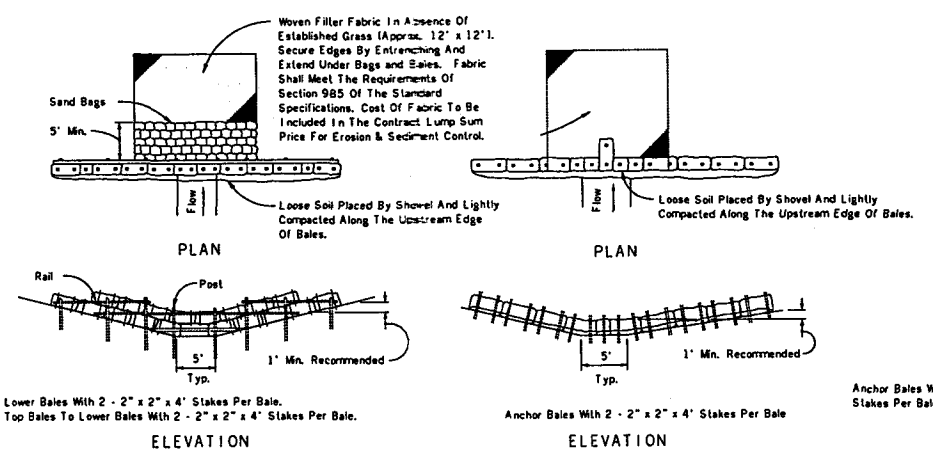


TO BE USED AT SELECTED SITES WHERE THE NATURAL GROUND SLOPES AWAY FROM THE TOE OF SLOPE

**BARRIERS FOR FILL SLOPES**

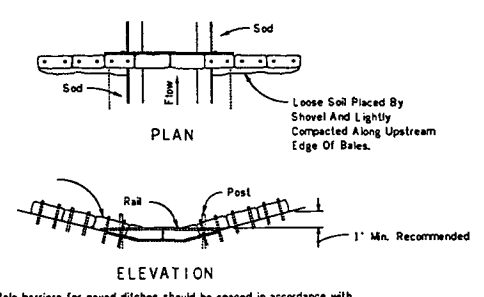


**DITCH INSTALLATIONS AT DRAINAGE STRUCTURES**



Application and Spacing: The use of Types I & II bale barriers should be limited to the conditions outlined in Chd. 1, Sheet 1 of 3, Index No. 102

**BARRIER FOR UNPAVED DITCHES**

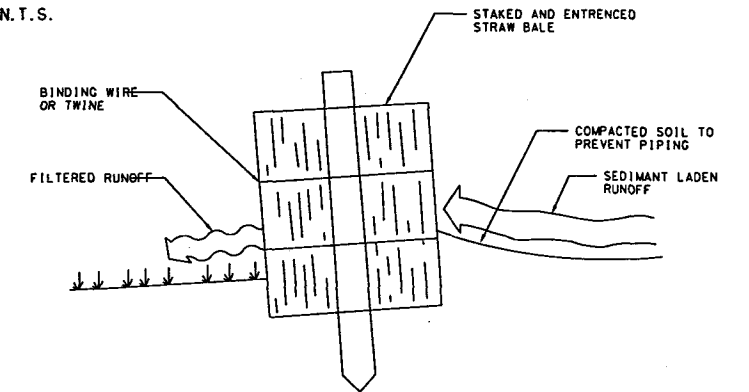


Spacing: Bale barriers for paved ditches should be spaced in accordance with Chd. C Sheet 1 of 3, Index No. 102

**BARRIER FOR PAVED DITCH**

**HAY BALE LOCATION**

(D-901)  
N.T.S.



CROSS-SECTION OF A PROPERLY INSTALLED STRAW BALE

**STAKED HAY BALE**

(D-911)  
N.T.S.

**HAY BALE BARRIERS TYPE I & II**

(D-912)  
N.T.S.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

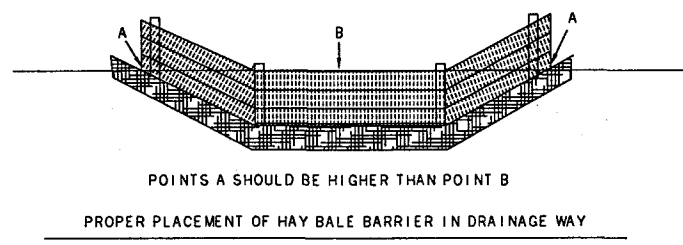
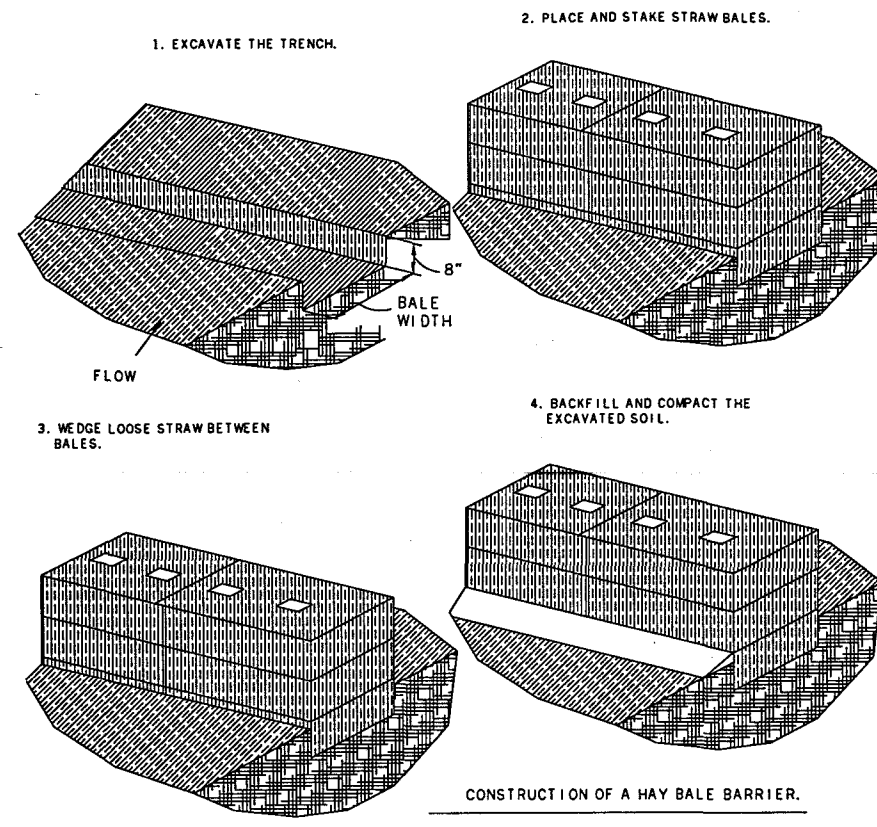
**PITMAN HARTENSTEIN & ASSOC., INC.**  
**ENGINEERS**  
JACKSONVILLE ▼ FORT MYERS ▼ TAMPA

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
91	ST. LUCIE	409327-1-52-01

**EROSION CONTROL**  
STORM WATER POLLUTION PREVENTION PLAN

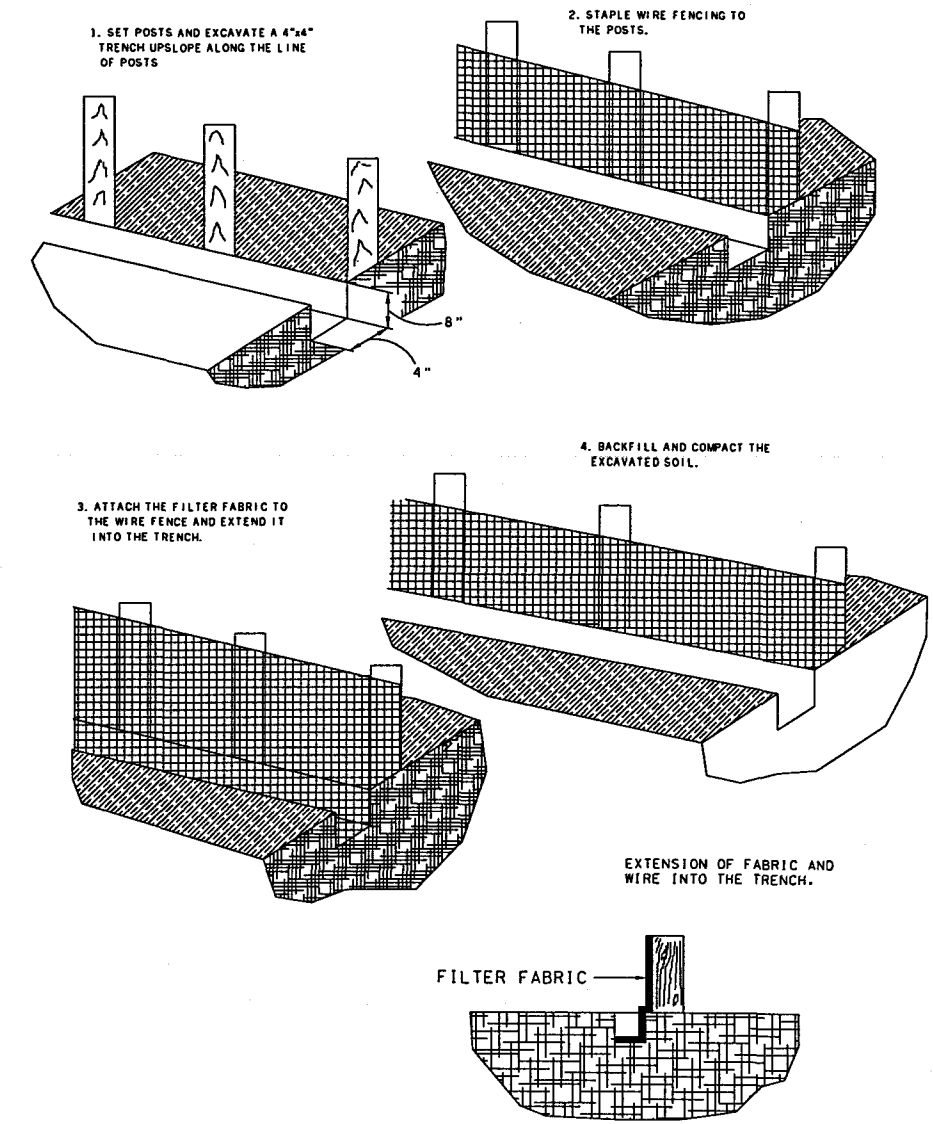
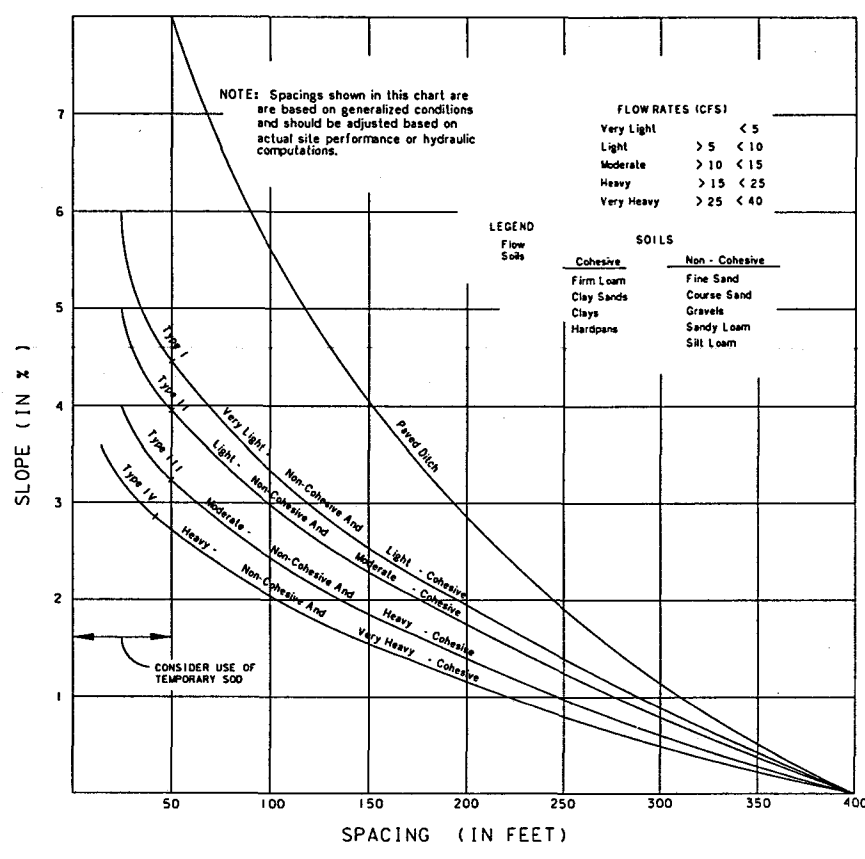
SHEET NO.
E-74





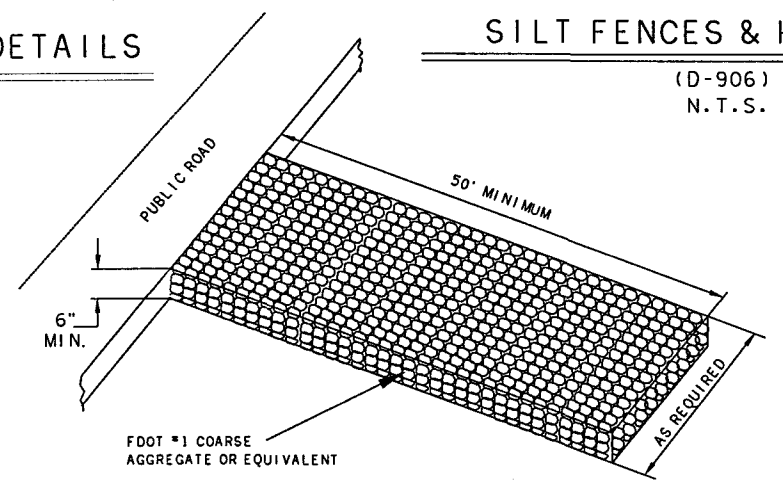
**HAY BALE BARRIER CONSTRUCTION DETAILS**

(D-913)  
N.T.S.



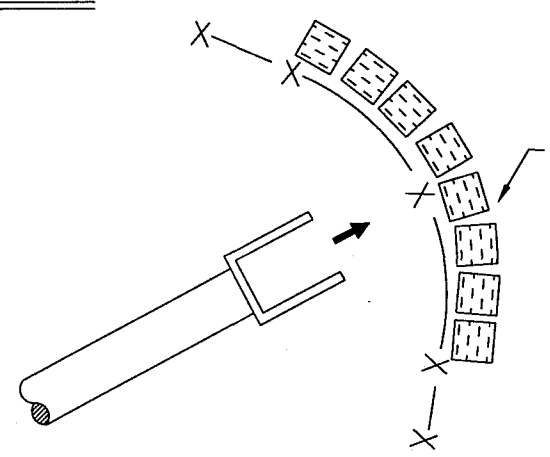
**CONSTRUCTION DETAILS FOR SILT FENCES**

(D-909)  
N.T.S.



**STABILIZED CONSTRUCTION ENTRANCE**

N.T.S.



**OUTLET PROTECTION**

N.T.S.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

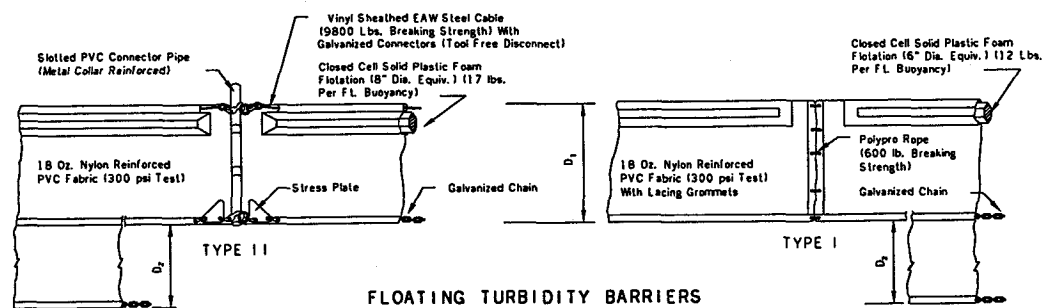
**PITMAN HARTENSTEIN & ASSOC., INC.**  
**ENGINEERS**  
JACKSONVILLE ▼ FORT MYERS ▼ TAMPA

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
91	ST. LUCIE	409327-1-52-01

**EROSION CONTROL**  
STORM WATER POLLUTION PREVENTION PLAN

SHEET NO.  
**E-75**

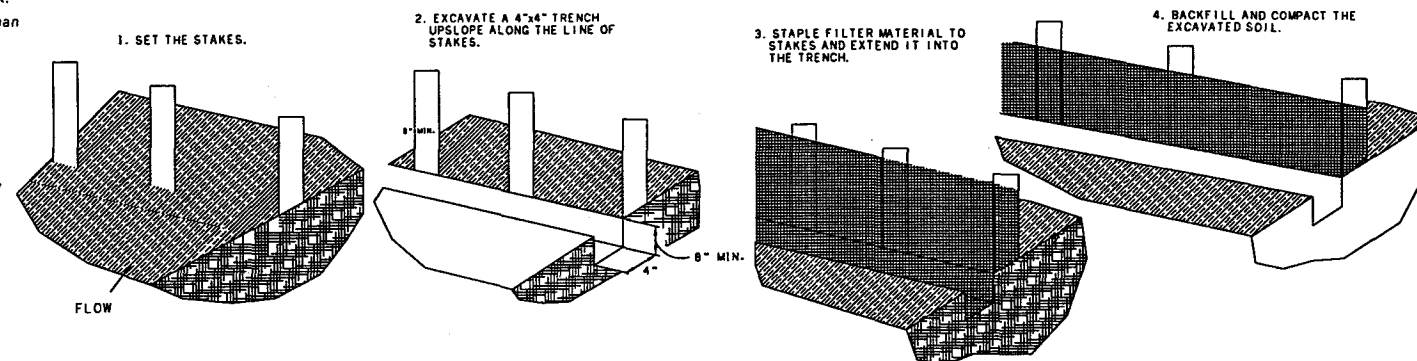




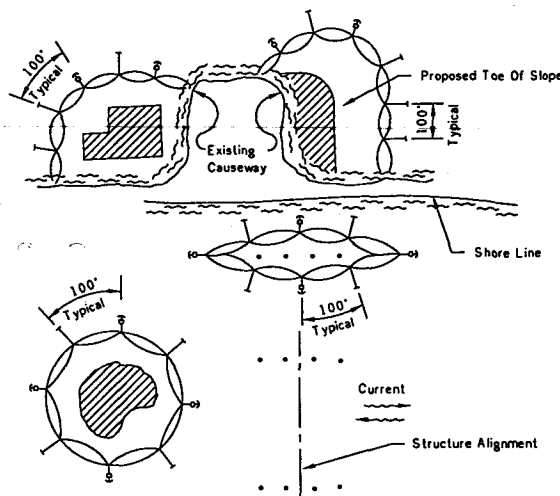
FLOATING TURBIDITY BARRIERS

D<sub>1</sub> = 5' Std. (Single Panel For Depths 5' or Less).  
 D<sub>2</sub> = 5' Std. (Additional Panel For Depths > 5').  
 Curtain To Reach Bottom Up To Depths Of 10 Feet.  
 Two (2) Panels To Be Used For Depths Greater Than  
 10 Feet Unless Special Depth Curtains Specifically  
 Called For in The Plans Or As Determined  
 By The Engineer

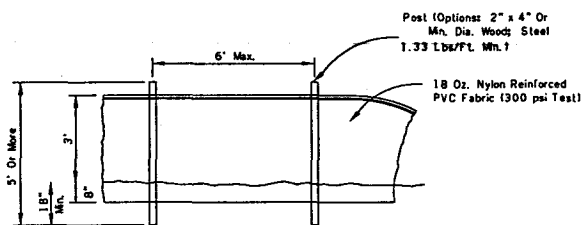
NOTICE:  
 COMPONENTS OF TYPES I & TYPE II MAY  
 BE SIMILAR OR IDENTICAL TO PROPRIETARY  
 DESIGNS. ANY INFRINGEMENT ON THE  
 PROPRIETARY RIGHTS OF THE DESIGNER  
 SHALL BE THE SOLE RESPONSIBILITY OF  
 THE USER. SUBSTITUTIONS FOR TYPES  
 I AND II SHALL BE AS APPROVED BY  
 THE ENGINEER.



CONSTRUCTION OF A FILTER BARRIER

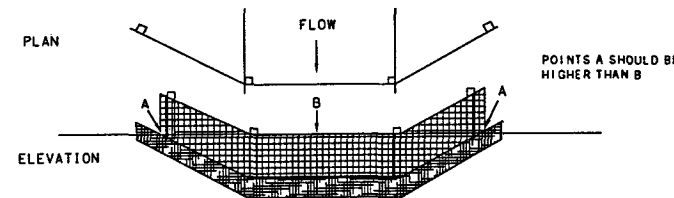
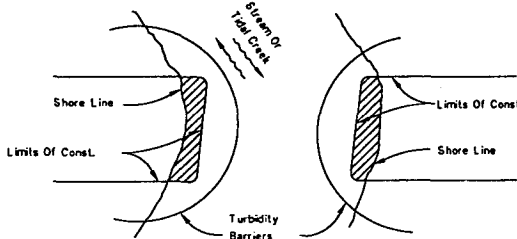


LEGEND  
 • Pile Locations  
 ▨ Dredge Or Fill Area  
 → Mooring Buoy w/Anchor  
 — Anchor  
 ○ Barrier Movement Due To Current Action



STAKED TURBIDITY BARRIER

Notes:  
 Turbidity barriers for flowing streams and tidal creeks may be either floating, or staked types or any combinations of types that will suit site conditions and meet erosion control and water quality requirements. The barrier type(s) will be at the Contractor's option unless otherwise specified in the plans, however payment will be under the contract lump sum price established in the bid proposal for Erosion & Sediment Control Posts in staked turbidity barriers to be installed in vertical position unless otherwise directed by the Engineer.



FILTER BARRIER CONSTRUCTION DETAIL

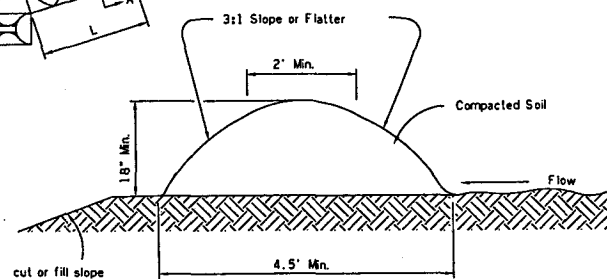
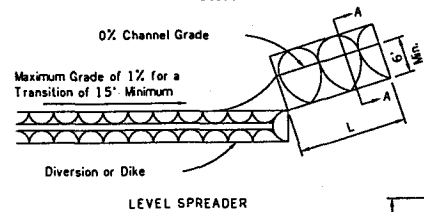
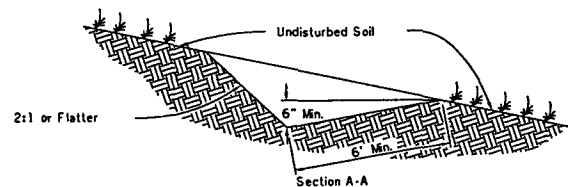
(D-901)  
 N.T.S.

- NOTES:
1. Turbidity barriers are to be used in all permanent bodies of water regardless of water depth.
  2. Number and spacing of anchors dependent on current velocities.
  3. Deployment of barrier around pile locations may vary to accommodate construction operations.
  4. Navigation may require segmenting barrier during construction operations.
  5. For additional information see Section 104 of the FDOT Standard Specifications.

TURBIDITY BARRIER APPLICATIONS

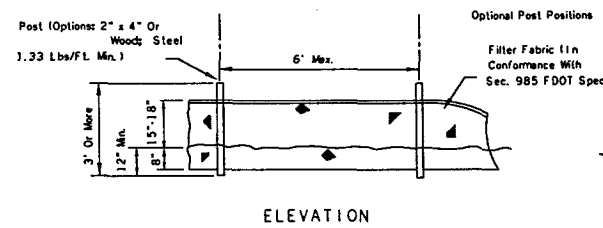
TURBIDITY BARRIERS

(D-907)  
 N.T.S.

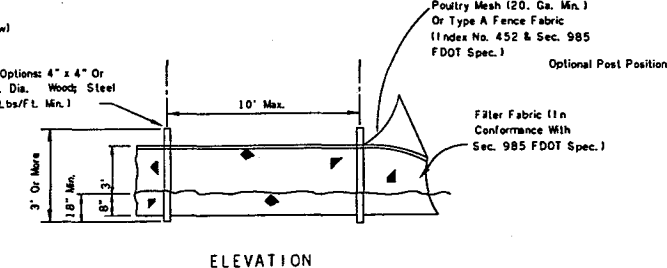
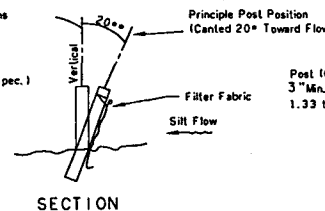


DIVERSION DIKE

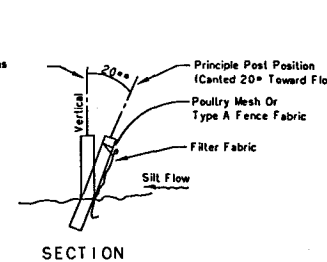
(D-914)  
 N.T.S.



TYPE III SILT FENCE



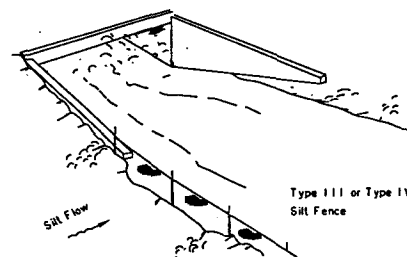
ELEVATION



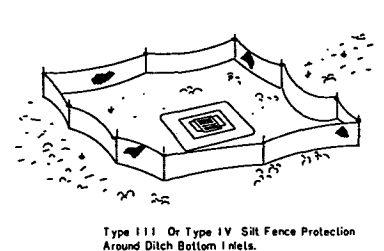
SECTION

Note: Silt Fence to be paid for under the contract lump sum price for Erosion and Sediment Control.

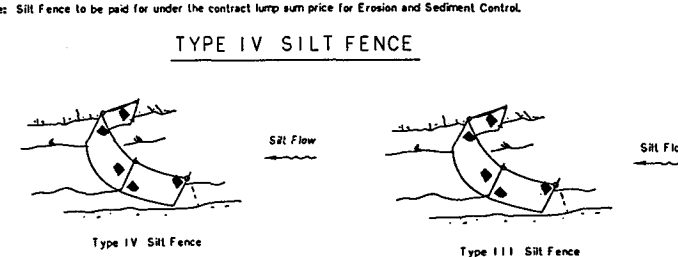
TYPE IV SILT FENCE



Type III or Type IV Silt Fence



Type III or Type IV Silt Fence Protection Around Ditch Bottom Inlets.



Type IV Silt Fence

Type III Silt Fence

Do not deploy in a manner that silt fences will act as a dam across permanent flowing watercourses. Silt fences are to be used at upland locations and turbidity barriers used at permanent bodies of water.

SILT FENCE APPLICATIONS

SILT FENCE TYPE III & IV

(D-908)  
 N.T.S.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

**PITMAN HARTENSTEIN & ASSOC., INC.**  
**ENGINEERS**  
 JACKSONVILLE ▼ FORT MYERS ▼ TAMPA

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
91	ST. LUCIE	409327-1-52-01

**EROSION CONTROL**  
 STORM WATER POLLUTION PREVENTION PLAN

SHEET NO.  
**E-76**



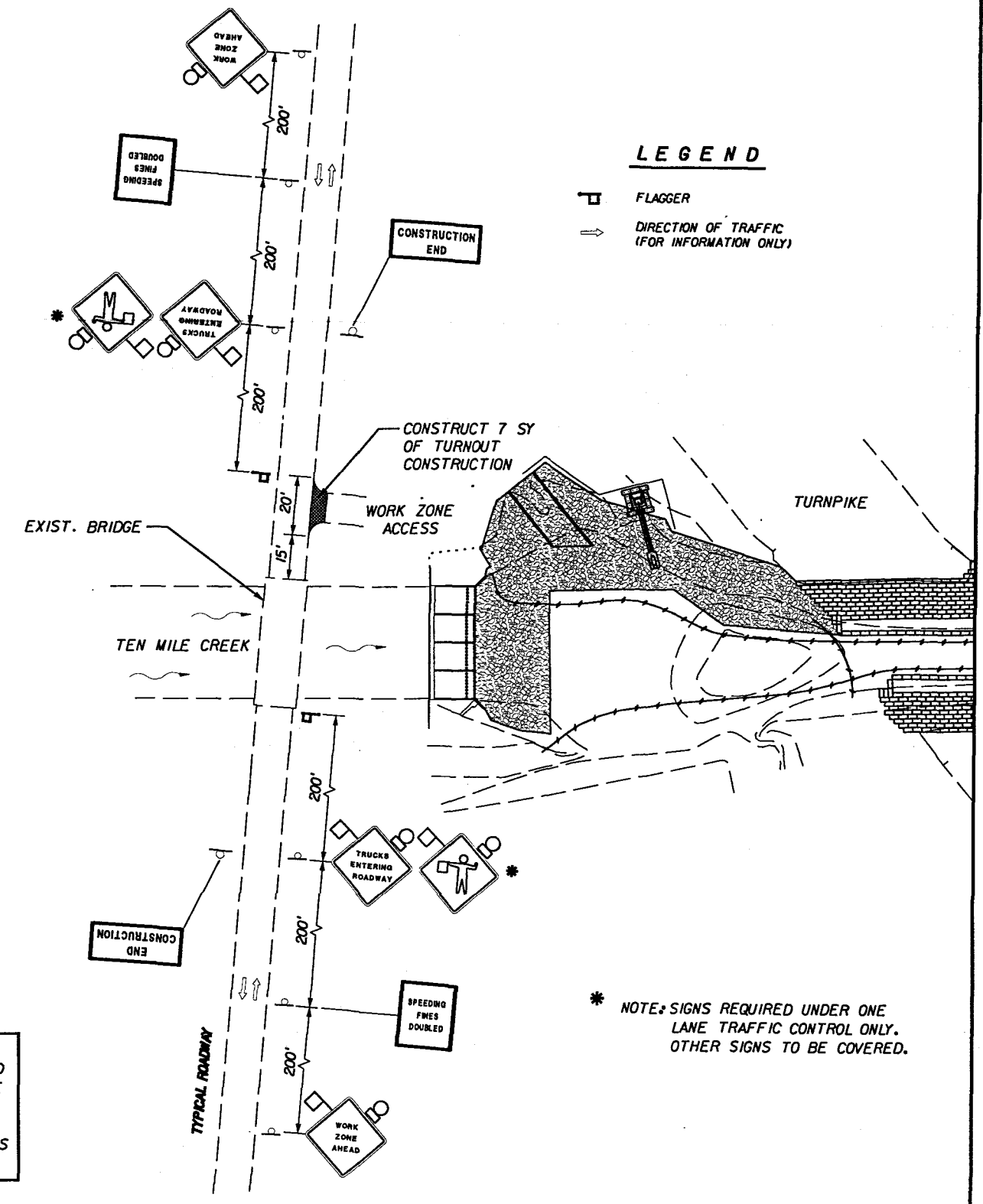
**GENERAL NOTES**

1. TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH F.D.O.T. ROADWAY TRAFFIC STANDARDS, INDEX 600 SERIES.
2. SPEED FOR THIS MOT PLAN IS 35 MPH.
3. ONE LANE CLOSURES SHALL NOT BE ALLOWED BETWEEN THE HOURS OF 5:30 AM TO 9:00 AM AND 3:30 PM TO 7:00 PM.
4. CONTRACTOR SHALL KEEP ALL TRAFFIC BOUND ROADS FREE OF MUD AND DEBRIS TRACKED ON BY EQUIPMENT AND TRUCKS. COST SHALL BE INCLUDED IN THE UNIT PRICE FOR MAINTENANCE OF TRAFFIC L.S. PAY ITEM 102-1. IF CONTRACTOR FAILS TO KEEP ROADS REASONABLY CLEAN THE ENGINEER MAY REQUIRE CONTRACTOR TO INSTALL SOIL TRACKING DEVICES AT NO ADDITIONAL COST TO THE PROJECT.
5. IF ONE LANE CLOSURES ARE ANTICIPATED OR REQUIRED THE COST FOR SUCH OPERATIONS SHALL INCLUDE BUT IS NOT LIMITED TO SIGNS, EQUIPMENT, TWO-WAY RADIOS, FLAGS ETC. AND SHALL BE INCLUDED IN THE UNIT PRICE FOR MAINTENANCE OF TRAFFIC L.S. ITEM 102-1.
6. IF TRAFFIC CONTROL DEVICES ARE USED IN CONJUNCTION WITH FLAGGERS NO LIGHTS ARE REQUIRED. HOWEVER, IF USED UNDER ANY OTHER CONDITIONS, TYPE "C" STEADY BURNING LIGHTS SHALL BE REQUIRED ON EACH DEVICE AS SHOWN IN THE STANDARD INDEX. FLAGGERS MUST BE PRESENT OR THE MOT REMOVED AND TRAFFIC RESTORED
7. ALL SIDE ROADS WITHIN 200 FEET (200') OF THE CONSTRUCTION ZONE SHALL RECEIVE "ROAD WORK AHEAD" & "END CONSTRUCTION" SIGNS AS DIRECTED BY THE ENGINEER.
8. ANTICIPATED QUANTITIES HAVE BEEN INCLUDED IN THE UNIT PRICE BIDS FOR ALL ITEMS SHOWN AND NOT SHOWN ON THE TYPICAL DETAIL PLAN. HOWEVER, THIS SHOULD NOT BE PERCEIVED AS ALL INCLUSIVE AND THE CONTRACTOR SHALL DETERMINE THE FINAL QUANTITIES NECESSARY TO COMPLETE THE MOT PLAN FOR INCLUSION IN HIS FINAL BID FOR ALL ITEMS REQUIRED.
9. NO MAINLINE CLOSURES WILL BE ALLOWED FRIDAY AND SUNDAY AFTERNOONS AND EVENINGS (NOON TO 10:00 PM).

**PHASING PLAN**

1. PRIOR TO ANY CONSTRUCTION, CONTRACTOR SHALL DEVELOP AND HAVE APPROVED A CONSTRUCTION SCHEDULE, THIS SCHEDULE SHOULD OUTLINE HIS CONSTRUCTION OPERATIONS AND PHASING OF WORK AND BE APPROVED BY THE ENGINEER.
2. UPON APPROVAL OF THE CONSTRUCTION SCHEDULE ADVANCE SIGNING SHALL BE PLACED ON ALL ROADWAY CROSSINGS AS SHOWN ON THE TYPICAL (THIS SHEET). SOME ADJUSTMENT MAY BE REQUIRED IN THE SPACING OF SIGNS IN HOUSING DEVELOPMENTS UPON THE APPROVAL OF THE ENGINEER.
3. CONSTRUCTION OF DRAINAGE AND GRADING FOR ACCESS INTO WORK ZONE MAY BEGIN AT ANY TIME. CONTRACTOR SHOULD REFER TO GENERAL NOTES PERTAINING TO MUD ON THE ROADWAYS.
4. FLAGGERS MAY BE REQUIRED AT VARIOUS TIMES DURING THE PROJECT. ONE LANE ROAD AHEAD AND FLAGGER SIGNS MAY BE PLACED OVER THE ADVANCE WARNING SIGNS AS SHOWN ON THE TYPICAL THIS SHEET, HOWEVER SHOULD BE REMOVED WHEN NO LONGER REQUIRED AND THE ORIGINAL SIGNS EXPOSED.
5. SIGNS SHALL REMAIN UNTIL WORK HAS BEEN COMPLETED.

NOTE: WHERE F.D.O.T. SPECIFICATIONS AND INDICES ARE REFERENCED, PLEASE REFER TO THE F.D.O.T. ROADWAY & TRAFFIC DESIGN STANDARDS AND THE F.D.O.T. STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.



**TYPICAL TRAFFIC CONTROL**  
NOT TO SCALE

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION



STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
91	ST. LUCIE	409327-1-52-01

**TRAFFIC CONTROL SHEET**

SHEET NO.  
**E-77**



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**FPID Number:** 423374-1-22-01

**Project Description:** Turnpike Mainline (SR 91) Widening PD&E from Jupiter to Ft. Pierce

**Meeting Name:** North St. Lucie River Water Control District Coordination Meeting

**Date:** July 2, 2020 @ 1:00 PM

**Location:** [Join Microsoft Teams Meeting](#)

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1. Introductions

- a. North St. Lucie River Water Control District
- b. Florida's Turnpike Enterprise (FTE) and GEC
- c. Lochner and PGA

2. Project Overview

- a. Evaluating the potential widening of the Turnpike Mainline (SR 91) from four to eight lanes from Indiantown Rd (SR 706) to Okeechobee Rd (SR 70)
- b. Evaluating potential interchange reconfigurations
- c. Identifying stormwater management and ROW needs to meet FDOT and permitting agency requirements
- d. Conducting Environmental Look Around efforts to identify joint-use and nonconventional stormwater opportunities
  - i. Florida Forever Lands
    - 1. Pepper Farms and Flow-Through Marsh
  - ii. Martin County Septic-to-Sewer Conversions

3. Proposed Design at Ten Mile Creek

- a. Proposed widening of bridge over Ten Mile Creek
  - i. Ten Mile Creek is a FEMA regulatory floodway and will require a FEMA No-Rise Certification
  - ii. Anticipate 6.8 acres of encroachment into the Ten Mile Creek FEMA floodplain

4. History of Erosion and Shoaling

- a. FTE Bridge Embankment Protection (FPID 409327-1) in 2003

5. Additional Discussion/Questions



## Ashley Abdel-Hadi

---

**From:** Ashley Abdel-Hadi  
**Sent:** Thursday, November 5, 2020 5:07 PM  
**To:** Ashley Abdel-Hadi  
**Subject:** FW: Section 408 Authorization



**Ashley Abdel-Hadi**  
**Senior Environmental Scientist/Project Manager**  
Email: [AAbdel-Hadi@kcaeng.com](mailto:AAbdel-Hadi@kcaeng.com)  
Work: 813.871.5331  
201 N. Franklin St. Suite 400, Tampa, FL 33602

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**From:** Swartz, Teri <[tswartz@sfwmd.gov](mailto:tswartz@sfwmd.gov)>  
**Sent:** Friday, July 17, 2020 10:34 AM  
**To:** Robert Whitman Jr. <[RWhitman@kcaeng.com](mailto:RWhitman@kcaeng.com)>  
**Subject:** RE: Section 408 Authorization

Good morning Mr. Whitman,

Yes, I am the District's Section 408 coordinator, so I would handle making approval requests to USACE for projects within our ROW limits. I don't work in ROW, so specific questions about ROW permitting would need to be directed to staff in that section.

For the Turnpike Widening project:

- C-44/St. Lucie Canal is under USACE jurisdiction, so I don't believe we do any ROW permitting there. Section 408 approval should be coordinated directly with USACE.
- C-23 Canal – this crossing requires ROW permitting (modify existing) and Section 408 authorization (to be handled by SFWMD).
- C-24 Canal – this crossing requires ROW permitting (modify existing), but since it is located downstream of the coastal water control structure, we typically do not submit Section 408 approval requests there.

The Ft. Denaud Bridge crosses the C-43/Caloosahatchee River, which is under USACE jurisdiction. I don't believe we do any ROW permitting there, so Section 408 approval should be coordinated directly with USACE.

I'm not familiar with the SFWMD report you're referencing. But hopefully the information above is helpful to you.

Teri

~~~~~  
Teri Swartz, P.E.  
Lead Engineer, Project Management Section  
Engineering and Construction Bureau  
South Florida Water Management District  
(561)682-2505





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**From:** Robert Whitman Jr. <[RWhitman@kcaeng.com](mailto:RWhitman@kcaeng.com)>  
**Sent:** Monday, July 13, 2020 8:02 AM  
**To:** Swartz, Teri <[tswartz@sfwmd.gov](mailto:tswartz@sfwmd.gov)>  
**Subject:** Section 408 Authorization

[Please remember, this is an external email]

Good morning Ms. Swartz, I am working on two projects which may require Section 408 Authorization initiated through the SFWMD. I understand that you are the District's 408 Coordinator and want to discuss these projects with you. I gave you a call and your message indicated that you were best reached by email. We would like to confirm which project crossings would require a SFWMD Right of Way Occupancy Permit and a Section 408 Authorization. These projects include:

**The Project Development and Environment (PD&E) study for widening Florida's Turnpike** from north of SR 706 (Indiantown Road) near Jupiter, to the SR 70 Interchange in Ft. Pierce.

**The Repair and Rehabilitation of the Ft. Denaud Bridge**, which crosses the Okeechobee Waterway approximately 5.2 miles west of La Belle.

**The Project Development and Environment (PD&E) study for widening Florida's Turnpike** from north of SR 706 (Indiantown Road) near Jupiter, to the SR 70 Interchange in Ft. Pierce. This is a 37-mile project and crosses several regional flood protection facilities, including the St. Lucie Canal, the County Line Canal, and the Rim Ditch. The project crosses the Loxahatchee River, but the C-18 Canal, located farther south, is not within our project area. I have included a project location map with the waterway crossings identified (below) to assist with your review. I would like to confirm which of these crossings will require a SFWMD Right of Way Occupancy Permit and Section 408 Authorization. Also, I've noticed some inconsistencies in the canal nomenclature within the project correspondence, and want to get some clarity on this item. Note that the nomenclature in the graphic below was taken from a SFWMD Technical Report (reference below), but is not consistent with some of the other project correspondence and documents. We are revising the documents for consistency and wanted some clarification.

**St. Lucie Canal** – The Turnpike crosses the St. Lucie Canal downstream of control structure S-80; the reference document identifies this as the C-44A Canal; however other correspondence identifies this as the C-44 Canal (the reference document identifies the C-44 Canal as upstream of S-80).

**County Line Canal – C-23 Canal** (consistent nomenclature).

**Rim Ditch** – The Turnpike crosses the Rim Ditch downstream of control structure S-49; the reference document identifies this as the C-23A Canal; however other correspondence commonly identifies this canal as the C-24 Canal (the reference document identifies the C-24 Canal as upstream of S-49).

SFWMD Document Reference:

South Florida Water Management District, 2010. *Canals In South Florida: A Technical Support Document*. West Palm Beach, Florida.

Appendix C – Description of SFWMD Primary Water Management Features





**The Repair and Rehabilitation of the Ft. Denaud Bridge**, which crosses the Okeechobee Waterway approximately 5.2 miles west of La Belle. The project will involve repair of bridge structural elements and replacing the fender system, but may also include stabilization of the banks of the waterway. The extend of the shoreline stabilization has not yet been designed, but is anticipated to stay within the roadway right of way, and could extend below the Ordinary High Water



Line. Please let me know if this project would require a SWFWMD Right of Way Occupancy Permit and a Section 408 Authorization.

I have permitted several projects requiring Section 408 authorization within the SWFWMD and worked closely with Jeff Hagberg, SWFWMD's 408 Coordinator, and recognize that early coordination is important to this process. Any information that you could provide would be valuable. I'm available to discuss at your convenience, and feel free to call if that would be easier for you. Thank you in advance for your assistance. Best regards, Bob



**Robert Whitman Jr.**  
**Chief Environmental Scientist/Sr. Project Manager**  
Email: [RWhitman@kcaeng.com](mailto:RWhitman@kcaeng.com)  
Work: 813.871.5331 ext 4167  
Cell: 813.391.4773  
201 N. Franklin St., Suite 400, Tampa, FL 33602

CONFIDENTIALITY NOTE: This communication may be privileged and confidential. It should not be disseminated to others. If received in error, please immediately reply that you have received this communication in error and then delete it. Thank you.



# National Park Service Meeting #01

Turnpike Mainline (SR 91)  
Widening PD&E Study from  
Jupiter to Fort Pierce

**PROJECT: Florida's Turnpike (SR 91) Widening PD&E Study from Jupiter (Indiantown Road) to Okeechobee Road (SR 70) (FPID#: 423374-1-22-01)**

Palm Beach, Martin and St. Lucie Counties

**MEETING DATE:** February 15, 2018

**MEETING TIME:** 10:30 AM to 11:30 AM

**LOCATION:** Teleconference

**GO TO MEETING LINK** <https://global.gotomeeting.com/join/716488589>

**CONFERENCE CALL NUMBER:** (646) 749-3112

**ACCESS CODE:** 716-488-589

## 1. Introductions

## 2. Project Description

- a. Overall Project
- b. Loxahatchee River Crossing
  - a. Proposed Action
  - b. Constraints
    - i. FGT Gas Pipelines
    - ii. Interstate 95
    - iii. Public Lands
    - iv. Wetlands / Habitat

## 3. Section 7(a) Requirements

## 4. Open Discussion

## 5. Future Coordination



**6. Action Items**

| <b>ID#</b> | <b>Description</b> | <b>Responsible Person</b> | <b>Due Date</b> | <b>Completion Date</b> | <b>Status</b> |
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# National Park Service (NPS) Coordination Meeting #

Turnpike Mainline (SR 91)  
Widening PD&E Study from  
Jupiter to Fort Pierce

PROJECT: **Florida's Turnpike (SR 91) Widening PD&E Study from Jupiter (Indiantown Rd/SR 706) to Ft. Pierce (Okeechobee Rd/SR 70) (FPID#: 423374-1-22-01)**

Palm Beach, Martin and St. Lucie Counties

MEETING DATE: September 25, 2020

FACILITATOR: Brian Ribaric *BR*

## 1. Attendees

Jeff Duncan (NPS)  
Jennifer Back (NPS)  
Cesar Martinez, PE (FDOT, D-4)  
Lynn Kelly (FDOT, D-4)  
Henry Pinzon, PE (FTE)  
Rax Jung, PhD, PE (FTE)  
Philip Stein (FTE)

Annemarie Hammond (FTE)  
Brian Ribaric, PE (Atkins/FTE)  
Doug Zang (Atkins/FTE)  
Fred Gaines, PWS (Atkins/FTE)  
Bill Howell, PE (Lochner)  
Mark Easley (H&H)

## 2. Background

- a. After introductions, Brian Ribaric narrated a PowerPoint presentation that provided an overview of the project to the attendees and stated that the PD&E Study limits are along Florida's Turnpike Mainline (SR 91) from Indiantown Road (SR 706) to Okeechobee Road (SR 70), MP 117 to MP 153.7. A copy of the agenda and PowerPoint presentation are attached to these meeting notes.
- b. Brian Ribaric stated that the project includes the widening of the mainline from two to four lanes in each direction and the replacement/widening of the existing bridge over the Loxahatchee River.
- c. Brian Ribaric stated that the bridge over the Loxahatchee is within a Scenic segment of the river and that the existing vegetation includes the river channel, cypress dominated river swamp, drainage ditches adjacent to the existing roadway, and flatwoods landward of existing wetlands. He also identified some of the federal and state listed species that are present.
- d. Brian Ribaric then discussed the existing Turnpike bridge over the Loxahatchee River and stressed that it had four spans approximately 45 feet in length, which resulted in piles in the middle of the river channel.
- e. Brian Ribaric identified design constraints including: limited right-of-way, I-95 to the east, centuries old cypress trees adjacent to the right-of-way, and three Florida Gas Transmission pipelines in the right-of-way. He also identified the need to meet the Wild & Scenic River Act requirements.
- f. Brian Ribaric then discussed the proposed bridge structure, that it would have four 12-foot lanes in each direction and that it would be a three-span bridge with the center span being 110 feet. This would allow for the removal of bridge piles from the river channel.



- g. With the proposed span arrangement for the new bridge, the piles are not anticipated to be in the water during normal stages but may be in the water at flood stage.
- h. Brian Ribaric also stated that new piles would line up with the piles of the existing I-95 structure and locate them farther away from the open water channel banks.
- i. Jeff Duncan asked if the old bridge piles would be removed. FTE discussed that piles may not be able to be removed due to engineering/site condition concerns. Sometimes removal of piles causes significant site disturbance. If piles are not removed completely, they will be cut off 2-feet below the mudline.
- j. Construction of the new bridge would close the gap between the Turnpike bridge and the I-95 bridges, and would widen to the west beginning immediately adjacent to the west side of the I-95 bridges, entirely within existing Turnpike right-of-way.

**3. Loxahatchee River Wild & Scenic River Designation**

- a. Brian Ribaric then discussed the Loxahatchee River Wild and Scenic River designation and stated that the project was within the upper scenic segment of the river at approximately river mile 12.9.
- b. Brian Ribaric stated that based on the river’s 2010 Management Plan, within the existing right-of-way, the wild and scenic designations extend approximately 350 feet to the north and approximately 465 feet to the south of the river channel centerline. He also stated that based on these distances, there were approximately 1.11 acres of floodplain wetlands and 0.46 acres of upland buffer within the right-of-way.
- c. Brian Ribaric then went through the Outstandingly Remarkable Values (ORV) used in the designation of the river as Wild and Scenic, based on the National Park Service’s (NPS) 1984 Environmental Impact Statement. These criteria included: ecological, fish and wildlife and recreational. He also identified the ORVs identified in the Wild and Scenic River Act.

**4. Potential Project Impacts**

- a. Brian Ribaric then went through each of the six ORVs identified in the Wild and Scenic River Act and discussed potential project related impacts and mitigating actions that the Turnpike will implement to minimize/minimize these impacts. These included:
  - i. Scenic Values –
    - 1. Potential impacts included visual and auditory.
    - 2. To offset these impacts, the Turnpike will use a longer bridge main-span to eliminate piles in the river channel and reduce noise (fewer bridge joints); maintain the existing vertical clearance; and limit lighting as much as possible in the area of the bridge.
    - 3. Jeff Duncan asked if the Turnpike has considered additional actions, such as painting/staining the bridge to better blend into the surrounding environment and soften visual features. Also, Jeff Duncan asked if the Turnpike would consider additional aesthetic design features such as railings, piles/piers/parapet designs such as those done for the Wekiva River crossing. While the Loxahatchee River may not provide the same aesthetic opportunities as the Wekiva River, Brian Ribaric stated that the additional aesthetic features mentioned are things that could be considered during the design phase.
  - ii. Recreational Values
    - 1. Potential impacts include visual and auditory.
    - 2. To offset these impacts, the Turnpike will maintain/enhance the paddling network in the area; use a longer bridge main-span to eliminate piles in the river channel (reduce obstacles in channel); and maintain the bridge vertical clearance.



3. NPS asked about paddle direction. The Turnpike responded west to east. NPS indicated that since the Turnpike bridge is first (upstream) seen, then aesthetics are very important. Lynn Kelly indicated that the river channel access is from River Bend Park (upstream) to Jonathan Dickinson State Park (downstream).
- iii. Historical/Cultural Values
  1. There should be no impacts to Historical/Cultural Values as a result of the project.
- iv. Free Flow Nature
  1. The project should enhance the free flow nature of the river due to the longer bridge main-span and removal of piles from the river channel.
- v. Water Quality/Quantity Values
  1. Potential impacts include increased pollutant loads due to increased impervious surface. No impact to quantity is anticipated. In order to offset these impacts, Jeff Duncan wanted to verify that the new bridge would not direct discharge into the river. The Turnpike will not directly discharge stormwater off of the bridge into the river (i.e., no scuppers will be placed on the new bridge). The stormwater from the new bridge would be carried off the bridge in the bridge shoulders and conveyed to stormwater treatment facilities prior to its discharge into the river.
  2. Jennifer Back asked if the Turnpike has modeled the river's flood flows. Brian Ribaric stated that this is something that is done during the project's design phase. Mark Easley added that there are multiple restoration plans, such as the Loxahatchee River Watershed Restoration Project, that were being implemented by state and federal agencies with the purpose of restoring the river's historic flows. Fred Gaines also pointed out that the Loxahatchee River was an Outstanding Florida Water (OFW), and as a result, additional stormwater treatment would be required over and above normal regulatory requirements.
- vi. Wildlife/Habitat Values
  1. Potential impacts include loss of habitat due to the conversion of wetlands/uplands to roadway.
  2. To offset these impacts, the Turnpike will keep construction activities within its existing right-of-way and enhance wildlife movement under the bridge by placing a wildlife path through the bridge rip-rap. The Turnpike is also open to assessing the addition of wildlife fencing to directing wildlife to the path. In addition, a site-specific clearing plan will be developed to minimize impacts to existing vegetation (i.e., removal and trimming).

## 5. Next Steps

- a. Brian Ribaric then discussed the next steps the Turnpike would use to further avoid and minimize impacts to the Wild & Scenic River segment. This included further analysis of the project and development of avoidance/minimization options, and further coordination with the Florida Department of Environmental Protection, South Florida Water Management District, and the Loxahatchee River Management Coordinating Council.
- b. Brian Ribaric then asked Jeff Duncan what the NPS would like to see from the Turnpike as the project moves forward.
- c. Jeff Duncan asked Brian Ribaric when the segment of the project containing the Loxahatchee River would be going to US Army Corps of Engineers (USACE) permitting. Brian Ribaric responded that the project is not in the current five-year work plan and that it would likely be closer to ten years before the project goes to permitting. Jeff Duncan stated that the Section 7(a) document (Analysis and Determination) would be in response to the USACE permit



application. At this point in the PD&E Study, the project meeting notes and a copy of the presentation for the file would be adequate.

- d. Jeff Duncan also stated that NPS appreciated the details on the proposed mitigation measures – removal of piles from the channel, wildlife path, and limited lighting. There would need to be more discussions about bridge aesthetics during the project design phase.
- e. NPS indicated that no actionable phase yet, but presentation and meeting minutes will start historical file of record for the project. FTE discussed that coordinating during the PD&E Study to allow for a complete environmental document and for a smooth transition for required aspects into design.
- f. Mark Easley stated that the Turnpike’s primary concern is avoiding fatal flaws that may exist with the proposed bridge replacement. Jeff Duncan stated that he did not see any fatal flaws with the project as proposed, but there would be details that will need to be worked out during the permitting of the project. He also stated that the NPS mandate is to protect and enhance the river and that there can be no direct impacts as there is no way to mitigate direct impacts. Since there is a span there already, the Turnpike will only need to show improvements.

**6. Project Schedule**

- a. Brian then reviewed the project’s Project Development & Environment (PD&E) schedule. At present, the Turnpike anticipates the project’s PD&E phase to be complete by summer 2021.

Brian Ribaric provided Jeff Duncan with his contact information in case he had any additional comments or wished to discuss the project and proposed bridge crossing.

The meeting ended at approximately 1:50 pm.

**END MEETING NOTES**



## National Park Service Coordination Meeting

Project Development and Environment (PD&E) Study  
to Widen Florida's Turnpike (SR 91) from Jupiter (Indiantown Road)  
to Okeechobee Road (SR 70)  
Palm Beach, Martin, and St. Lucie Counties, FL

Financial Project ID #: 423374-1-22-01  
ETDM #: 14295  
September 25, 2020

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## PD&E Study Improvements

### • Purpose

- Add capacity & interchange access to the Florida's Turnpike (SR 91)

### • Proposed Improvements

- Widen SR 91 by adding lanes in each direction
- Widen/reconstruct bridges over Loxahatchee River & St. Lucie Canal
- Reconfigure 4 existing interchanges & construct 2 new interchanges



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## Existing Conditions at Loxahatchee River

### • Florida's Turnpike

- Bridge in Scenic Segment of River
- Existing vegetation
  - River Channel
  - Cypress dominated river swamp
    - Additional species – maple, water hickory, water oak, red bay, cabbage palm
  - Drainage ditches
  - Slash Pine dominated pine flatwoods
    - Additional species – saw palmetto, wax myrtle, Lyonia
- Potential protected species
  - American alligator, snail kite, wood stork, indigo snake, gopher tortoise, SE American kestrel, wading birds

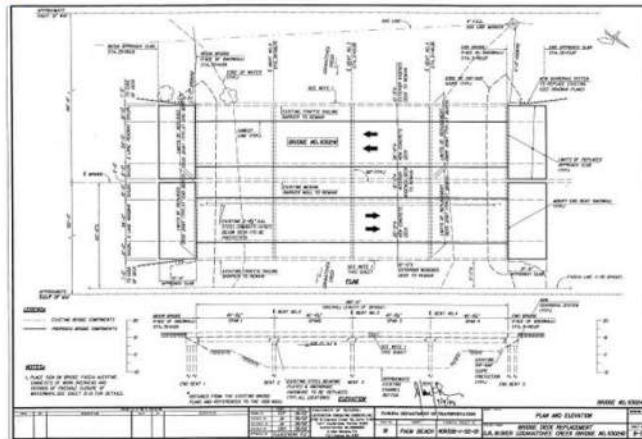


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## Existing Conditions at Loxahatchee River

### • Existing Bridge Structure

- Bridge constructed in 1956 (deck reconstructed 1991)
- Bridge type is AASHTO Type II
  - Travel lanes – two 12-foot in each direction
  - Outside shoulders – 10 feet
  - Inside shoulders – 10 feet northbound, 8 feet southbound
  - Length - 180.4 feet
  - Width - 91 feet
  - Number of spans - four
  - Span length – 45.1 feet
  - Low member elevation – approx. 15.0 NAVD
  - Vertical clearance under bridge – approx. 9 feet



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## Existing Conditions at Loxahatchee River

### • Design Constraints

- Limited right-of-way
- Florida Gas Transmission Pipeline (3 Lines)
- I-95 (current and ultimate)
- Wild & Scenic River Requirements
- Centuries Old Cypress Tree

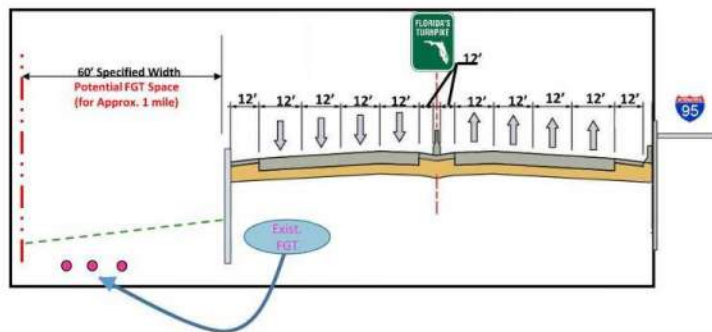


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## Proposed Loxahatchee River Bridge

### • Proposed Typical Section

- Widening to west
  - I-95 on east
  - FGT pipeline on west
- Staying within existing right-of-way
- FGT 75' standard specified width reduced to 60'



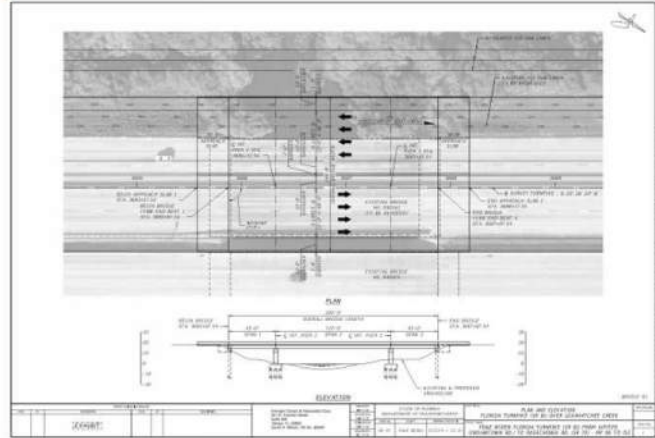
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# Proposed Loxahatchee River Bridge

## • Proposed Bridge Structure

- Bridge type FIB 45
  - Travel lanes – four 12-foot lanes in each direction
  - Outside shoulders – 12 feet
  - Inside shoulders – 12 feet
  - Length - 200 feet
  - Width - 149 feet
  - Number of spans - three
  - Span length – 45 feet / 110 feet / 45 feet (Similar to I-95 bridge spans)
  - Low member elevation – match existing approx. 15' (NAVD)
  - Vertical clearance under bridge – approx. 9 feet



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# Loxahatchee River Wild & Scenic River Designation

## • Wild & Scenic River Segment

- 2010 Management Plan
  - River mile 5.2 to river mile 15.5
  - 10.3 miles total
  - Differs from 1984 EIS which lists 7.5 miles
- Segment Designations (approximately)
  - Scenic - 5.2 to 10.5
  - Wild – 10.5 to 12.5
  - Scenic – 12.5 to 14.5
  - Recreational – 14.5 to 15.5
- Turnpike in Scenic segment of river (River Mile 12.9)

The Loxahatchee River Wild and Scenic Designation and Preservation Act delineates the boundaries of the designated portion of the Northwest Fork:

...described as that portion of the Northwest Fork downstream of the southern boundary of Riverbend Park [RM 15.5] located in Palm Beach County and upstream of an east-west line passing through a point where the southern boundary of Jonathan Dickinson State Park intersects the eastern shoreline of the river [RM 5.2].



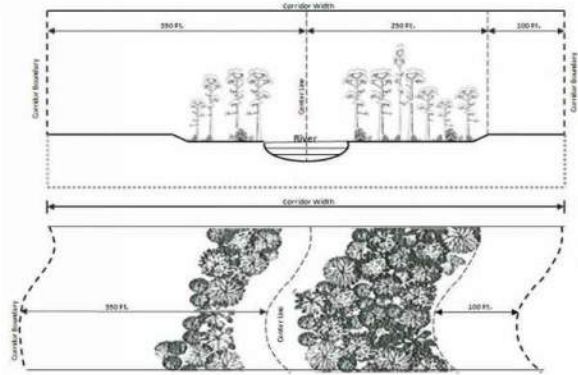
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# Loxahatchee River Wild & Scenic River Designation

## • Boundaries Within Project Area

- Limits from river centerline (2010 Management Plan)
  - The greater of:
    - Upland extent of floodplain wetlands + 100-foot buffer
    - 350 feet measured from center of main river channel
- Turnpike right-of-way (approximate)
  - North side
    - Floodplain wetlands 215' + upland buffer 135' = 350'
  - South side
    - Floodplain wetlands 365' + upland buffer 100' = 465'
- Area within right-of-way (approximate)
  - North side
    - Floodplain wetlands 0.44 acres; flatwoods 0.28 acres
  - South side
    - Floodplain wetlands 0.67 acres; flatwoods 0.18 acres



# Loxahatchee River Wild & Scenic River Designation

## • Outstandingly Remarkable Values (ORVs) (based on 1984 NPS EIS for Listing)

- Ecological
  - Best remaining example of SE Florida river swamp
  - Diverse plant community – tropical & temperate
  - Subtropical river-swamp system – unique to W&S river system
  - 300 to 500-year old cypress
- Fish and Wildlife
  - 267 species – 169 genera, 78 families
  - Temperate, tropical & pelagic gulf species\*
  - Federal species – eagle, snail kite, indigo snake, manatee, red-cockaded woodpecker, alligator
- Recreational
  - Narrow meandering channel – challenging & interesting recreational experience
  - Diverse vegetation & habitats

\* The 1984 EIS identified "pelagic gulf species" should have identified "pelagic south Atlantic species"





## Loxahatchee River Wild & Scenic River Designation

### • Outstandingly Remarkable Values (ORVs) (based on Criteria)

- Scenic
- Recreational
- Historical / Cultural
- Free Flow Nature
- Water Quality / Quantity
- Wildlife / Habitat



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## Potential Project Impacts

### • Scenic Values

- Potential Impacts
  - Visual
  - Auditory
- Potential Mitigating Actions
  - Use of longer bridge main span
  - Removal of piles from river channel
  - Maintain vertical clearance
  - Limit lighting as feasible



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## Potential Project Impacts

### • Recreational Values

- Potential Impacts
  - Visual
  - Auditory
- Potential Mitigating Actions
  - Maintain/enhance paddle network in area
  - Use of longer bridge main span
  - Removal of piles from river channel
  - Maintain vertical clearance



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## Potential Project Impacts

### • Historical / Cultural Values

- Potential Impacts
  - Impacts not anticipated
- Potential Mitigating Actions
  - None proposed



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## Potential Project Impacts

### • Free Flow Nature

- Potential Impacts
  - Enhanced
- Potential Mitigating Actions
  - Use of longer bridge main span
  - Removal of piles from river channel
  - Maintain vertical clearance



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## Potential Project Impacts

### • Water Quality / Quantity Values

- Potential Impacts
  - Quality - Increased pollutant loading
  - Quantity – None anticipated
- Potential Mitigating Actions
  - No direct discharge of stormwater into river
  - Treatment of new impervious surfaces to OFW standards
  - Removal of piles from river channel (enhance flow)



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## Potential Project Impacts

### • Wildlife / Habitat Values

- Potential Impacts
  - Loss of habitat
- Potential Mitigating Actions
  - Staying within existing right-of-way
  - Enhance wildlife movement under bridge / placement of wildlife path only through rip-rap
  - Wildlife fencing as appropriate
  - Site-specific clearing plan to minimize impacts
  - Minimize lighting as feasible



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## Next Steps

### • Next Steps

- Analyze project information
- Identify project impacts
- Continued coordination
  - NPS
  - FDEP/SFWMD
  - LRMCC
- Develop avoidance/minimization options
- Identify mitigative actions
- Document findings (Analysis and Determination)



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### Contact Information

**Brian Ribaric, P.E.**  
Project Manager for Florida's Turnpike Enterprise (Atkins)  
(407) 264-3095  
[Brian.Ribaric@dot.state.fl.us](mailto:Brian.Ribaric@dot.state.fl.us)

**FDOT** Florida Department of Transportation 20 

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Questions?



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## **FDOT, Florida's Turnpike Enterprise/Florida Fish and Wildlife Conservation Commission Technical Assistance Meeting Notes**

### **FPID 423374-1-22-01 (Turnpike) Turnpike Mainline Widening from Jupiter to Fort Pierce**

#### **Palm Beach, Martin, and St. Lucie Counties**

Date: Friday, October 16, 2020  
Time: 11:00 am – 12:00 pm  
Venue: Microsoft TEAMS meeting  
Facilitator: Brian Ribaric

*BR*

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*Note: The italicized text below in the meeting agenda are the notes for topics that were discussed during the meeting.*

#### **1. Introductions**

- **FWC Staff – Brian Barnett**
- **FTE Environmental Administrator – Philip Stein**
- **FTE Permits Coordinator - Annemarie Hammond**
- **FTE Project Manager – Brian Ribaric, PE (Atkins)**
- **FTE Permits Coordinator – Fred Gaines, PWS (Atkins)**
- **Lochner Project Manager – William Howell, PE**
- **KCA Project Manager/Chief Environmental Scientist – Robert Whitman**
- **KCA Senior Environmental Scientist – Ashley Abdel-Hadi**

#### **2. Project Overview (map provided)**

- Current Alignment
  - Approx. 37-mile segment of proposed widening of existing 4-lane facility to 8-lanes primarily within existing right of way with 4 existing and 2 proposed interchanges
- ETDM No. 14295 published on May 19, 2017
  - FWC concerns included potential impacts to the following state listed species:
    - Florida sandhill crane (*Antigone canadensis pratensis*)
    - Florida burrowing owl (*Athene cunicularia floridana*)
    - Wading birds
      - Little blue heron (*Egretta caerulea*)
      - Tricolored heron (*Egretta tricolor*)
      - Roseate spoonbill (*Platalea ajaja*)
    - Southeastern American kestrel (*Falco sparverius paulus*)
    - Gopher tortoise (*Gopherus polyphemus*)
    - Florida pine snake (*Pituophis melanoleucus mugitus*)
    - Southern fox squirrel (*Sciurus niger niger*) – FWC species guidelines relative to fox squirrel nests
    - Least tern (*Sternula antillarum*)
    - State protected plants – coordination as required with FDACS
- 428.21 acres of wetland impacts anticipated with the preferred alternative (preliminary impacts - 62.67 acres of wetlands and 365.54 acres of surface waters)



*Florida's Turnpike Enterprise (FTE) provided an overview of the project and interchanges. A background and current status of the project was provided, which included the project location and primary land uses within the proposed project area. FTE noted that the project proposes widening from the existing 4 lane typical section to an ultimate 8 lane section, and includes improvements to existing interchanges, as well as evaluation of two new interchanges. FTE indicated that the widening is proposed principally to the west, with a few exceptions (e.g. Loxahatchee River crossing), due to the location of the FGT gas lines. FTE indicated that the project construction was currently unfunded, and discussed potential interim 6-lane segment needs, in comparison with the ultimate 8- lane facility. FTE noted that FWC reviewed and provided comment on the project's ETDM Programming Screen, published in 2017.*

*FTE proceeded with a review of the project aerial photographic exhibits and discussed the riverine/canal systems that the project will be crossing, including Loxahatchee River (OFW, Wild and Scenic River), Cypress Creek, tributaries to the South Fork of the St. Lucie River, Roebuck Creek, St. Lucie Canal (C-44), Mapps Creek, Danforth Creek, Bessy Creek, County Line Canal (C-23) Canal, Winters Creek, Blakeslee Creek, Rim Ditch (C-24), Ten Mile Creek, and other unnamed tributaries. FTE indicated that preliminary wetland impact acreage includes areas within the existing ROW and proposed ROW of the preferred interchange alternatives. FTE then initiated a discussion of state-protected species.*

### **3. Florida Sandhill Crane**

*FTE noted a Florida sandhill crane nest was documented during field reviews in the SW Martin Highway Interchange within 400 feet of the project area. FTE stated that surveys for the species would be conducted during design, disturbance would be avoided to all extent practicable, active nests would be avoided during construction (400-foot buffer), and an Incidental Take Permit would be submitted to FWC with suitable mitigation provided for unavoidable impacts.*

*FTE indicated that they would avoid the need to obtain an Incidental Take Permit for Florida sandhill cranes by avoiding active nests and proposed an effect determination of "no adverse effect anticipated" and FWC agreed. FWC noted that nest locations will likely change prior to construction and FTE stated that that surveys would be conducted during design and prior to construction.*

### **4. Florida Burrowing Owl**

*FTE discussed the lack of Florida burrowing owl habitat and lack of documentation within the area. If additional surveys identify a burrowing owl burrow, it would be avoided to the greatest extent practicable and FTE would submit a Nest Removal Permit (Incidental Take Permit) for any nest.*

*FTE indicated that they would avoid the need for an Incidental Take Permit and proposed an effect determination of "no adverse effect anticipated" for the Florida burrowing owl and FWC agreed. FWC agreed that project area does not contain quality burrowing owl habitat and the species is more likely to be encountered further south along the project and noted that some burrowing owl habitat is located further east in St. Lucie County.*



## **5. Wading Birds (Little Blue Heron, Tricolored Heron, and Roseate Spoonbill)**

*FTE explained that a non-listed wading bird rookery (varying historically in use by anhingas, night herons, cattle egrets, and great egrets) at the Okeechobee Road (SR 70) interchange that has been utilized annually for over 18 years. FTE discussed avoidance and minimization measures, including construction phasing during non-nesting season and no nighttime construction, if construction must occur during nesting season.*

*FTE requested FWC input regarding involvement of the non-listed colonial wading bird species that are utilizing the rookery. FWC noted that these birds are tolerant to disturbance and that a program could be developed that would minimize disturbance to these species, allow FTE to accommodate nesting, including seasonal adjustment to the construction schedule and restricting nighttime construction. FTE inquired if a permit would be necessary for construction activities around the rookery. FWC indicated that they do not think a permit would be required as the plan developed to minimize disturbance would be sufficient to avoid take, and the rookery is used by birds that are tolerant to disturbance. FWC noted that if take is avoided, FTE would not need a permit. FWC stated that the plan to minimize disturbance would be reviewed by their wading bird experts, who would also consult on the need for a permit.*

*FTE proposed an effect determination of “no adverse effect anticipated” at this time for the little blue heron, tricolored heron, and roseate spoonbill, and the FWC agreed.*

## **6. Southeastern American Kestrel**

*FTE explained that they have not found documentation of kestrels within the project area; however, potential for utilization of the project area will be continually assessed in man-made (i.e., wooden utility poles) and natural structures.*

*FTE discussed the latest kestrel guidelines, which mention minor projects/existing facility impacts on kestrels, including potential for the need to coordinate with FWC for milling and resurfacing projects. FTE requested FWC input regarding kestrels in the project area and widening in existing ROW inquiring if, based on current guidance, FTE will be required to conduct surveys throughout the length of the corridor. FWC asked if FTE has contacted Jonathan Dickinson State Park to see if they have record of kestrel use of the park. FTE stated they would follow-up with the park. FWC indicated that the project area does not contain much suitable kestrel habitat; however, if surveys are conducted, they can be restricted to areas with good kestrel habitat and disregard cleared farmland with cleared ROW adjacent to it and the species will not likely be in suburban areas.*

*FTE stated they will put together a plan during design with areas proposed for survey for FWC review and agreement. FWC agreed with this approach and noted that by the time the project goes into construction, the project corridor may likely be more developed.*

*FTE proposed a determination of “no adverse effect anticipated” at this time for the Southeastern American kestrel and FWC agreed, recommending FTE contact Jonathan Dickinson State Park for documented occurrences.*

## **7. Gopher Tortoise**

*FTE noted that gopher tortoise burrows had been documented within the project area and that current guidelines will be followed. A permit application will be submitted for burrows that cannot be avoided and tortoises relocated. FTE proposed an effect determination of “no adverse effect anticipated” for the gopher tortoise due to permitting process and mitigation, and FWC agreed.*



## 8. Florida Pine Snake

FTE stated that there have been no known documented occurrences of pine snakes within 1 mile of the project area; however, gopher tortoise burrows are present. Surveys are not proposed for the pine snake and the species will be addressed as a commensal species with gopher tortoise permitting during design. FTE proposed an effect determination of “no adverse effect anticipated” for the Florida pine snake and FWC agreed.

## 9. Least Tern

FTE indicated that least terns have been documented nesting along the project area on flat roofed buildings, with the closest nesting location approximately 300 feet from the project.

FTE inquired on FWC's current approach regarding proximity to nesting and what qualifies as disturbance. FWC stated that rooftop colonies are accustomed to disturbance and buffer distances, other than required for the use of explosives, do not apply. FWC indicated that if 300 feet is the closest nesting activity, then they should not be of concern. FTE indicated that blasting was not currently being considered for project construction.

FTE inquired if additional surveys were warranted. FWC stated that they keep track of rooftop nesting terns internally and at this time surveys would not be required. Surveys would be warranted if work occurred adjacent to the project area (i.e., sand mine) that created nesting habitat, but currently there is no existing habitat other than rooftops. If nesting occurs within closer proximity to the project area, this would not be detrimental to the project if avoidance and minimization measures were implemented. FTE proposed an effect determination of “no adverse effect anticipated” for the least tern and FWC agreed.

## 10. State Protected Plants – Florida Department of Agriculture and Consumer Services (FDACS)

- Golden Leather Fern (*Acrostichum aureum*), Meadow Jointvetch (*Aeschynomene pratensis* var *pratensis*), Many-flowered Grass-pink (*Calopogon multiflorus*), Piedmont Joint Grass (*Coelorachis tuberculosa*), Cutthroat Grass (*Coleataenia abscissa*), Florida Tree Fern (*Ctenitis sloanei*), Cuplet Fern (*Dennstaedtia bipinnata*), Night-scented Orchid (*Epidendrum nocturnum*), Redberry Eugenia (*Eugenia confusa*), Coastal Vervain (*Glandularia maritima*), Spreading Pinweed (*Lechea divaricata*), Celestial Lily (*Nemastylis floridana*), Giant Sword Fern (*Nephrolepis biserrata*), Hand Fern (*Ophioglossum palmatum*), Scrub Bluestem (*Schizachyrium niveum*), Ray Fern (*Schizaea pennula*), Southern Ladies'-tresses (*Spiranthes torta*), Toothed Maiden Fern (*Thelypteris serrata*), Banded Wild-pine (*Tillandsia flexuosa*), Scentless Vanilla (*Vanilla mexicana*), and Redmargin Zephyrlily (*Zephyranthes simpsonii*) and others

FTE noted that they are cognizant of the potential for state protected plant species to occur along the project corridor and will coordinate with FDACS as required. FWC agreed that FTE should coordinate with the FDACS regarding state protected plants.

## 11. Federal Species

- USFWS/NMFS Technical Assistance Meeting with John Wrublik (USFWS) and Jennifer Schull (NMFS) scheduled for Monday 10/26/2020
- Species being addressed with coordination with USFWS include:
  - American alligator (*Alligator mississippiensis*)
  - Florida grasshopper sparrow (*Ammodramus savannarum floridanus*)
  - Florida scrub-jay (*Aphelocoma coerulescens*)
  - Audubon's crested caracara (*Caracara cheriway*)
  - Eastern indigo snake (*Drymarchon couperi*)



- Florida bonneted bat (*Eumops floridanus*)
- Bald eagle (*Haliaeetus leucocephalus*)
- Red-cockaded woodpecker (*Leuconotopicus borealis*)
- Wood stork (*Mycteria americana*)
- Everglade snail kite (*Rostrhamus sociabilis plumbeus*)
- West Indian manatee (*Trichechus manatus*)
- Federal listed plants
- NMFS Protected Species: Smalltooth sawfish (*Pristis pectinata*)

*FTE provided an overview of the species to be discussed with USFWS and NMFS during a future Technical Assistance Meeting. FWC noted that FDEP will be assuming Section 404 review from the USACE and that the USFWS and FWC are currently working on agreements for review of project effects on federal protected species from Section 404 permit assumption. FWC indicated that they will eventually take over the role of commenting on effects to federal protected species resulting from Section 404 permit assumption. FWC anticipates that USFWS will continue to be involved on any FDEP assumed projects that result in a "jeopardy" determination.*

*FWC inquired about potential crested caracara involvement. FTE stated that a single caracara was documented within 1 mile of the proposed project; however, this documentation is from 1995. FWC stated that there was red-cockaded woodpecker habitat at Port St. Lucie that has since been cleared. FTE inquired if FWC would want to attend the USFWS/NMFS Technical Assistance Meeting or receive meeting minutes. FWC explained that they would wait until they are aware of their responsibilities after the Section 404 assumption was completed.*

## **12. Wildlife Habitat Connectivity**

*FTE noted coordination efforts with the NPS regarding the Loxahatchee River crossing. The Loxahatchee River crossing has natural habitat on both sides and meets FDOT criteria for potential consideration of wildlife crossings and habitat connections. FTE indicated that they are considering adding a dry wildlife path underneath the bridge but noted that potential future constriction resulting from improvements to I-95 were unknown at this time. FTE inquired if there are other crossings FWC would want them to consider for wildlife habitat connectivity. FWC agreed the Loxahatchee River crossing is a good location and inquired if FTE was leaving the existing structure. FTE discussed that the current plan was to replace the existing FTE bridge with a similar design and pile arrangement to the abutting the I-95 bridge. The mainline widening at the Loxahatchee crossing would be to the west, noting that I-95 and FTE share ROW. FWC indicated they were concerned with habitat loss. FTE noted that they are intending to span the Loxahatchee River channel (from approx. OHWL to OHWL) and will be removing pilings from the middle of Loxahatchee River channel. This will keep the river channel open and the dry shelf will enable wildlife passage under the bridge.*

*FWC noted their concern with the potential need to take public conservations lands, which would need to be replaced. FTE indicated that they will be staying within the mainline ROW, especially in sensitive areas, and avoiding, to the maximum extent practicable, the taking of public conservation lands.*

*FWC inquired about the Cypress Creek bridge. FTE indicated that bridge is mostly over wetland, with minimal clearance for wildlife during dry season. FWC inquired if this was the same at the I-95 bridge. FWC recommended considering this area due to habitat on both sides and to see if clearance is sufficient for potential for wildlife crossing. FTE noted that they would review the potential for connectivity at the Cypress Creek bridge location to benefit to wildlife, noting the location of the adjacent I-95 bridge.*

## **13. Roundtable/Questions/Comments**



## Florida Fish & Wildlife Conservation Commission Technical Assistance

**Project Development and Environment (PD&E) Study  
for Florida's Turnpike from Jupiter (Indiantown Rd/SR 706)  
to Ft. Pierce (Okeechobee Rd/SR 70)  
Palm Beach, Martin and St. Lucie Counties, FL**

Financial Project ID #: 423374-1-22-01  
October 16, 2020

1

## Agenda

- Project Process & Overview
- Project Challenges
- Widening & Interchange Concepts
- Future Phases & Schedule



2



# Project Process



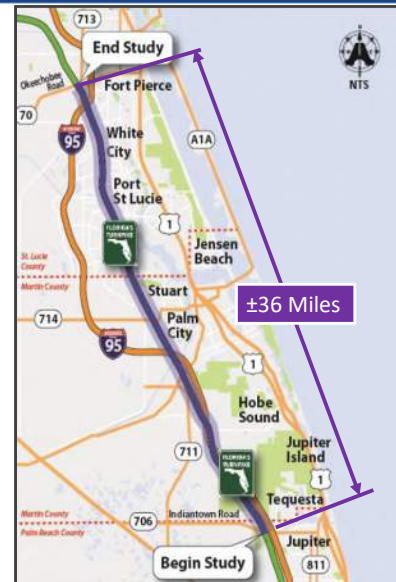
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# PD&E Study



- Jupiter (Indiantown Rd/SR 706) to Ft. Pierce (Okeechobee Rd/SR 70)
  - 2045 Traffic Demand
  - Improve Access
  - Enhance Safety & Evacuation

**Project Facts**  
 36 Miles  
 3 Counties  
 1 Service Plaza



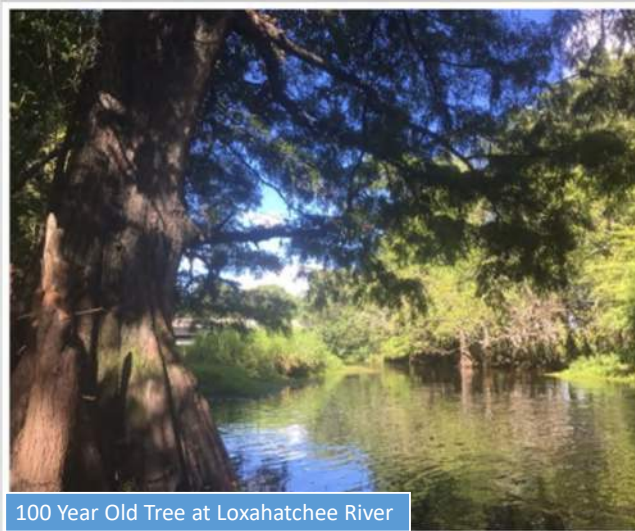
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# Project Challenges



- Florida Gas Transmission (FGT) within Right of Way
  - Primarily on East side
  - Three gas lines
- I-95 (FDOT District 4)
  - Master Plan
  - Shared Right of Way
- Loxahatchee River
  - Wild and Scenic
- Thomas B. Manuel Bridge
  - St. Lucie Canal



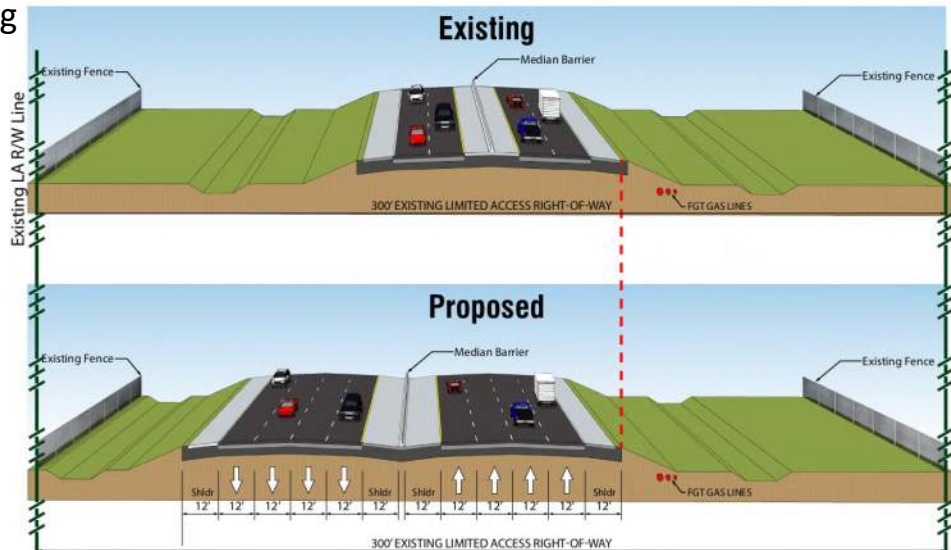
100 Year Old Tree at Loxahatchee River

5

# Proposed Improvements



- Mainline Widening
  - 4 to 8 Lanes
  - FGT Constraints
  - Widen to West
- Interchanges
  - Modify Existing
  - New Interchanges



6



# Interchanges



- Existing Interchange Modifications ◆
  - SR 714 / SW Martin Hwy (Exit 133)
  - Becker Road (Exit 138)
  - Port St. Lucie Blvd. (Exit 142)
  - SR 70 / Okeechobee Road (Exit 152)
  
- Potential Interchange Locations ◆
  - I-95 Direct Connection
    - To be evaluated under a separate study (FPID 446975-1)
  - Crosstown Pkwy (MP 145)
  - Midway Road (MP 150)

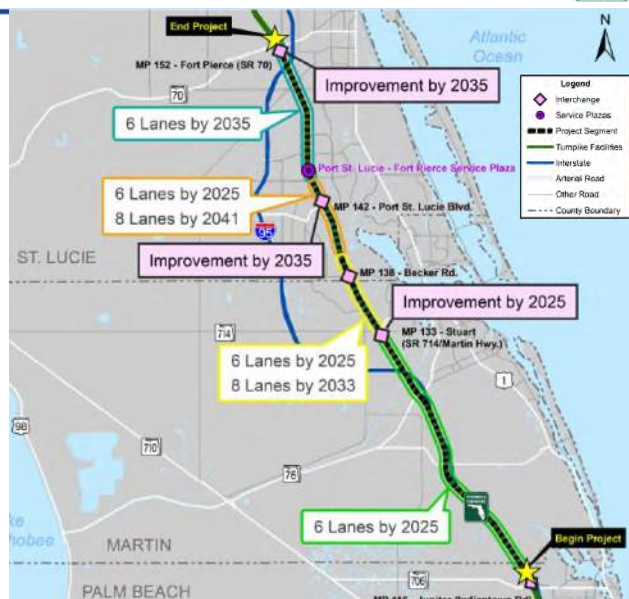


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# Future Phases



- No Future Phases Programmed
- Unfunded Needs Ranking\*
  - #15 – Widening Mainline from Indiantown Rd to SR 70 (through 2030)
  - #25 - Interchange SR 714 (through 2030)



\* Source: 2019 Traffic Trends

8





# Questions?



## FDOT, Florida's Turnpike Enterprise/U.S. Coast Guard Technical Assistance Meeting Notes

### FPID 423374-1-22-01 (Turnpike) Turnpike Mainline Widening from Jupiter to Fort Pierce

#### Palm Beach, Martin, and St. Lucie Counties

Date: Monday, October 19, 2020

Time: 11:00 am

Venue: Microsoft TEAMS meeting

Facilitator: Brian Ribaric

*BR*

*Note: The italicized text below in the meeting agenda are the notes for the topics that were discussed during the meeting.*

#### 1. Introductions

- USCG, Director, District Bridge Program – Randall Overton, MPA
- USCG Bridge Management Specialist – Lisia Kowalczyk
- FTE Environmental Administrator – Philip Stein
- FTE Environmental Permits Coordinator - Annemarie Hammond
- FTE Project Manager – Brian Ribaric, PE (Atkins)
- FTE Permits Coordinator – Fred Gaines, PWS (Atkins)
- FTE Environmental Management Office – Douglas Zang, AICP (Atkins)
- Lochner Project Manager – William Howell, PE
- KCA Project Manager/Chief Environmental Scientist – Robert Whitman

#### 2. Project Overview (Refer to USCG Technical Assistance Meeting Attachments)

- Current Alignment
  - Approx. 37-mile segment of proposed widening of existing 4-lane facility to 8-lanes within existing right of way with four existing and two proposed interchanges being studied.
- ETDM No. 14295 published on May 19, 2017
  - U.S. Coast Guard concerns included potential impacts to the St. Lucie Canal (C-44 Canal):
    - Existing U.S. Coast Guard Permit No. 9-01-7
    - Congressional Authorized Federal Navigation Project channel
    - Navigable Waters of the United States



| Name                                               | Latitude   | Longitude   | Section       | Township | Range |
|----------------------------------------------------|------------|-------------|---------------|----------|-------|
| Loxahatchee River                                  | 26.9542020 | -80.1655250 | 32            | 40S      | 42E   |
| Unnamed Tributary to Cypress Creek                 | 26.9666090 | -80.1746990 | 30            | 40S      | 42E   |
| Cypress Creek                                      | 26.9720150 | -80.1784300 | 19            | 40S      | 42E   |
| Unnamed Tributary to South Fork St. Lucie River #1 | 27.0773870 | -80.2492590 | 16            | 39S      | 41E   |
| Unnamed Tributary to South Fork St. Lucie River #2 | 27.0833930 | -80.2519350 | 16            | 39S      | 41E   |
| Roebuck Creek                                      | 27.1155760 | -80.2733490 | 43            | 38S      | 41E   |
| St. Lucie Canal (C-44; Permit No. 9-01-7)          | 27.1176300 | -80.2748670 | Not Available |          |       |
| Unnamed Tributary to Mapps Creek                   | 27.128227  | -80.2822730 | Not Available |          |       |
| Mapps Creek                                        | 27.1345670 | -80.2865330 | Not Available |          |       |
| Danforth Creek                                     | 27.1550590 | -80.3005280 | 24            | 38S      | 40E   |
| Unnamed Tributary to Bessy Creek                   | 27.1741970 | -80.3133330 | 14            | 38S      | 40E   |
| Bessy Creek                                        | 27.1842820 | -80.3202270 | 10            | 38S      | 40E   |
| County Line Canal (C-23)                           | 27.2054550 | -80.3317800 | 3             | 38S      | 40E   |
| Winters Creek                                      | 27.2269820 | -80.3416500 | 28            | 37S      | 40E   |
| Blakeslee Creek                                    | 27.2340600 | -80.3434900 | 28            | 37S      | 40E   |
| Rim Ditch (C-24)                                   | 27.2624260 | -80.3523430 | 17            | 37S      | 40E   |
| Tenmile Creek                                      | 27.4026050 | -80.3979050 | 25            | 35S      | 39E   |

*Florida's Turnpike Enterprise (FTE) provided an overview of the project and interchanges. A background and current status of the project was provided, which included the project location, and major waterway crossings including the Thomas B. Manuel Bridge across the St. Lucie Canal (C-44) and the Turnpike crossing of the Loxahatchee River, a National Wild and Scenic River (Turnpike crossing is a Scenic Segment). FTE acknowledged the existing USCG Bridge Permit (No. 9-01-7) for the Thomas B. Manuel Bridge crossing. FTE noted that the project proposes widening from the existing 4 lane typical section to an ultimate 8 lane section, and includes improvements to existing interchanges, as well as evaluation of two new interchanges. FTE indicated that the widening is proposed principally to the west, with a few exceptions, due to the location of the FGT gas lines. FTE indicated that the project construction was currently unfunded, and discussed potential interim 6-lane segment needs, in comparison with the ultimate 8-lane facility. FTE noted that USCG reviewed and provided comment on the project's ETDM Programming Screen, published in 2017.*

*USCG inquired if federal funds would be involved in order to determine the lead federal agency. FTE indicated that FTE operates on state funds and federal funds would likely not be involved with this project. USCG inquired if the US Army Corps of Engineers (USACE) had been contacted, and FTE acknowledged that a pre-application meeting had been held with the USACE and the South Florida Water Management District (SFWMD). FTE also informed the USCG of the upcoming technical assistance meeting with the US Fish and Wildlife Service (USFWS) and the National Marine Fisheries Service (NMFS). FTE assumes that the USACE will be the lead federal agency for the permitting of this project if it moves forward into design.*

*FTE indicated the project environmental document was being prepared as a State Environmental Impact Report (SEIR), which is consistent with the National Environmental Policy Act (NEPA). The differences between a SEIR and federal NEPA documents were discussed and focused on*



*Section 4(f) properties as the major difference. USCG indicated that they are not bound by Section 4(f) properties in their review of bridge permit applications and instructed FTE to follow the guidance provided in the USCG Bridge Permit Application Guide (BPAG) for their requirements. If FTE sends an email to USCG, they will forward a link to the USCG BPAG.*

*FTE indicated that the project is anticipated to be a Type 2 Categorical Exclusion (CE) if a federal document was being produced. FTE will send the supporting environmental documentation along with the SEIR when USCG Bridge Permit applications are submitted. USCG indicated that they only require the conclusion documentation resulting from technical assistance with cooperating federal agencies, such as concurrence with species effects determinations from USFWS and NMFS. FTE agreed with this approach.*

*FTE proceeded with a review of the project's waterway bridge crossings including the Loxahatchee River (OFW, Wild and Scenic River), Cypress Creek, tributaries to the South Fork of the St. Lucie River, Roebuck Creek, St. Lucie Canal (C-44), Mapps Creek, Danforth Creek, Bessy Creek, County Line Canal (C-23) Canal, Winters Creek, Blakeslee Creek, Rim Ditch (C-24), Tenmile Creek, and other unnamed tributaries. FTE continued with a discussion of the larger waterways aided by the aerial photographic exhibits. These crossings included the St. Lucie Canal (C-44 Canal), the Rim Ditch (C-24 Canal), Tenmile Creek, and the Loxahatchee River. FTE noted that the presence of a vertical weir in the County Line Canal (C-23 Canal) effectively excluded any marine navigation into the project area along this waterway.*

### **3. St. Lucie Canal (C-44 Canal) – Thomas B. Manuel Bridge**

- For new southbound replacement structure, maintain existing navigational horizontal and vertical clearances of newer northbound structure proposed for widening
- Modification of Existing USCG Permit No. 9-01-7 for new bridge construction activities
- Neighborhood southwest of bridge; noise will be a concern if bridge is widened closer to the neighborhood and should be evaluated
- NEPA documentation and approvals

*The proposed improvements to the Thomas B. Manuel Bridge crossing of the St. Lucie Canal were discussed and aided by bridge concept drawings. The USCG indicated that a Permit Amendment to the existing USCG Bridge Permit (No. 9-01-7) would be required for the St. Lucie Canal (C-44 Canal) crossing.*

*USCG also indicated that the St. Lucie Canal is a federal channel administered by the USACE, and a Section 408 authorization from the USACE in Jacksonville would be required for this crossing. FTE acknowledged their understanding that Section 408 approval would be needed. Since this section of the St. Lucie Canal is not operated by SFWMD, USCG recommended that FTE contact the Section 408 reviewers directly to coordinate the proposed project during design. USCG indicated that they would only need the Section 408 approval letter for their permit file.*

*USCG indicated that the proposed design should not include additional encroachment into the existing navigational envelope and maintenance of the existing horizontal and vertical navigational clearances at the crossing at a minimum would address permit related criteria.*

*The existing bridge fender system was discussed. USCG indicated that if the existing fender system were evaluated and proposed for modification or removal, discussions about the fender system would be best held before the permit application was submitted.*



*FTE indicated the neighborhood adjacent to the bridge had raised concerns about additional noise and light due to additional traffic resulting from the widening. FTE indicated that studies to address these items would be performed to support the design of the bridge widening project. USCG agreed to the approach.*

*USCG discussed that their public notification process for projects that they are not the lead federal agency addresses navigation aspects only and is separate from the USACOE Section 404 permit process. The USCG process involves mailing a 1-page notice of availability of a Public Notice (PN) to nearby residents and marine interests with waterfront access within ½ mile of the bridge. Public navigation related comments received during the comment period (usually 30 days) are forwarded to the applicant for response through the USCG, all other comments are forwarded to the applicant for response directly to the commenter. USCG indicated that if the project moves into design, a bridge permit amendment would be issued by the USCG Miami District offices instead of a new bridge permit.*

#### **4. Rim Ditch (C-24 Canal)**

- Discussion on potential USCG permitting requirements

*The proposed improvements to the Turnpike crossing of the C-24 Canal (Rim Ditch) were discussed. The USCG indicated that a Bridge Permit would be required for the C-24 Canal crossing, and that this waterway would not qualify for an Advance Approval as the vessels appear to exceed the size for USCG advanced approval. FTE identified a water control structure located approx. 0.44 miles upstream that limited navigation. USCG recommended that FTE reach out to the residents west of the Turnpike bridge to discuss the proposed project prior to submitting an application. USCG indicated that no formal vessel survey would be required to document waterway usage, just a generalized characterization of the types of vessel using the C-24 Canal, both east and west of the Turnpike crossing. USCG requested that any downstream structures be identified as to their impact on navigation and specified the bridge at SE Oaklyn Street. FTE stated that coordination with the SFWMD resulted in identifying the requirement for a SFWMD ROW Occupancy Permit for crossing the C-24 Canal.*

#### **5. Tenmile Creek**

- Discussion on potential USCG permitting requirements

*The proposed improvements to the Turnpike crossing over Tenmile Creek were discussed. The USCG indicated that a Bridge Permit would not be required for the Tenmile Creek crossing due to the proximity of the upstream control structure to the bridge, and that no USCG coordination would be required for the proposed Turnpike improvements crossing Tenmile Creek. If FTE would like documentation of this for the file, then USCG can provide but not required by USCG.*

#### **6. Loxahatchee River Bridge**

- No USCG Permit required based on previous coordination with USCG

*The proposed improvements to the Turnpike crossing over the Loxahatchee River were discussed. The USCG indicated that a Bridge Permit would not be required for the Loxahatchee River crossing since only non-motorized river traffic is allowed at the crossing. FTE inquired about USCG involvement and coordination with the National Park Service (NPS) due to the river's classification as a National Wild and Scenic River. USCG indicated that they would only be concerned if a USCG Bridge Permit were required for this crossing; and it is not.*



## 7. Other Waterway Crossings

- Determination of navigable waters of US and need for USCG Permit at each location

*The proposed improvements to the Turnpike crossing over County Line Canal (C-23 Canal) were discussed. FTE reiterated the presence of a vertical weir in the County Line Canal (C-23 Canal) preventing navigation at the Turnpike crossing. The USCG indicated that a Bridge Permit would not be required for the C-23 Canal crossing, and that no USCG coordination would be required for the proposed Turnpike improvements crossing the C-23 Canal. The USCG did indicate that in some situations, that a portage can maintain navigability of a waterway and if there were signs of commercial waterway usage, but that was not the case at the C-23 Canal crossing.*

*A brief review of the other smaller waterway crossings resulted in the USCG acknowledging that no further USCG coordination would be required for these other crossings.*



## United States Coast Guard Technical Assistance

**Project Development and Environment (PD&E) Study  
for Florida's Turnpike from Jupiter (Indiantown Rd/SR 706)  
to Ft. Pierce (Okeechobee Rd/SR 70)  
Palm Beach, Martin and St. Lucie Counties, FL**

Financial Project ID #: 423374-1-22-01  
October 19, 2020

1

## Agenda

- Project Process & Overview
- Project Challenges
- Widening & Interchange Concepts
- Future Phases & Schedule



2



# Project Process



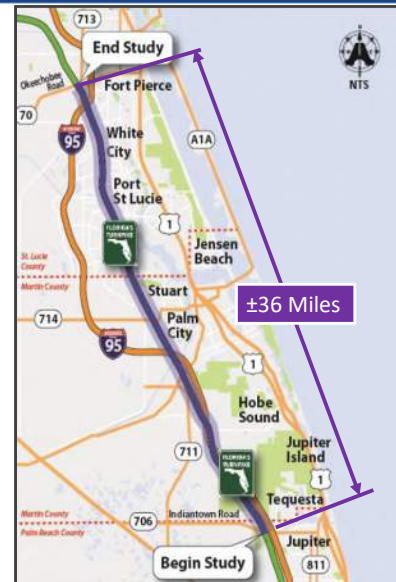
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# PD&E Study



- Jupiter (Indiantown Rd/SR 706) to Ft. Pierce (Okeechobee Rd/SR 70)
  - 2045 Traffic Demand
  - Improve Access
  - Enhance Safety & Evacuation

**Project Facts**  
 36 Miles  
 3 Counties  
 1 Service Plaza



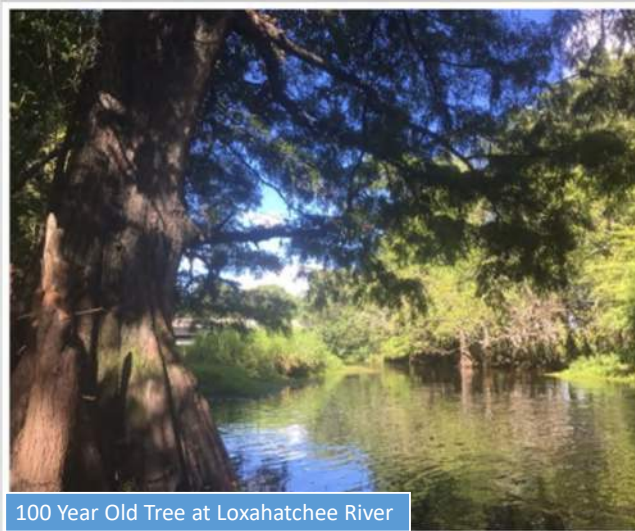
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# Project Challenges



- Florida Gas Transmission (FGT) within Right of Way
  - Primarily on East side
  - Three gas lines
- I-95 (FDOT District 4)
  - Master Plan
  - Shared Right of Way
- Loxahatchee River
  - Wild and Scenic
- Thomas B. Manuel Bridge
  - St. Lucie Canal



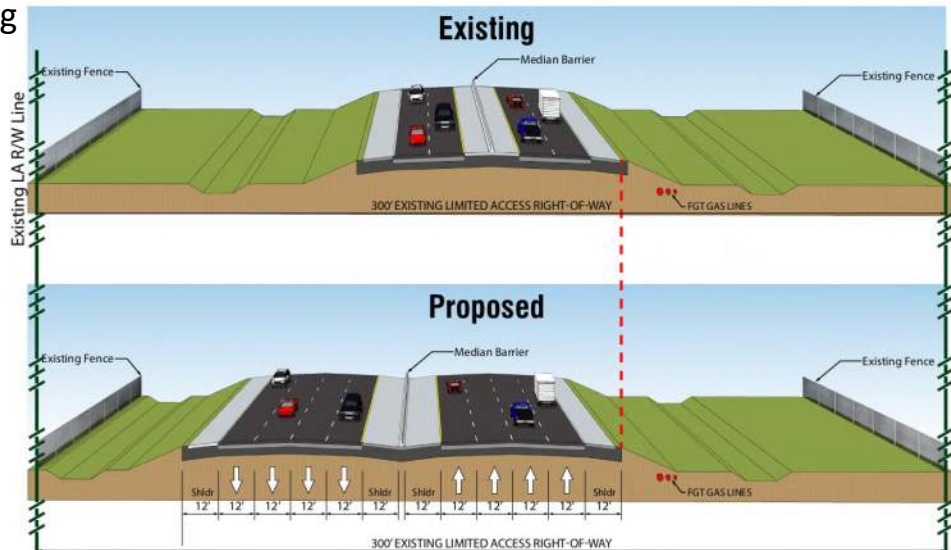
100 Year Old Tree at Loxahatchee River

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# Proposed Improvements



- Mainline Widening
  - 4 to 8 Lanes
  - FGT Constraints
  - Widen to West
- Interchanges
  - Modify Existing
  - New Interchanges



6



# Interchanges



- Existing Interchange Modifications ◆
  - SR 714 / SW Martin Hwy (Exit 133)
  - Becker Road (Exit 138)
  - Port St. Lucie Blvd. (Exit 142)
  - SR 70 / Okeechobee Road (Exit 152)
  
- Potential Interchange Locations ◆
  - I-95 Direct Connection
    - To be evaluated under a separate study (FPID 446975-1)
  - Crosstown Pkwy (MP 145)
  - Midway Road (MP 150)

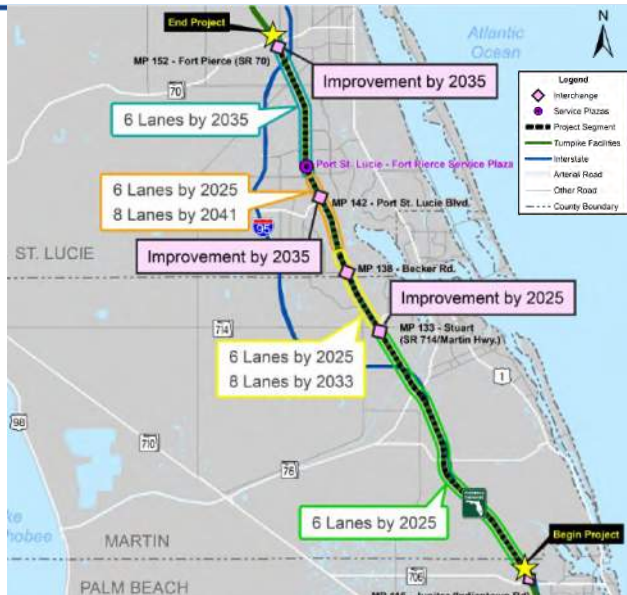


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# Future Phases



- No Future Phases Programmed
- Unfunded Needs Ranking\*
  - #15 – Widening Mainline from Indiantown Rd to SR 70 (through 2030)
  - #25 - Interchange SR 714 (through 2030)



\* Source: 2019 Traffic Trends

8





# Questions?



## FDOT, Florida's Turnpike Enterprise/U.S. Fish and Wildlife Service/National Marine Fisheries Service Technical Assistance Meeting Notes

### FPID 423374-1-22-01 (Turnpike) Turnpike Mainline Widening from Jupiter to Fort Pierce

#### Palm Beach, Martin, and St. Lucie Counties

Date: October 26, 2020

Time: 10:00-11:00 am

Venue: Microsoft TEAMS meeting

Facilitator: Brian Ribaric

*BR*

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*Note: The italicized text below in the meeting agenda are the notes for topics that were discussed during the meeting.*

#### 1. Introductions

- **USFWS Staff – John Wrublik**
- **NMFS Staff – Jennifer Schull**
- **FTE Environmental Administrator – Philip Stein**
- **FTE Environmental Permits Coordinator - Annemarie Hammond**
- **FTE Project Development Engineer - Rax Jung, PhD, PE**
- **FTE Project Manager – Brian Ribaric, PE (Atkins)**
- **FTE Permit Coordinator – Fred Gaines, PWS (Atkins)**
- **FTE Senior Environmental Scientist - Douglas Zang (Atkins)**
- **Lochner Project Manager – William Howell, PE**
- **KCA Project Manager/Chief Environmental Scientist – Robert Whitman**
- **KCA Senior Environmental Scientist – Ashley Abdel-Hadi**

#### 2. Project Overview (map provided)

*Florida's Turnpike Enterprise (FTE) provided an overview of the project and interchanges. A background and current status of the project was provided, which included the project location and summary of primary land uses within the proposed project area. FTE noted that the 37-mile project proposes widening from the existing 4 lane typical section to an ultimate 8 lane section, and includes improvements to existing interchanges, as well as evaluation of two new interchanges. FTE indicated that the widening is proposed principally to the west, with a few exceptions, due to the location of the FGT gas lines. FTE discussed other important project details including the Loxahatchee River National Wild and Scenic River, the close proximity of I-95 (an FDOT District Four facility), and the Thomas B. Manuel Bridge crossing of the St. Lucie Canal (C-44). FTE stated that a related project, the I-95 Direct Connection Interchange, which will provide a direct connection between the Turnpike and I-95, will be evaluated under a separated study, in compliance with NEPA. FTE indicated that the project construction was currently unfunded, and discussed potential interim 6-lane segment needs, in comparison with the ultimate 8-lane facility.*

*NMFS inquired when permitting would occur, and if permitting would be for the 6-lane facility or 8-lane facility. FTE stated that the project would be divided into several design segments, and that these design segments were currently not included on the FTE 5-Year Work Program. FTE stated that the permitting time frame is uncertain at this time and that permitting will depend on how project segments are ultimately scheduled in the FTE work program. FTE noted that some segments are in greater need and may proceed directly to 8-lanes, while other segments may advance to 6-lanes first, before expanding to the ultimate 8-lane facility.*



USFWS inquired if this project will have federal funding. FTE stated that it will not, and that the project will be funded through FTE toll revenue and other non-federal funds. USFWS stated that if the project requires a USACE permit that will be the federal nexus for their consultation.

FTE noted that meetings had been held with the USACE (and SFWMD), the NPS (National Park Service) regarding the Loxahatchee River (Wild and Scenic River) crossing, and the USCG regarding three of the bridge crossings. FTE indicated that the SEIR for this project will be prepared to be consistent with NEPA.

FTE proceeded with a review of the project aerial photographic exhibits and discussed the existing general land uses within the project corridor and the riverine/canal systems that the project will be crossing, including the Loxahatchee River (OFW, Wild and Scenic River), Cypress Creek, tributaries to the South Fork of the St. Lucie River, Roebuck Creek, St. Lucie Canal (C-44), Mapps Creek, Danforth Creek, Bessy Creek, County Line Canal (C-23) Canal, Winters Creek, Blakeslee Creek, Rim Ditch (C-24), Tenmile Creek, and other unnamed tributaries.

FTE summarized the USFWS and NMFS review comments included in the project's ETDM No. 14295 Programming Screen, published in May 2017. These comments related to federally protected species and Essential Fish Habitat for the white shrimp. FTE provided the preliminary wetland and surface water impact acreage of - 62.67 acres of wetlands and 365.54 acres of surface waters for a total of 428.21 acres and noted that these impacts include areas within the existing ROW and proposed ROW for the preferred interchange alternatives and involve many surfacewater ditches. FTE noted that these preliminary impacts were for the Build Alternative, and that the No-Build Alternative remains a viable project option. FTE then initiated a discussion of federally protected species.

### **3. American Alligator**

FTE stated that the American alligator is listed as threatened due to similarity in appearance to the American crocodile; however, there is no habitat for the American crocodile and proposed determination of "no effect" on the American alligator.

USFWS agreed with the "no effect" determination and stated they would not consult on the species as they would for other federally protected species due to threatened by similarity of appearance.

### **4. Florida Grasshopper Sparrow**

FTE stated that the project area is within the Florida Grasshopper Sparrow Consultation Area and there is dry prairie habitat near the St. Lucie interchanges. FTE noted that this area is not burned and does not meet the known habitat requirements for the species. FTE explained that some of the species requirements are met in pasturelands; however, these areas are not prime Florida grasshopper sparrow habitat and there is no known documentation of this species within 30 miles. FTE proposed an effect determination of "may affect, but is not likely to adversely affect", and proposed that no surveys would be performed for this species.

USFWS explained that the effect determination for this species could be changed to "no effect" if there is no habitat or evidence of Florida grasshopper sparrows. FTE inquired if a "no effect" determination could be based on lack of habitat and observations without surveys. USFWS noted that it is preferred to survey if there is potential habitat, though unlikely to encounter the species. USFWS further clarified that surveys could be conducted in appropriate habitat areas and then if the species was determined to be absent the effect determination could be changed to "no effect". FTE stated that the effect determination would be kept as "may affect, but is not likely to adversely affect" but inquired if surveys would be required. USFWS explained that they do not



*anticipate Florida grasshopper sparrows within the project area and would likely not request surveys during design based on the location of the project.*

#### **5. Florida Scrub-jay**

*FTE indicated that the project is within the Florida Scrub-jay Consultation Area; however, there are no documented occurrences within 1 mile of the project. FTE explained that based on the habitat distribution map, there is some Type I and Type II habitat, but no historic occurrence of Florida scrub-jay families within 1 mile. FTE noted that there are Florida scrub-jays in Jonathan Dickinson State Park, approximately 4 miles northeast of project. FTE stated that surveys during design will be conducted in Type I, Type II and consider surveying Type III and adjacent habitats, if appropriate. FTE noted that, based on observations, potential Florida scrub-jay habitat within the project vicinity generally appeared to be of low quality and unmanaged for the species.*

*FTE is proposing a determination of “may affect” until surveys are conducted during design and will change to ‘may affect, but is not likely to adversely affect” if the species is determined to be absent from the project area. USFWS agreed with the proposed determination. FTE inquired if surveys of adjacent habitat to Jonathan Dickinson State Park will be necessary during design and USFWS stated that surveys should be conducted in suitable habitat with scrub oaks.*

#### **6. Audubon’s Crested Caracara**

*FTE indicated that the project is within the Crested Caracara Consultation Area. The FTE noted that a single caracara was documented in 1995 within 1 mile of the project, based on the USFWS telemetry data, and that the project contains potential foraging and nesting habitat adjacent to the project area in large tracts of pastureland in the area near the C-23 Canal. FTE stated that they anticipate performing surveys for this species during design and are proposing a determination of “may affect” until survey results are known. The USFWS agreed with this determination and approach.*

#### **7. Eastern Indigo Snake**

*FTE stated that there are no known observations within 1 mile of the preferred alternative. FTE is currently relying on the Eastern Indigo Snake Programmatic Effect Determination Key. FTE explained that they are not proposing surveys for this species, but will implement the Standard Protection Measures for the Eastern Indigo Snake during project construction. FTE proposed an effect determination of “may affect, but is not likely to adversely affect” due to lack of observations within 1 mile and no real scrub habitat. USFWS agreed with this determination.*

#### **8. Florida Bonneted Bat**

*FTE noted that the project is within the Florida Bonneted Bat Consultation Area and there is potential roosting habitat and natural habitats proposed for impacts in the interchanges. FTE proposed hybrid presence/absence surveys during design in existing cleared right of way areas with minimal tree presence and full acoustic surveys in new right of way areas with natural vegetative communities (i.e., interchanges). USFWS agreed with the conceptual survey approach. FTE is proposing a determination of “may affect” until surveys are completed and USFWS agreed with this determination.*

#### **9. Red-cockaded Woodpecker**

*FTE indicated that there are no known red cockaded-woodpecker colonies within 1 mile of the project, but the project is within the Red-cockaded Woodpecker Consultation Area. FTE noted that pine flatwoods are present throughout project area, particularly near the Loxahatchee River, Bessy Creek, and Tenmile Creek. FTE discussed that the majority of existing flatwoods habitat is fire suppressed; however, flatwoods are managed in areas within and near Jonathan Dickinson State Park. FTE proposed conducting limited surveys during design for this species in areas of higher quality habitat. FTE is proposing a determination of “may affect” until surveys are conducted during design. The USFWS agreed with this effect determination and approach.*



### **10. Wood Stork**

*FTE stated that the project is within the CFA of 5 wood stork colonies; however, none are within 1,500 feet of the project. FTE noted that wood stork foraging habitat will be addressed during design and surveys for the species are not proposed. FTE explained that they will follow the Wood Stork Effect Determination Key and propose mitigation as required. FTE proposed a determination of “may affect, but is not likely to adversely affect” and USFWS agreed with this determination.*

### **11. Everglade Snail Kite**

*FTE explained that there is documented Everglade snail kite nesting in the reservoirs associated with and to the northwest of Tenmile Creek and the project is within the Everglade Snail Kite Consultation Area. FTE proposes limited survey areas during design adjacent to the project where the species has been observed or nesting documented, and not the remainder of project area. USFWS agreed with the approach. FTE proposed a determination of “may affect” until surveys are completed but noted that they are not anticipating impacts. The USFWS agreed with this effect determination.*

### **12. West Indian Manatee**

*FTE discussed that they are not impacting critical manatee areas and will implement the Standard Manatee Conditions for In-water Work. FTE explained that Critical Habitat is within a 1-mile buffer surrounding the project area near the Loxahatchee River and Cypress Creek. FTE proposed an effect determination of “may affect, but is not likely to adversely affect” and USFWS agreed with the determination.*

### **13. Federal Protected Plants**

- Okeechobee Gourd (*Cucurbita okeechobeensis*)

*FTE stated that there are no observations or documentation of federal protected plant species, including the Okeechobee gourd, within 1 mile of the project. FTE proposed a determination of “no effect” and USFWS agreed with the determination.*

### **14. Bald Eagle**

*FTE explained that there are no bald eagle nests proposed to be impacted by the project and if there is work proposed within 660 feet on an eagle nest, FTE will seek technical assistance with the USFWS Bald Eagle group (Ulgonda Kirkpatrick).*

*USFWS agreed that no additional federal listed species needed to be included in the evaluation at this time.*

### **15. NMFS Protected Species: Smalltooth Sawfish**

*FTE stated that there was the potential for involvement with the smalltooth sawfish at the C-24 Canal and St. Lucie Canal (C-44 Canal). FTE stated that the Sea Turtle and Smalltooth Sawfish Construction Conditions would be implemented during construction. FTE proposed an effect determination of “may affect, but is not likely to adversely affect”, will implement the construction conditions and is not proposing surveys. NMFS agreed, stating that blasting is the only thing of concern. FTE noted that they are not proposing blasting currently, but design will have to confirm and that other unknowns include potential SFWMD requirements for channel deepening/dredging at Turnpike crossings. NMFS noted that the closest known smalltooth sawfish documentation was approximately 3 miles away. NMFS indicated that further coordination during design would involve informal consultation unless blasting was proposed, but agreed that the species should be included in the FTE's assessment.*



FTE explained that they had previously coordinated with SFWMD and they anticipate that Section 408 coordination with the USACE will be required for the St. Lucie Canal crossing. FTE inquired if NMFS coordination would be required with USACE for Section 408 approval, or only under Section 404. NMFS anticipates that the S404 consultation should address S408 aspects as well but indicated they would investigate this further. NMFS indicated that the smalltooth sawfish could tolerate low salinities and they were interested if reasonable and appropriate habitat for the species was available within or nearby the project area. NMFS indicated that mangrove lined shorelines provide habitat for the smalltooth sawfish and would not consider anything upstream of salinity-control structures (barriers) to be habitat for this species. FTE stated that there is mangrove habitat along the St. Lucie Canal. NMFS agreed with the "may affect, but is not likely to adversely affect" determination for the smalltooth sawfish, and the implementation of the construction conditions.

### **16. Essential Fish Habitat**

FTE discussed the existing GIS database sources of Essential Fish Habitat (EFH). EFH mapped along the Loxahatchee River and Cypress Creek crossings is outside of the project area and FTE is not proposing an EFH assessment of these locations. NMFS stated that the GIS databases are good sources but should not be relied on exclusively. NMFS explained that anything upstream of a salinity control structure would not be considered EFH, and that EFH must be tidally influenced and accessible by NOAA managed fisheries species. NMFS further explained that any tidal river or canal bottom would be considered EFH for penaeid shrimp (white shrimp) and other habitats they would be concerned with are mangroves and submerged aquatic vegetation (SAV; seagrasses) in tidally influenced areas.

FTE indicated that the existing water management dam structures along the Loxahatchee River are located upstream of the Loxahatchee River crossing. FTE inquired if structures located downstream of the Turnpike crossings would allow elimination of those areas from consideration for EFH involvement. NMFS referred to the discussion above and stated they would have to research this further in more detail. FTE referenced the ETDM comments (7 crossings listed in ETDM). FTE stated that a salinity control structure was located downstream of the Turnpike crossing at the C-23 Canal. NMFS agreed that no EFH involvement would be considered at the C-23 Canal crossing. FTE discussed the Tenmile Creek crossing, which has a water control structure located upstream of the Turnpike. NMFS indicated that this area would be considered EFH if it were tidal and accessible to NOAA managed fisheries species. NMFS and FTE discussed the potential for NMFS providing the locations of EFH involvement for Turnpike crossings, and if not readily available, FTE will reach out again during design permitting.

NMFS stated that they would require implementation of avoidance and minimization measures, but mitigation requirements for impacts to penaeid shrimp EFH (river and canal bottoms) would likely be minimal, if even required as these areas typically rebound quickly. NMFS explained that they are most concerned with impacts to mangroves and seagrasses. FTE explained that no seagrasses or SAV have been documented in the project area, thus far, but that they will look further into tidal regimes and SAV during project design and permitting.

FTE and NMFS further discussed the potential for EFH involvement at Turnpike crossings:

- Roebuck Creek – FTE stated this crossing is likely somewhat tidally influenced and will be assessed in more detail during design.
- St. Lucie Canal – FTE stated this crossing is tidally influenced and will be assessed during design.
- Unnamed tributary to Mapps Creek – EFH mapped within ½ mile and FTE will investigate if it is tidal during design.
- C-23 Canal – FTE stated there is a major structure upstream and weir downstream with water flowing west to east. NMFS agreed this would not be



*considered EFH.*

- *C-24 Canal – FTE noted this crossing is tidal and will address this crossing moving forward during design.*
- *Tenmile Creek – FTE stated there are some tidal gauges and will look further into it during design. NMFS stated this was on their list as tidal.*

*FTE inquired if an EFH assessment is only required for major bridge crossings or if an assessment is also necessary for culverts and minor crossings. NMFS explained that this depends if culverts are receiving tidal water and accessible by NOAA fisheries trust resources. NMFS indicated that if these culverts have tidal water exchange, and they are considered accessible to NMFS species, then they should be considered as EFH for assessment. FTE inquired if utilizing the nearest appropriate tidal gauge would be sufficient in determining if a crossing was tidally influenced. NMFS agreed and stated that if flow is not impacted or restricted (i.e., culvert sizes/volumes were maintained or increased and associated extension minimal impacts) by the project, then NMFS would find that there were no impacts to EFH for these crossings.*

*NMFS noted that FTE will likely have mangrove impacts and mitigation will be challenging as limited mitigation banking options are currently available in the area. FTE stated they have not yet specifically quantified project impacts and inquired if mangroves growing in manmade areas (rip-rap, sand cement revetments) could be cut back, maintained, or potentially buried during replacement without requiring mitigation, as mangroves would likely recruit into the replaced rip rap after construction. NMFS explained that all mangroves are important and are considered EFH regardless if they are on manmade riprap or natural shorelines. FTE stated that they will propose a survey methodology to document mangrove impacts for NMFS early in the design process to avoid issues during permitting. NMFS agreed with the approach.*

#### **17. Wildlife Habitat Connectivity**

*FTE inquired if USFWS has concerns or issues with wildlife habitat connectivity. USFWS stated the project area is mostly developed and the project involves widening of an existing facility. FTE explained that they have coordinated with NPS regarding dry wildlife crossing (shelf) under the replaced Loxahatchee River Bridge. USFWS indicated that anything that could be done to improve or enhance wildlife connectivity would be appreciated, but that USFWS was not currently contemplating requirements for wildlife crossings for this project.*

#### **18. Roundtable/Questions/Comments**

*NMFS inquired on timeline and next steps. FTE explained that they are planning public hearings for early 2021 and completing the PD&E study. FTE stated the design for this project has not made it into the current 5-year work program. Some design segments of the overall project have the potential to be included within the next 10-year plan and that will be when the agencies are engaged again. NMFS inquired if this would be permitted in segments and FTE confirmed that design/construction of the overall project will be divided into segments.*



## FDOT, Florida's Turnpike Enterprise/U.S. Fish and Wildlife Service/National Marine Fisheries Service Technical Assistance Meeting Agenda

### FPID 423374-1-22-01 (Turnpike) Turnpike Mainline Widening from Jupiter to Fort Pierce

#### Palm Beach, Martin, and St. Lucie Counties

Date: October 26, 2020

Time: 10:00-11:00 am

Venue: GoTo Meeting`

#### 1. Introductions

#### 2. Project Overview (map provided)

- Current Alignment
  - Approx. 37-mile segment of proposed widening of existing 4-lane facility to 8-lanes within existing right of way with four existing & 2 proposed interchanges being studied.
  - Loxahatchee River Wild and Scenic River designation - NPS
- ETDM No. 14295 published on May 19, 2017
  - USFWS concerns included potential impacts to the following federal listed species:
    - American alligator (*Alligator mississippiensis*)
    - Florida grasshopper sparrow (*Ammodramus savannarum floridanus*)
    - Florida scrub-jay (*Aphelocoma coerulescens*)
    - Audubon's crested caracara (*Caracara cheriway*)
    - Eastern indigo snake (*Drymarchon couperi*)
    - Florida bonneted bat (*Eumops floridanus*)
    - Red-cockaded woodpecker (*Leuconotopicus borealis*)
    - Wood stork (*Mycteria americana*)
    - Everglade snail kite (*Rostrhamus sociabilis plumbeus*)
    - West Indian manatee (*Trichechus manatus*)
    - Federal listed plants
    - Bald eagle (*Haliaeetus leucocephalus*)
    - NMFS Protected Species:
      - Smalltooth sawfish (*Pristis pectinata*)
      - Essential fish habitat
- 428.21 acres of wetland impacts anticipated with the preferred alternative (preliminary impacts - 62.67 acres of wetlands and 365.54 acres of surface waters)

#### 3. American Alligator

#### 4. Florida Grasshopper Sparrow

#### 5. Florida Scrub-jay

#### 6. Audubon's Crested Caracara

#### 7. Eastern Indigo Snake



- 8. Florida Bonneted Bat**
- 9. Red-cockaded Woodpecker**
- 10. Wood Stork**
- 11. Everglade Snail Kite**
- 12. West Indian Manatee**
- 13. Federal Protected Plants**
  - Okeechobee Gourd (*Cucurbita okeechobeensis*)
- 14. Bald Eagle** – coordinate proposed impacts with USFWS Bald Eagle group
- 15. NMFS Protected Species:**
  - **Smalltooth Sawfish**
- 16. Essential Fish Habitat**
- 17. Wildlife Habitat Connectivity**
- 18. Roundtable/Questions/Comments**



## USFWS & NMFS Technical Assistance

**Project Development and Environment (PD&E) Study  
for Florida's Turnpike from Jupiter (Indiantown Rd/SR 706)  
to Ft. Pierce (Okeechobee Rd/SR 70)  
Palm Beach, Martin and St. Lucie Counties, FL**

Financial Project ID #: 423374-1-22-01  
October 26, 2020

1

## Agenda

- Project Process & Overview
- Project Challenges
- Widening & Interchange Concepts
- Future Phases & Schedule



2



# Project Process



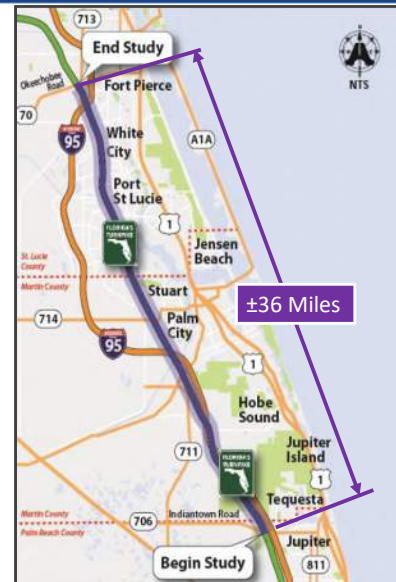
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# PD&E Study



- Jupiter (Indiantown Rd/SR 706) to Ft. Pierce (Okeechobee Rd/SR 70)
  - 2045 Traffic Demand
  - Improve Access
  - Enhance Safety & Evacuation

**Project Facts**  
 36 Miles  
 3 Counties  
 1 Service Plaza



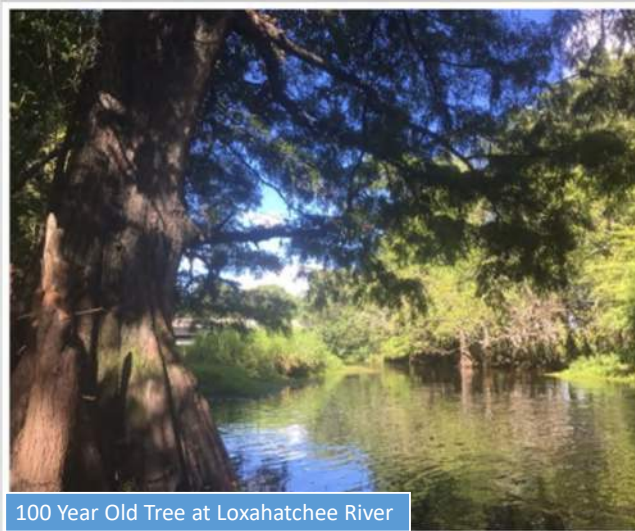
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# Project Challenges



- Florida Gas Transmission (FGT) within Right of Way
  - Primarily on East side
  - Three gas lines
- I-95 (FDOT District 4)
  - Master Plan
  - Shared Right of Way
- Loxahatchee River
  - Wild and Scenic
- Thomas B. Manuel Bridge
  - St. Lucie Canal



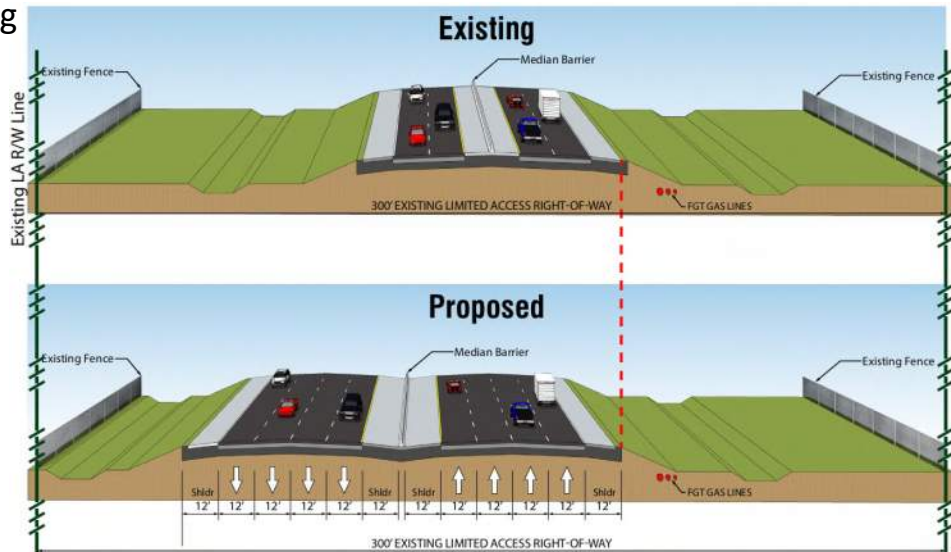
100 Year Old Tree at Loxahatchee River

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# Proposed Improvements



- Mainline Widening
  - 4 to 8 Lanes
  - FGT Constraints
  - Widen to West
- Interchanges
  - Modify Existing
  - New Interchanges



6



# Interchanges



- Existing Interchange Modifications ◆
  - SR 714 / SW Martin Hwy (Exit 133)
  - Becker Road (Exit 138)
  - Port St. Lucie Blvd. (Exit 142)
  - SR 70 / Okeechobee Road (Exit 152)
  
- Potential Interchange Locations ◆
  - I-95 Direct Connection
    - To be evaluated under a separate study (FPID 446975-1)
  - Crosstown Pkwy (MP 145)
  - Midway Road (MP 150)

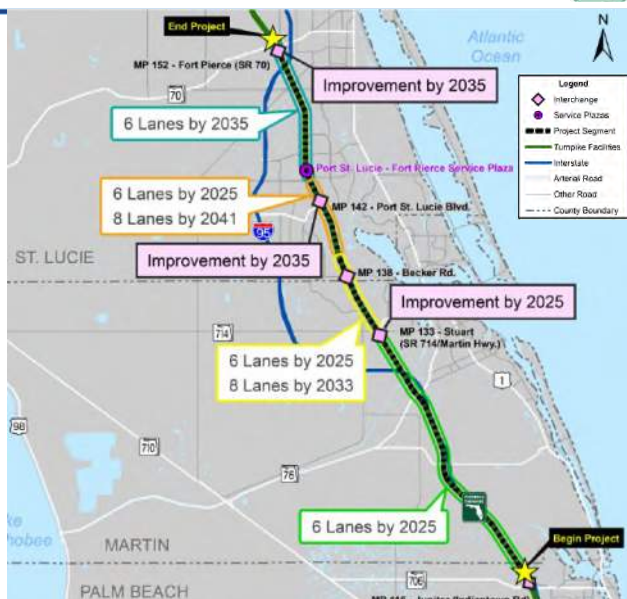


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# Future Phases



- No Future Phases Programmed
- Unfunded Needs Ranking\*
  - #15 – Widening Mainline from Indiantown Rd to SR 70 (through 2030)
  - #25 - Interchange SR 714 (through 2030)



\* Source: 2019 Traffic Trends

8





# Questions?



**APPENDIX F**  
*Recreational Areas Map*



# FLORIDA'S TURNPIKE (SR 91) WIDENING

FROM JUPITER (INDIANTOWN ROAD/SR 706) TO FT. PIERCE (OKEECHOBEE ROAD/SR 70)  
PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY

Financial Project Identification (FPID) Number: 423374-1



# RECREATIONAL AREAS MAP



Efficient Transportation Decision Making (ETDM) Number: 14295





# FLORIDA'S TURNPIKE (SR 91) WIDENING

FROM JUPITER (INDIANTOWN ROAD/SR 706) TO FT. PIERCE (OKEECHOBEE ROAD/SR 70)

PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY

Financial Project Identification (FPID) Number: 423374-1



# RECREATIONAL AREAS MAP



Efficient Transportation Decision Making (ETDM) Number: 14295





# FLORIDA'S TURNPIKE (SR 91) WIDENING

FROM JUPITER (INDIANTOWN ROAD/SR 706) TO FT. PIERCE (OKEECHOBEE ROAD/SR 70)  
PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY

Financial Project Identification (FPID) Number: 423374-1



# RECREATIONAL AREAS MAP



Efficient Transportation Decision Making (ETDM) Number: 14295





# APPENDIX G

*Public Hearing Transcripts*



## PUBLIC HEARING CERTIFICATION

**Florida's Turnpike (SR 91) Widening**

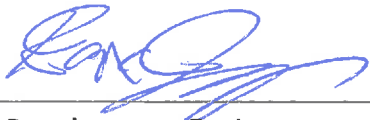
**Project Development and Environment (PD&E) Study**

**from: Jupiter (Indiantown Road/SR 706) to Ft. Pierce (Okeechobee Road/SR 70)**

**Palm Beach, Martin, and St. Lucie Counties, Florida**

**Financial Management No.: 423374-1**

I certify that a public hearing was conducted on July 22, 2021, beginning at 5:30 p.m. for the above project. Transcripts were made, and the documents attached are full, true, and complete transcripts of what was said during the hearing.



\_\_\_\_\_  
Project Development Engineer  
Environmental Management Office  
Florida's Turnpike Enterprise



\_\_\_\_\_  
Date



**MILESTONE | REPORTING COMPANY**

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Fax 407.841.2779

Toll Free 855-MYDEPOS



1 In re: Florida Turnpike Widening Project

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3 PUBLIC MEETING

**CONDENSED**

4 DATE: JULY 22, 2021

5 REPORTER: ELIANETH BRITO

6 PLACE: 2400 SOUTHEAST SALERNO ROAD

7 STUART, FLORIDA 34997

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| 2 | <p>1 INDEX</p> <p>2 Page</p> <p>3 PROCEEDINGS 3</p> <p>4</p> <p>5 EXHIBITS</p> <p>6 (NONE MARKED)</p> <p>7</p> <p>8</p> <p>9</p> <p>10</p> <p>11</p> <p>12</p> <p>13</p> <p>14</p> <p>15</p> <p>16</p> <p>17</p> <p>18</p> <p>19</p> <p>20</p> <p>21</p> <p>22</p> <p>23</p> <p>24</p> <p>25</p>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  | 4 | <p>1 present at your location that have not already been</p> <p>2 identified?</p> <p>3 MR. HOWELL: Brian, there are no other</p> <p>4 officials that have not been recognized.</p> <p>5 MR. RIBARIC: Thanks, Bill. Mark, are there</p> <p>6 any officials present at your location that have not</p> <p>7 already been identified?</p> <p>8 MR. EASLEY: Yes. Frannie Hutchinson, St.</p> <p>9 Lucie County Commissioner, Linda Bartz, St. Lucie</p> <p>10 County Commissioner, Selena Griffett, St. Lucie</p> <p>11 County Public Works Engineering, and Edmund Bas, St.</p> <p>12 Lucie County Public Works Engineering.</p> <p>13 MR. RIBARIC: Thank you, Mark. So now we will</p> <p>14 watch our narrated public hearing presentation.</p> <p>15 PRESENTATION: Welcome to Florida's Turnpike</p> <p>16 Enterprises public hearing for the Turnpike main</p> <p>17 line widening from Jupiter to Fort Pierce Project</p> <p>18 Development and Environment or PD&amp;E Study. We</p> <p>19 appreciate your attendance and participation. If</p> <p>20 you happen to experience technical issues during the</p> <p>21 meeting, please send an e-mail to</p> <p>22 <a href="mailto:tpkmeetingsupport@dot.state.fl.us">tpkmeetingsupport@dot.state.fl.us</a> to report it.</p> <p>23 Turnpike staff will do their best to assist you.</p> <p>24 This hearing is being conducted in a hybrid format</p> <p>25 to provide multiple opportunities for the public to</p>                                                             |
| 3 | <p>1 PROCEEDINGS</p> <p>2 MR. RIBARIC: Good evening, everyone, and</p> <p>3 welcome to the public hearing for the Turnpike Main</p> <p>4 Line Widening PD&amp;E Study. Financial project</p> <p>5 identification number 423375-1. We appreciate your</p> <p>6 attendance and participation. I'd like to formally</p> <p>7 open the public hearing. Today is Thursday, July</p> <p>8 22, 2021 and the time is approximately 6:00. My</p> <p>9 name is Brian Ribaric. I am the Turnpike Project</p> <p>10 Manager with Atkins for this PD&amp;E study and will be</p> <p>11 moderating this public hearing from the Turnpike</p> <p>12 headquarters. Bill Howell, the Consultant Project</p> <p>13 Manager from Lochner, is overseeing the attendees at</p> <p>14 our Stuart location, and Mark Easley from Lochner is</p> <p>15 overseeing the attendees at the Port St. Lucie</p> <p>16 location. We are all being supported by</p> <p>17 representatives of the Turnpike and Lochner staff.</p> <p>18 At this time, we'd like to recognize all Federal,</p> <p>19 State, County or City officials who are present</p> <p>20 tonight. Those that have preregistered were Mayor</p> <p>21 Steven Grant with Boynton Beach, Commissioner Maria</p> <p>22 Marino with Palm Beach County, Commissioner Harold</p> <p>23 Jenkins and Commissioner Edward Ciampi from Martin</p> <p>24 County and Councilman David Pickett from the City of</p> <p>25 Port St. Lucie. Bill, are there any officials</p> | 5 | <p>1 receive information and provide input. This</p> <p>2 approach uses both a virtual and face to face</p> <p>3 component. There is no cost to the public to</p> <p>4 participate in the hearing. We will follow the</p> <p>5 agenda shown on this slide starting first with the</p> <p>6 purpose of the public hearing and how you can</p> <p>7 comment and ask questions. We will then review the</p> <p>8 preferred project build alternative and potential</p> <p>9 environmental effects and open the public comment</p> <p>10 period. The purpose of tonight's public hearing is</p> <p>11 to share information with the public about the</p> <p>12 preferred build alternative, its conceptual design,</p> <p>13 access changes, and the potential social, economic,</p> <p>14 and environmental effects. The public hearing also</p> <p>15 serves as an official forum for members of the</p> <p>16 public to express their opinions regarding the</p> <p>17 project. While comments and questions are accepted</p> <p>18 at any time and must be received or postmarked by</p> <p>19 August 11, 2021 to become part of the study's public</p> <p>20 hearing record. Participation is encouraged and</p> <p>21 solicited without regard to race, color, national</p> <p>22 origin, age, sex, religion, disability, or family</p> <p>23 status. There have been various opportunities for</p> <p>24 the public to provide input on this project. Public</p> <p>25 information meetings held on February 27, 2020 and</p> |





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| 6 | <p>1 March 5, 2020 provided opportunities for public<br/>                 2 input. A total of 155 people attended these<br/>                 3 meetings, and 99 written comments were submitted and<br/>                 4 addressed. In advance of this public hearing,<br/>                 5 interested persons were encouraged to review project<br/>                 6 information and to contact the project manager with<br/>                 7 comments and questions. We continue to encourage<br/>                 8 public input to help us make this important<br/>                 9 decision. You can submit written comments at the<br/>                 10 project website, <a href="http://www.treasurecoastTurnpike.com">www.treasurecoastTurnpike.com</a>. If<br/>                 11 you are at one of the in-person locations, you can<br/>                 12 complete a printed comment form. You can also e-<br/>                 13 mail the project manager directly. And lastly, you<br/>                 14 can mail in your written comments. The project<br/>                 15 manager's contact information will be spelled out<br/>                 16 for the benefit of our listen only participants<br/>                 17 towards the end of the presentation and is also<br/>                 18 available on the hearing notification that you may<br/>                 19 have received by mail. You can also make verbal<br/>                 20 comments. Virtual participants that requested to<br/>                 21 speak when registering will be called upon during<br/>                 22 the public comment period. If attending in person,<br/>                 23 you can fill out the speaker request card to comment<br/>                 24 in the microphone during the public comment period.<br/>                 25 There is also a court reporter at the in person</p> | 8 |
| 7 | <p>1 locations to whom you can provide your comments<br/>                 2 directly. Lastly, you can call the project manager<br/>                 3 at (407) 264-3095 to provide verbal comments during<br/>                 4 normal business hours after the public hearing.<br/>                 5 Every comment carries equal weight. All comments or<br/>                 6 questions will be accepted at any time. Those<br/>                 7 submitted or mailed by August 11, 2021 will be then<br/>                 8 part of the study's public hearing record. We will<br/>                 9 respond to all comments and questions in writing at<br/>                 10 a later date. This public hearing is being recorded<br/>                 11 and a verbatim transcript will be made of all oral<br/>                 12 proceedings. The public hearing video will be<br/>                 13 posted to the project website,<br/>                 14 <a href="http://www.treasurecoastTurnpike.com">www.treasurecoastTurnpike.com</a> in the video section<br/>                 15 by Monday, July 26, 2021. A link to the video will<br/>                 16 also be provided by e-mail to all persons that<br/>                 17 register. This hearing is being held in compliance<br/>                 18 with Title VI, Civil Rights Act of 1964 as amended.<br/>                 19 Public participation at this hearing is encouraged<br/>                 20 and solicited without regard to race, color,<br/>                 21 national origin, age, sex, religion, disability, or<br/>                 22 family status. Persons wishing to express their<br/>                 23 concerns about Title VI may do so by contacting<br/>                 24 either the Florida's Turnpike Enterprise or the<br/>                 25 Tallahassee Offices of the Florida Department of</p>           | 9 |
| 6 | <p>1 Transportation. The contact information is shown<br/>                 2 here and provided on the signs at the entrance to<br/>                 3 this hearing and online in the public hearing<br/>                 4 exhibit room. This public hearing is being held in<br/>                 5 accordance with federal and state regulations that<br/>                 6 govern the project development process to ensure<br/>                 7 adequate opportunity for public input is provided<br/>                 8 including section 339.155 and section 335.199 of the<br/>                 9 Florida Statutes. This public hearing was<br/>                 10 advertised consistent with Chapter 120 of Florida<br/>                 11 Statutes and is being conducted consistent with the<br/>                 12 Americans with Disabilities Act as amended. In<br/>                 13 addition to local government agencies, a study was<br/>                 14 coordinated with stakeholders in the region and<br/>                 15 regulatory and resource agencies. This public<br/>                 16 hearing was advertised in the Florida Administrative<br/>                 17 Register, FDOT's public notices website, Palm Beach<br/>                 18 Post, Stuart's Treasure Coast Newspaper, and Fort<br/>                 19 Pierce Tribune (TC Palm). In addition, adjacent<br/>                 20 property owners, interested individuals, elected and<br/>                 21 appointed officials, Native American tribes and<br/>                 22 government agencies were sent information about<br/>                 23 tonight's public hearing. Project documents are<br/>                 24 available for viewing until August 11, 2021 at the<br/>                 25 Clare and Gladys Wolf High-Technology Center, Indian</p>                                                | 8 |
| 7 | <p>1 River State College, 2400 Southeast Salerno Road,<br/>                 2 Stuart, Florida 34997, phone number (772) 419-5600<br/>                 3 and Havert L. Fenn Center, 2000 Virginia Avenue,<br/>                 4 Fort Pierce, Florida 24962, phone number (772) 462-<br/>                 5 1521 with available hours of Monday through Friday<br/>                 6 from 9:00 a.m. to 5:00 p.m. Project documents are<br/>                 7 also available on the project website at<br/>                 8 <a href="http://www.treasurecoastTurnpike.com">www.treasurecoastTurnpike.com</a>. The Turnpike main<br/>                 9 line widening project is located in Palm Beach,<br/>                 10 Martin, and Saint Lucie counties and extends from<br/>                 11 north of the Indiantown Road or State Road 706<br/>                 12 interchange to north of Okeechobee Road, or State<br/>                 13 Road 70 interchange. Total project length is<br/>                 14 approximately 37 miles. Currently the project<br/>                 15 segment of the Florida's Turnpike consists of a<br/>                 16 four-lane divided roadway, two lanes in each<br/>                 17 direction. There are four existing interchanges<br/>                 18 within the project limits: Southwest Martin Highway<br/>                 19 in Martin County, and Southeast Becker Road.<br/>                 20 Southwest Port Saint Lucie Boulevard, and Okeechobee<br/>                 21 Road in Saint Lucie County. Numerous bridge<br/>                 22 structures are located along the project length<br/>                 23 including crossings of the Loxahatchee River and<br/>                 24 Saint Lucie Canal. The project team identified the<br/>                 25 following deficiencies based on the existing</p>                             | 9 |





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| <p style="text-align: right;">10</p> <p>1 mainline and interchange configurations as well as<br/>                 2 the volume and characteristics of the traffic. High<br/>                 3 truck volumes lead to congestion at interchanges,<br/>                 4 existing low speed ramps degrade operations,<br/>                 5 emergency evacuation is hampered by the existing<br/>                 6 Turnpike lane configuration. Turnpike access for<br/>                 7 existing and proposed freight logistics zones is<br/>                 8 restricted by the low number of interchange points.<br/>                 9 Long term traffic projections show a need for eight<br/>                 10 lanes on the Turnpike. This PD&amp;E study was<br/>                 11 initiated by the Florida's Turnpike Enterprise to<br/>                 12 develop alternatives to meet future mobility needs<br/>                 13 along Florida's Turnpike. The purpose of the<br/>                 14 project is to accommodate future travel demands<br/>                 15 expected along Florida's Turnpike due to increased<br/>                 16 population, freight demands, and employment<br/>                 17 opportunities. We have developed study build<br/>                 18 alternatives to meet the project goals, address<br/>                 19 traffic needs, provide long term mobility, enhance<br/>                 20 evacuation routes, enhance safety, and avoid and<br/>                 21 minimize environmental impacts. Interagency<br/>                 22 collaboration continues as we consider opportunities<br/>                 23 to incorporate emerging transportation technologies<br/>                 24 to further advance safety. Now let's review the<br/>                 25 study's preferred build alternative. The study team</p>      | <p style="text-align: right;">12</p> <p>1 structures that cross over the Turnpike. These<br/>                 2 structures do not accommodate the widened Turnpike<br/>                 3 proposed typical section. The existing overpass at<br/>                 4 West Midway Road is not included in the study as it<br/>                 5 is presently being designed by FDOT District IV and<br/>                 6 Saint Lucie County. This new bridge will<br/>                 7 accommodate the widened Turnpike proposed typical<br/>                 8 section, the placement of all the electronic toll<br/>                 9 gate entries at the interchanges with Crosstown<br/>                 10 Parkway and Midway Road. The existing Florida's<br/>                 11 Turnpike mainline typical section includes four 12<br/>                 12 foot travel lanes, two in each direction, 10 foot<br/>                 13 outside paved shoulders, a 10 foot inside paved<br/>                 14 shoulder northbound, an 8 foot inside paved<br/>                 15 shoulders southbound and a 2 foot wide medium<br/>                 16 barrier. Located to the east of the existing<br/>                 17 roadway is a Florida Gas Transmission Pipeline.<br/>                 18 Proposed improvements to the Turnpike mainline<br/>                 19 include the widening of the roadway from four lanes,<br/>                 20 two in each direction, to eight lanes, four in each<br/>                 21 direction. The proposed typical section of the<br/>                 22 widened roadway includes four 12 foot travel lanes<br/>                 23 in each direction, 12 foot inside and outside paved<br/>                 24 shoulders, and a two foot wide concrete median<br/>                 25 barrier. Widening of the mainline will be to the</p> |
| <p style="text-align: right;">11</p> <p>1 developed and evaluated multiple project build<br/>                 2 alternatives. Engineering and environmental<br/>                 3 evaluations were performed, and local agency and<br/>                 4 public input were solicited. The preferred project<br/>                 5 build alternative is the one that best meets the<br/>                 6 project purpose and need. The project segment of<br/>                 7 Florida's Turnpike was originally constructed in the<br/>                 8 1950s and has been modified over the years to meet<br/>                 9 the changing needs of the area and users. Florida's<br/>                 10 Turnpike is an important route for commerce,<br/>                 11 commuters, and tourists traveling through southeast<br/>                 12 Florida. It is also a major evacuation route during<br/>                 13 emergency events. The preferred build alternative<br/>                 14 includes widening the existing mainline roadway from<br/>                 15 four lanes, two lanes in each direction, to eight<br/>                 16 lanes, four lanes in each direction from north of<br/>                 17 Indian Town Road or State Road 706 to north of<br/>                 18 Okeechobee Road or State Road 70, a distance of<br/>                 19 approximately 37 miles. A proposed mainline<br/>                 20 widening will occur to the west of the existing<br/>                 21 road. Additional improvements include the<br/>                 22 replacement of 13 of the 14 mainline bridges within<br/>                 23 the project area and the widening of one mainline<br/>                 24 bridge, the northbound Thomas B. Manual Bridge over<br/>                 25 the Saint Lucie Canal, the replacement of 15 bridge</p> | <p style="text-align: right;">13</p> <p>1 west of the existing roadway. Also shown is the<br/>                 2 proposed typical section of the Thomas B. Manual<br/>                 3 Bridge over the Saint Lucie Canal. The proposed<br/>                 4 typical section of the replaced southbound bridge<br/>                 5 and northbound widened bridge includes four 12- foot<br/>                 6 travel lanes in each direction, 12-foot inside and<br/>                 7 outside paved shoulders, and one-and one-half foot<br/>                 8 wide concrete barriers on the outside and the inside<br/>                 9 of each bridge. This project also includes<br/>                 10 operational improvements at four existing<br/>                 11 interchanges and the addition of two new<br/>                 12 interchanges along the Turnpike mainline. The<br/>                 13 existing interchanges are located at Southwest<br/>                 14 Martin Highway, or State Road 714, Southeast Becker<br/>                 15 Road, Southwest Port Saint Lucie Boulevard, and<br/>                 16 Okeechobee Road or State Road 70. The new<br/>                 17 interchanges are located at Crosstown Parkway and<br/>                 18 West Midway Road. Operational improvements at each<br/>                 19 of these interchanges are discussed in detail during<br/>                 20 the following slides. Operational improvements<br/>                 21 proposed at the Florida's Turnpike interchange at<br/>                 22 Southwest Martin Highway include modification to<br/>                 23 both the southbound and northbound travel movements.<br/>                 24 The southbound off-ramp will diverge from the<br/>                 25 Turnpike and split into a west and east movement</p>         |





14

1 with the western ramp tying to a proposed roundabout  
 2 at Southwest Layton Farms Avenue which will convey  
 3 traffic northward to Southwest Martin Highway. The  
 4 off-ramps eastern movement will carry traffic over  
 5 the Turnpike, tie into Southwest Martin Highway east  
 6 of the Turnpike. The southbound on ramp starts east  
 7 of the Turnpike and carries traffic over the  
 8 Turnpike. The loop ramp connecting to the Turnpike  
 9 south of Southwest Martin Highway. The northbound  
 10 off-ramp will diverge from the Turnpike and split  
 11 into a west and east movement with the western ramp  
 12 tying to Southwest Martin Highway. This ramp will  
 13 allow for a west and through movement at Southwest  
 14 Martin Highway. The eastern ramp also ties to  
 15 southwest Martin Highway and allows for an east  
 16 through movement at Southwest Martin Highway. The  
 17 northbound on ramp starts east of the Turnpike and  
 18 loops north connecting to the Turnpike north of  
 19 Southwest Martin Highway. Proposed improvements to  
 20 the Southwest Martin Highway Interchange will result  
 21 in roadway access changes. At present, southbound  
 22 traffic exiting the Turnpike connects to Southwest  
 23 Martin Highway at an existing traffic light  
 24 controlled intersection with Southwest Martin Downs  
 25 Boulevard. This intersection allows for east, west,

15

1 and through movements. As discussed previously, the  
 2 proposed southbound off-ramp from the Turnpike will  
 3 split with the right lane connecting to a new  
 4 roundabout at the Southwest Leighton Farms  
 5 intersection with Southwest 39th Street. Traffic  
 6 can then travel north on Southwest Leighton Farms  
 7 Avenue. At its northern end, Southwest Leighton  
 8 Farms Avenue will be realigned to form a new  
 9 intersection with Deggeller Court. This  
 10 intersection will be traffic light controlled and  
 11 allows for east, west, and through movements. The  
 12 left lane of the southbound Turnpike off-ramp  
 13 crosses over the Turnpike and connects the Southwest  
 14 Martin Highway at the Southbound Martin Downs  
 15 Boulevard intersection. This intersection is  
 16 traffic light controlled and will allow for an east  
 17 and through traffic movement. No west turn movement  
 18 will be allowed at this intersection for southbound  
 19 traffic. While modifications to the southbound off-  
 20 ramp are proposed, this ramp's connection to the  
 21 Southwest Martin Downs Boulevard intersection will  
 22 not change. Operational improvements to the  
 23 Turnpike's interchange at Southeast Becker Road will  
 24 be limited primarily to the Turnpike's southbound on  
 25 and off ramps. The southbound off-ramp will be

16

1 shifted slightly to the west to connect the  
 2 Southeast Becker Road at an existing signalized  
 3 intersection. The southbound on-ramp will begin at  
 4 its present location, shift slightly to the west,  
 5 and connect to the Turnpike south of Southeast  
 6 Becker Road. The location of the northbound on and  
 7 off ramps will not change. Additional improvements  
 8 will include the replacement of the Becker Road  
 9 Bridge over the Turnpike to accommodate the proposed  
 10 widening of the Turnpike. Interchange improvements  
 11 at Becker Road will not result in changes to roadway  
 12 access. While the southbound Turnpike off and on  
 13 ramp will be shifted slightly, they will still  
 14 connect at an existing traffic light controlled  
 15 intersection. No modifications to the northbound  
 16 Turnpike off-ramp or on-ramp are proposed at this  
 17 interchange. Operational improvements at the  
 18 Florida's Turnpike interchange at Southwest Port  
 19 Saint Lucie Boulevard will include modifications to  
 20 both the southbound and northbound travel movements.  
 21 The southbound off-ramp will diverge from the  
 22 Turnpike north of Southwest Port Saint Lucie  
 23 Boulevard, loop to the east, and carry traffic over  
 24 the Turnpike to a new intersection with Southwest  
 25 Bayshore Boulevard. Traffic may then travel south

17

1 along Southwest Bayshore Boulevard to Southwest Port  
 2 Saint Lucie Boulevard or north along Southwest  
 3 Bayshore Boulevard to Southwest Thornhill Drive.  
 4 Two new southbound on-ramps will also be  
 5 constructed. For traffic traveling west on  
 6 Southwest Port Saint Lucie Boulevard, the Turnpike  
 7 on-ramp will diverge to the north through a loop  
 8 ramp and connect to the Turnpike south of Southwest  
 9 Port Saint Lucie Boulevard. For traffic traveling  
 10 east on Southwest Port Saint Lucie Boulevard, the  
 11 Turnpike on-ramp will diverge to the south and  
 12 connect to the Turnpike south of Southwest Port  
 13 Saint Lucie Boulevard. The northbound off-ramp will  
 14 diverge from the Turnpike south of Southwest Port  
 15 Saint Lucie Boulevard then turn east and north and  
 16 connect to Southwest Bayshore Boulevard south of  
 17 Southwest Port Saint Lucie Boulevard. The  
 18 northbound on-ramp will extend from the new  
 19 intersection with Southwest Bayshore Boulevard, turn  
 20 northward, and connect to the Turnpike north of  
 21 Southwest Port Saint Lucie Boulevard. Traffic may  
 22 also travel south along Southwest Bayshore Boulevard  
 23 from Southwest Thornhill Drive to connect to this  
 24 on-ramp. Additional improvements will include the  
 25 replacement of the Southwest Port Saint Lucie





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| 18 | <p>1 Boulevard Bridge over the Turnpike to accommodate<br/>                 2 the proposed Turnpike widening. Proposed<br/>                 3 improvements at the Southwest Port Saint Lucie<br/>                 4 Boulevard interchange will result in roadway access<br/>                 5 changes to the Turnpike northbound on-ramp and<br/>                 6 southbound on and off ramps. At present, all on and<br/>                 7 off-ramp movements from the Turnpike connect to<br/>                 8 Southwest Port Saint Lucie Boulevard at an existing<br/>                 9 intersection with Southwest Bayshore Boulevard. The<br/>                 10 proposed southbound Turnpike off-ramp will cross<br/>                 11 over the Turnpike and connect to the Southwest<br/>                 12 Bayshore Boulevard at a new traffic light controlled<br/>                 13 intersection located north of Southwest Port Saint<br/>                 14 Lucie Boulevard. Traffic can then travel south on<br/>                 15 Southwest Bayshore Boulevard to Southwest Port Saint<br/>                 16 Lucie Boulevard. Two new Turnpikes southbound on-<br/>                 17 ramps are also proposed, one for westbound traffic<br/>                 18 and one for eastbound traffic. These on ramps are<br/>                 19 located west of the Turnpike and connect to the<br/>                 20 Turnpike south of the Southwest Port Saint Lucie<br/>                 21 Boulevard. The proposed Turnpike northbound on-ramp<br/>                 22 will begin at the proposed new intersection at<br/>                 23 Southwest Bayshore Boulevard and connect to the<br/>                 24 Turnpike north of Southwest Port Saint Lucie<br/>                 25 Boulevard. A new partial interchange connecting</p>        | 20 |
| 19 | <p>1 Florida's Turnpike to the Crosstown Parkway is<br/>                 2 proposed as part of this project. This interchange<br/>                 3 will include a southbound on-ramp and a northbound<br/>                 4 on-ramp. The southbound on-ramp will connect to the<br/>                 5 Turnpike to Southwest Cameo Boulevard north of<br/>                 6 Crosstown Parkway. This ramp will loop to the east<br/>                 7 and south connecting to the Turnpike south of the<br/>                 8 Crosstown Parkway. The northbound off-ramp will<br/>                 9 begin south of Crosstown Parkway, loop to the west<br/>                 10 and south, connecting the Southwest Cameo Boulevard<br/>                 11 north of Crosstown Parkway. The new proposed<br/>                 12 partial interchange at Crosstown Parkway will result<br/>                 13 in new Turnpike access. At this interchange, a new<br/>                 14 northbound off-ramp and southbound on-ramp will<br/>                 15 connect to Southwest Cameo Boulevard at a new<br/>                 16 proposed roundabout at the Southwest Cameo Boulevard<br/>                 17 intersection with an entrance to the Saint Lucie<br/>                 18 West Centennial High School. Northbound traffic<br/>                 19 exiting the Turnpike can then travel south along<br/>                 20 Southwest Cameo Boulevard to Crosstown Parkway.<br/>                 21 Southbound traffic wishing to enter the Turnpike can<br/>                 22 travel north around Southwest Cameo Boulevard and<br/>                 23 access the Turnpike using the southbound on-ramp. A<br/>                 24 new diamond interchange at West Midway Road is also<br/>                 25 proposed as part of this project. At this</p> | 21 |





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| <p style="text-align: right;">22</p> <p>1 and off-ramps will connect to Okeechobee Road west<br/>                 2 of the Turnpike at a new traffic light controlled<br/>                 3 intersection with Gordy Road. No other roadway<br/>                 4 access changes are proposed at this interchange.<br/>                 5 The no-build alternative is just as it sounds, what<br/>                 6 happens if the Turnpike widening project is not<br/>                 7 built? For this study, the no-build alternative<br/>                 8 assumes projects previously approved in the study<br/>                 9 area would be constructed but no other improvements<br/>                 10 along the Turnpike mainline or at the existing or<br/>                 11 proposed interchanges will be built. The preferred<br/>                 12 build alternative was evaluated in detail to analyze<br/>                 13 potential effects to the social, cultural, natural,<br/>                 14 and physical environment in accordance with state<br/>                 15 and federal regulations. These evaluations are<br/>                 16 documented in the state environmental impact report<br/>                 17 which is available for public review. The<br/>                 18 evaluation of the preferred build alternative shows<br/>                 19 there are potential impacts to the social<br/>                 20 environment involving residential and commercial<br/>                 21 parcel impacts and relocations. No impacts to air<br/>                 22 quality, minimal impact to cultural resources and<br/>                 23 water quality, moderate impacts to wetlands,<br/>                 24 moderate involvement of existing contamination<br/>                 25 sites, and enhancements to areas with special</p>                         | <p style="text-align: right;">24</p> <p>1 If you are being moved and are not satisfied with<br/>                 2 the department's determination of your eligibility<br/>                 3 for payment or the amount of that payment, you may<br/>                 4 appeal that determination. You will be promptly<br/>                 5 furnished necessary forms and notified of the<br/>                 6 procedures to be followed in making an appeal. A<br/>                 7 special word of caution. If you move before you<br/>                 8 receive notification of the relocation benefits that<br/>                 9 you might be entitled to, your benefits may be<br/>                 10 jeopardized. If you'd like more information<br/>                 11 regarding relocation, please contact the project<br/>                 12 manager, Brian Ribaric, after this public hearing.<br/>                 13 Based on background research and field<br/>                 14 investigations, no archeological sites were found<br/>                 15 within the project's archeological area of potential<br/>                 16 effect. Two historic linear resources eligible for<br/>                 17 listing in the National Register of Historic Places<br/>                 18 were found within the project's historic area of<br/>                 19 potential effect. Impacts to these two resources,<br/>                 20 the Saint Lucie Canal and the Florida East Coast<br/>                 21 Railroad Lake Harbor Branch are not anticipated.<br/>                 22 Coordination with the state historic preservation<br/>                 23 office will continue during the project's design<br/>                 24 phase. Parks, trails, and other recreational<br/>                 25 resources are found throughout the project area.</p>                              |
| <p style="text-align: right;">23</p> <p>1 designations. It is possible for protected species<br/>                 2 to occur within the study area, however, no adverse<br/>                 3 impacts are expected. The evaluation matrix shown<br/>                 4 here and on display during the open house and in the<br/>                 5 online public hearing exhibit room summarizes the<br/>                 6 evaluation of the preferred build alternative. One<br/>                 7 of the unavoidable consequences on a project such as<br/>                 8 this is the necessary relocation of residential and<br/>                 9 commercial sites. On this project we anticipate<br/>                 10 three residential relocations and one commercial<br/>                 11 relocation. All right-of-way acquisition will be<br/>                 12 conducted in accordance with Florida Statute 339.09<br/>                 13 and the Federal Uniform Relocation Assistance and<br/>                 14 Real Property Acquisition Policies Act of 1970,<br/>                 15 commonly known as the Uniform Act. If you are<br/>                 16 required to make any type of move as a result of a<br/>                 17 Department of Transportation project, you can expect<br/>                 18 to be treated in a fair and helpful manner and in<br/>                 19 compliance with the Uniform Relocation Assistance<br/>                 20 Act. If a move is required, you will be contacted<br/>                 21 by an appraiser who will inspect your property.<br/>                 22 We'd encourage you to be present during the<br/>                 23 inspection and provide information about the value<br/>                 24 of your property. You may also be eligible for<br/>                 25 relocation advisory services and payment benefits.</p> | <p style="text-align: right;">25</p> <p>1 Key trails include the Loxahatchee River Paddling<br/>                 2 Trail, the Loxahatchee River Management Area<br/>                 3 Multiuse Trail and the Cypress Creek Trail, all of<br/>                 4 which abut or cross the Turnpike in the area of the<br/>                 5 Loxahatchee River. The proposed replacement of the<br/>                 6 Loxahatchee River Bridge will enhance the use of<br/>                 7 these trails by removing bridge piles from the river<br/>                 8 and aligning new piles with the adjacent Interstate<br/>                 9 95 Bridge. The Phipps Park and Boat Ramp is located<br/>                 10 adjacent to the Saint Lucie Canal with the park<br/>                 11 locate to the west of the Turnpike and the boat ramp<br/>                 12 east of the Turnpike. The park and boat ramp are<br/>                 13 connected by an access road under the Turnpike's<br/>                 14 Thomas B. Manual Bridge. The proposed replacement<br/>                 15 of the southbound and widening of the northbound<br/>                 16 Manual bridges will be done to avoid any impacts to<br/>                 17 this park and boat ramp. The City of Port Saint<br/>                 18 Lucie is proposing to construct an adventure park<br/>                 19 within a city owned parcel located west of the<br/>                 20 Turnpike and north of Crosstown Parkway. Ramps<br/>                 21 associated with the proposed Turnpike interchange at<br/>                 22 Crosstown Parkway will bisect this parcel north of<br/>                 23 their connection to Southwest Cameo Boulevard. The<br/>                 24 Turnpike has and will continue to coordinate with<br/>                 25 the City to ensure that the proposed interchange</p> |





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| <p style="text-align: right;">26</p> <p>1 ramps do not significantly impact the design and<br/>                 2 construction of this proposed park. Construction of<br/>                 3 the southbound on-ramp of the proposed Turnpike<br/>                 4 interchange at West Midway Road will result in the<br/>                 5 need for right-of-way from the Winter Lakes<br/>                 6 Neighborhood Park. This required right-of-way will<br/>                 7 not result in impacts to proposed park facilities.<br/>                 8 The Turnpike will continue to coordinate with the<br/>                 9 City of Port Saint Lucie to minimize these impacts<br/>                 10 to the greatest extent possible. The proposed<br/>                 11 improvements will potentially affect an estimated 63<br/>                 12 acres of wetlands and 367 acres of other surface<br/>                 13 waters. The impacted wetlands are located within or<br/>                 14 adjacent to the existing roadway right-of-way and<br/>                 15 were previously disturbed by commercial and<br/>                 16 residential development, roadway construction,<br/>                 17 maintenance activities, and the invasion of nuisance<br/>                 18 and exotic species. The FDOT will mitigate wetland<br/>                 19 impacts resulting from this project's construction<br/>                 20 to meet state and federal requirements. Federal<br/>                 21 listed species may be present or utilized areas<br/>                 22 within the proposed project, but no impacts are<br/>                 23 anticipated. Florida's Turnpike Enterprise will<br/>                 24 continue coordinating with the United States Fish<br/>                 25 and Wildlife Service regarding federal listed</p> | <p style="text-align: right;">28</p> <p>1 designated wild and scenic by the National Parks<br/>                 2 Service. The segment of the Loxahatchee River<br/>                 3 crossed by the Florida's Turnpike is designated<br/>                 4 scenic. To enhance this river segment's unique<br/>                 5 values, the replacement bridge over the river will<br/>                 6 be designed to eliminate bridge piles in the river<br/>                 7 channel, thereby improving river flow and<br/>                 8 recreational use. In addition, stormwater runoff<br/>                 9 will be conveyed off the bridge and adjacent roadway<br/>                 10 and treated in stormwater management facilities<br/>                 11 prior to discharge into the river. The Turnpike<br/>                 12 will continue to coordinate with the National Parks<br/>                 13 Service and State agencies during the project's<br/>                 14 design phase. Results of the contamination<br/>                 15 screening showed that 22 sites ranked medium risk<br/>                 16 and five sites ranked high risk could be potentially<br/>                 17 impacted by the preferred build alternative. These<br/>                 18 sites will be further evaluated during the design<br/>                 19 phase to identify options to avoid, minimize, or<br/>                 20 mitigate contamination involvement. A noise study<br/>                 21 was conducted in accordance with state and federal<br/>                 22 regulations to evaluate traffic noise levels for the<br/>                 23 proposed improvements. If you'd like more<br/>                 24 information regarding traffic noise, a copy of the<br/>                 25 draft noise study report and the Florida's Turnpike</p>     |
| <p style="text-align: right;">27</p> <p>1 species and will conduct additional species<br/>                 2 assessments during future project phases. State<br/>                 3 listed species may also be present or utilized areas<br/>                 4 within the proposed project, but no impacts are<br/>                 5 anticipated. Florida's Turnpike Enterprise will<br/>                 6 continue coordinating with the Florida Fish and<br/>                 7 Wildlife Conservation Commission regarding state<br/>                 8 listed species and will conduct additional species<br/>                 9 assessments during future project phases.<br/>                 10 Approximately 78 acres of impacts are anticipated to<br/>                 11 designated floodplains. However, these impacts are<br/>                 12 negligible when compared to the overall volume of<br/>                 13 floodplains in the area. Compensation methods such<br/>                 14 as floodplain storage ponds will be provided to<br/>                 15 minimize potential impacts. There were three<br/>                 16 federal emergency management agency regulatory<br/>                 17 floodways within the project area, Loxahatchee<br/>                 18 River, Roebuck Creek, and Tenmile Creek. These<br/>                 19 floodways will be bridged to avoid and minimize<br/>                 20 impacts. There is no significant change in flood<br/>                 21 risk as a result of the proposed roadway improvement<br/>                 22 and minimal impacts are expected to the 100-year<br/>                 23 flood plain. The segment of the Loxahatchee River<br/>                 24 crossed by the Florida's Turnpike is one of only two<br/>                 25 river segments within the State of Florida</p>                 | <p style="text-align: right;">29</p> <p>1 Enterprise traffic noise video are available in the<br/>                 2 document section of the project website. The<br/>                 3 preferred build alternative concept plans, also<br/>                 4 available on the project website, show the locations<br/>                 5 of potentially feasible and reasonable noise walls<br/>                 6 along the project corridor. If you have questions,<br/>                 7 you may contact the project manager Brian Ribaric<br/>                 8 after this public hearing during normal business<br/>                 9 hours. The current cost for the preferred build<br/>                 10 alternative for 37 miles of widening and various<br/>                 11 interchange improvements is approximately 1.6<br/>                 12 billion dollars. Included in this estimated cost is<br/>                 13 design, right-of-way acquisition, construction, and<br/>                 14 other services. Based on future traffic<br/>                 15 projections, the design of three project segments<br/>                 16 have been advanced into the State Transportation<br/>                 17 Improvement Program, or STIP. These three project<br/>                 18 segments include the widening of the Turnpike<br/>                 19 mainline from four to eight lanes from Southwest<br/>                 20 Martin Highway or State Road 714 in Martin County to<br/>                 21 Becker Road in Saint Lucie County, a total length of<br/>                 22 approximately four miles and improvements to the<br/>                 23 Turnpike interchange at Southwest Martin Highway.<br/>                 24 These project segments are only funded for design.<br/>                 25 Now that we've described the preferred build</p> |





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1 alternative with potential impacts and benefits,  
 2 let's review the next steps. We continue to  
 3 encourage public input to help us make this  
 4 important decision. You can submit written comments  
 5 at the project website,  
 6 www.treasurecoastTurnpike.com. If you are at one of  
 7 the in-person locations, you can complete a printed  
 8 comment form. You can e-mail the project manager  
 9 directly. For the benefit of our listen-only  
 10 participants, the project manager's contact  
 11 information is Brian Ribaric at Florida's Turnpike  
 12 Enterprise, PO Box 613069, Ocoee, Florida 34761.  
 13 His e-mail address is B-R-I-A-N dot R-I-B-A-R-I-C at  
 14 DOT dot state dot FL dot US. His telephone number  
 15 is (407) 264-3095. While written comments and  
 16 questions are accepted at any time, they must be  
 17 received or postmarked no later than August 11, 2021  
 18 to become part of the public hearing record. The  
 19 next step is to incorporate your input on this  
 20 public hearing into our decision-making process.  
 21 The comment period will close on August 11, 2021 and  
 22 your input will be considered. The PD&E report will  
 23 be sent to the Florida's Turnpike Enterprise and  
 24 will be signed by the Executive Director and Chief  
 25 Executive Officer of Florida's Turnpike Enterprise.

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1 The PD&E study was initiated in the winter of 2017  
 2 and is expected to be complete in the winter of  
 3 2022. The next phase in the process is design.  
 4 Preliminary design is currently funded for three  
 5 project segments. These three project segments  
 6 include the widening of the Turnpike mainline from  
 7 four to eight lanes from Southwest Martin Highway or  
 8 State Road 714 in Martin County to Becker Road in  
 9 Saint Lucie County, a total length of approximately  
 10 four miles and improvements to the Turnpike  
 11 interchange at Southwest Martin Highway. Right-of-  
 12 way and construction are currently not funded for  
 13 any project segment. This project has and will  
 14 continue to comply with all applicable state and  
 15 federal rules and regulations. This concludes the  
 16 presentation. We appreciate your interest in this  
 17 PD&E study. Anyone desiring to make a verbal  
 18 comment regarding the location, conceptual design,  
 19 or social, economic, and environmental effects of  
 20 the improvements will now have an opportunity to do  
 21 so.  
 22 MR. RIBARIC: That concludes the formal  
 23 presentation. We will now begin the public comment  
 24 period. Please note that we will not respond to  
 25 your comments and questions today but will respond

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1 in writing at a later date. Anyone desiring to make  
 2 comments regarding the project will now have an  
 3 opportunity to do so. There are multiple ways for  
 4 you to provide your comments tonight. Everyone on-  
 5 line or who dialed in can submit written comments  
 6 on-line, by mail, or e-mail. If you are in the  
 7 audience tonight at the Stuart or the Port Saint  
 8 Lucie location, you may complete a comment form and  
 9 drop it in the comment box or mail it in after the  
 10 public comment period by August 11, 2021. You may  
 11 also provide your written, verbal -- excuse me. You  
 12 may also provide verbal comments in one of four  
 13 ways. If you are attending either of our in-person  
 14 locations, please complete a speaker request card  
 15 and submit it to a member of the project team.  
 16 Second, if you are on- line, you may provide a  
 17 verbal comment if you requested to speak at  
 18 registration. Third, you may speak directly to the  
 19 court reporter at either of our in-person locations,  
 20 and forth you can call me at (407) 264-3095 during  
 21 normal business hours and after the public hearing  
 22 to provide your verbal comments. Anyone that has  
 23 dialed in using the telephone only option, we are  
 24 unable to unmute you, so if you wish to make a  
 25 comment, please use one of the other options

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1 presented tonight. So we will now call on our  
 2 participants at the Stuart location who have  
 3 requested to speak. As Bill calls your name, please  
 4 step up -- stand up to the microphone, and state  
 5 your name and address for the record. If you're  
 6 representing an organization, municipality or other  
 7 public body, please provide that information as  
 8 well. We ask that you limit your time to three  
 9 minutes. Bill.  
 10 MR. HOWELL: All right. Thank you, Brian. We  
 11 have seven speakers that have registered to speak.  
 12 The first speaker will be Beth Beltran, then  
 13 following her will be Terry Rosenblum and followed  
 14 by Antonia Barns. So Beth, if you would stand right  
 15 over here.  
 16 MS. BELTRAN: Hi, I'm Beth Beltran. I'm the  
 17 Administrator of the Martin Metropolitan Planning  
 18 Organization and our MPO it's also known as and I  
 19 just wanted to point out that the -- this project,  
 20 the widening of the Turnpike to eight lanes and the  
 21 changes to the State Road 714 Interchange are not  
 22 identified in the Martin MPO 2045 long-range  
 23 transportation plan. What is presented is the  
 24 preferred alternative for the State Road 714  
 25 interchange is not the preferred alternative of the





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1 MPO Board. In fact, twice the MPO Board has  
 2 requested of the Turnpike to be presented with all  
 3 of the alternatives considered on State Road 714  
 4 Interchange and to date that has not happened. I'm  
 5 hoping, however, that the Turnpike staff's  
 6 presentations at the August 10th Martin County  
 7 Commission meeting as well as the September 20th MPO  
 8 board meeting will include all of the alternatives  
 9 considered for the State Road 714 interchange as  
 10 requested. Thank you.

11 MR. HOWELL: Thank you, Beth. The next speaker  
 12 will be Terry Rosenblum followed by Antonia Barnes.

13 MR. ROSENBLUM: My name is Terry Rosenblum. I'm  
 14 HOA secretary for Copper Leaf. We have a petition  
 15 which every resident has signed. I've been in  
 16 contact with Hammock Creek and working on Martin  
 17 Downs Community for a sound wall to be complete all  
 18 the way down from Becker Road to 714. I noticed  
 19 that there is a part of the Turnpike that will have  
 20 a sound wall, but this section does not and then  
 21 there are some homes that are impacted, but they  
 22 felt that wasn't a need for it which we all know  
 23 comes down to money, but we all have to live there.  
 24 So I'm requesting that they -- and I've been in  
 25 contact with Toby Overdorf, our House Representative

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1 as well to foresee into this and hopefully we can  
 2 get the wall to go all the way down, we call it 2.5  
 3 which is from Copper Leaf all the way down to Martin  
 4 Highway. So that's all I have to say. Thank you.

5 MR. HOWELL: Thank you, Terry. Now Antonia  
 6 Barnes, I'll call you up and then you'll be followed  
 7 by Lisa Tompson.

8 MS. BARNES: Thank you. Good evening. My name  
 9 is Toni Barnes. I live just west of the Turnpike  
 10 and off of 714. And looking at that interchange  
 11 that they are proposing for redesigning onto 714, it  
 12 appears to me that that's not going to accomplish  
 13 any of the goals that they say they're going to  
 14 accomplish. It's certainly not going to improve  
 15 mobility for those of us that live west of the  
 16 Turnpike off 714 and it's certainly not going to  
 17 accommodate the future demand of traffic. As we all  
 18 know, there is a lot of building going on west in  
 19 Palm City Farms. One of the newest developments is  
 20 Newfield and its proposed traffic increase is going  
 21 to be 45,000 trips a day according to their own  
 22 traffic report. It seems to me that all you're  
 23 doing is splitting which way the traffic is going to  
 24 go, but it is not actually going to increase our  
 25 mobility because it's just going to be more and more

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1 traffic on that little strip of road between Citrus  
 2 Boulevard and the Turnpike. What I think needs to  
 3 be done, and other people may or may not agree with  
 4 me, is that we need to put a direct interchange  
 5 between the Turnpike and I-95. Not all traffic  
 6 comes off of there to just hit those two highways.  
 7 However, a large portion of it does. And if that  
 8 traffic was taken off of our local roads, we would  
 9 certainly then be able to use the off-ramp as it  
 10 currently exists. And they don't need to do this  
 11 very fancy split which frankly I really didn't  
 12 understand, it just moved a little too fast for me.

13 Also, Layton Farms Road is not a major thoroughfare.  
 14 It is a very small rural road, and it would clearly  
 15 change the whole dynamic of that area to start  
 16 putting huge volumes of traffic on there. So my one  
 17 thought is, if you really want to try and alleviate  
 18 our traffic, you need to do a direct interchange  
 19 between the Turnpike and I-95. My other thought was  
 20 that you really, I realize this isn't part of this  
 21 particular study, but you need to put an access ramp  
 22 onto I-95 directly onto Citrus Boulevard. Some of  
 23 the traffic that comes from the north part of Citrus  
 24 Boulevard turns onto 714 to get to I-95. If you had  
 25 a direct access straight down Citrus, you would,

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1 again, alleviate a lot of traffic that goes between  
 2 Citrus Boulevard and the Turnpike. And with all the  
 3 development that's coming up, especially that little  
 4 piece of land between the Turnpike and High Meadows,  
 5 there's going to be a Wawa, there's going to be a  
 6 Tractor Supply, there's going to be an Aldi's,  
 7 there's going to be a shopping center. That's all  
 8 local traffic, but you're going to have all this  
 9 other traffic using our local road when they don't  
 10 really need to. You've put those kind of  
 11 interchanges down in Broward County. I personally  
 12 use them, they're wonderful, you go directly from  
 13 the Turnpike to I-95. Why can't you do the same for  
 14 us? Thank you for listening.

15 MR. HOWELL: Thank you, Antonia, very much for  
 16 your comments. We have Lisa Tompson will be next  
 17 followed by Nancy Urcheck.

18 MS. TOMPSON: My name is Lisa Tompson and I  
 19 live in The Sanctuary at Hammock Creek, and I don't  
 20 have many comments, but I think the most important  
 21 thing I have to say is the question of the quality  
 22 of life that this will effect on many of us as well  
 23 as many other species and plants, insects, et  
 24 cetera. So I would please like to ask that people  
 25 really think about this, not only for the present





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1 but for the future way beyond 2045. And who knows,  
 2 maybe by 2045 we won't need gas stations anymore  
 3 anyway. So why are we thinking about that now as we  
 4 seem to be going in that direction? A sound barrier  
 5 is absolutely required. I would say from all four  
 6 miles, and I would like the sound barrier put in  
 7 before any construction begins. They were doing  
 8 some kind of construction on the Turnpike about six  
 9 months ago and all you could hear at night, when you  
 10 were trying to get to sleep was beep, beep, beep all  
 11 night long from machines going backwards and  
 12 forwards and all around the place, I guess. So if  
 13 we're going to be doing construction at night, then  
 14 we definitely need a sound barrier prior to  
 15 construction, and it should be of the best quality  
 16 and best absorption if there's any such thing that  
 17 exists. The other thing I would like them to  
 18 consider is the type of tarmac or asphalt or  
 19 whatever is it you put under the road, on the road  
 20 itself to reduce the sound if it's possible. I  
 21 mean, I'm sure if they can -- two billionaires can  
 22 go up into space in the space of about two weeks, we  
 23 can figure out some sort of road surfacing material  
 24 that is much more quiet and sound of -- sound  
 25 reduction. And I know there was some comment in

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1 that paper about a golf course. And I played golf  
 2 the other day at Hammock Creek along the bit that  
 3 parallels the Turnpike, and it was extremely loud.  
 4 Whether it's one golfer out there or 5,000 golfers I  
 5 think they were saying in one day or some crazy  
 6 number, the point is that the houses that are there  
 7 are vastly affected, and we need to really, really  
 8 think about the quality of life before we start  
 9 these enormously expensive projects when \$1.6  
 10 billion could probably be used in our school systems  
 11 a lot more than it could be use on our highways.  
 12 Thank you.  
 13 MR. HOWELL: Thank you very much, Nancy. So  
 14 Robert Boyer is next. I'm sorry, that was-- you're  
 15 Nancy. Nancy, and then Robert Boyer is next.  
 16 MS. URCHECK: My name is Nancy Urcheck and I  
 17 live in Palm City in the Hammock Creek development.  
 18 Certainly over the last several years the traffic  
 19 along the Turnpike has increased, the noise levels  
 20 have increased. Recently there was a resurfacing of  
 21 the Turnpike that has dramatically increased the  
 22 noise levels even now so that we find ourselves  
 23 impacted in the use of our back yard, the noise  
 24 levels, it's not quite so relaxing and even at night  
 25 now for the first time I'm hearing the Turnpike

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1 noise lying in bed. The noise level studies show an  
 2 increase of I think eight decibels which I think  
 3 almost doubles the current noise levels. I shudder  
 4 to think how our ability to use our properties and  
 5 even to sleep at night are going to be impacted by  
 6 this project once it's done and the construction, as  
 7 the prior speaker said, while they were resurfacing  
 8 all you could hear was the beeping of the noise at  
 9 night making it very difficult to sleep. The  
 10 construction, I can't imagine the impact on our  
 11 community with the noise from the construction. So  
 12 regardless, those noise barrier walls must stay on  
 13 the plans and, yes, if possible there needs to be  
 14 some way to mitigate the noise during the  
 15 construction period because our community is  
 16 severely impacted. Thank you.  
 17 MR. HOWELL: Thank you very much. Next speaker  
 18 is Robert Boyer, followed by Frank McGann.  
 19 MR. BOYER: Thank you. I'm Bob Boyer, I live  
 20 in Sanctuary also. As some of the previous speakers  
 21 have said, you know, the noise at night has just  
 22 gotten terrible. Whoever the last repave, I know  
 23 it's not part of this, whatever material change you  
 24 made, it drove the Turnpike noise up to unbearable  
 25 levels at times. So you know, let's not go cheap

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1 because we aren't saving any money. The sound  
 2 barrier is a must, as was mentioned. I love the  
 3 direct interchange from I-95 to the Turnpike, it  
 4 serves more than just our needs, it serves anybody  
 5 commuting when there is a traffic jam, they can get  
 6 from one road to another. If they've got to  
 7 evacuate the state, I mean, that's a great thing.  
 8 You know, we're spending a lot of money, \$1.6  
 9 million, that seems in this day and age people might  
 10 not think that's much money, but if inflation takes  
 11 off, watch these projects get killed. Hours of  
 12 construction, as they said, I could hear the grind  
 13 -- I live 1,000 feet away. I wasn't included in the  
 14 mailing, but I could hear the grinding of the  
 15 Turnpike when they were repaving at night. I mean,  
 16 it would wake me up 2:00, 3:00 in the morning. If I  
 17 just woke up a little bit it was there. It was the  
 18 steady beep, beep, beep and the noise and the  
 19 grinding. I mean, I think everything has been said  
 20 so far and I agree with the majority of it, and I  
 21 just hope they listen to the residents of Florida  
 22 and not just the consultants. Thank you.  
 23 MR. HOWELL: Thank you, and Frank.  
 24 MR. MCGANN: Good evening, my name is Frank  
 25 McGann. I'm a resident within the Palm City Farms





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1 area just off 714. I'm very concerned about our  
 2 junction at 714 and what we plan to do there. I  
 3 think we've got something wrong in terms of the  
 4 conduit of traffic that flows across the area. It  
 5 appears that all traffic emerges right on that  
 6 junction in order to get into Palm City, in order to  
 7 get to Stuart, in order to get to the Roosevelt  
 8 Bridge, especially if you're coming south to north  
 9 traffic flow. We also then have the potential of  
 10 people getting off at 714 to head to Okeechobee if  
 11 you're coming down south, so again, that is just a  
 12 major network that's built up in one area and that  
 13 is concerning. Anybody that lives in Palm City, if  
 14 you want to get to Jensen Beach, whatever else,  
 15 you're all hitting that junction area. If you get  
 16 off at 101, get off at 102, and they bring you up on  
 17 Kanner or bring you up on High Meadows again, you're  
 18 hitting 714 and that network on that junction. And  
 19 it questions is that actual junction a major problem  
 20 for the future of our community and I believe it is.  
 21 And therefore maybe part of this project should be  
 22 looking at an extra juncture within our community to  
 23 help create a new conduit. Whether that's the  
 24 extension of Becker, I don't know. I'm not into any  
 25 of these whatsoever else but it also greats a

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1 crossing over the canal of the river at Southport to  
 2 get onto US1, so I think there's a major problem.  
 3 The second part of my question would be, you know,  
 4 I'm involved in things in Port Saint Lucie as well  
 5 and I'm hearing talks of maybe an extra exit that  
 6 may go in at Kings Highway to access the airport,  
 7 that area, and there's maybe talks of another  
 8 junction between Becker and Gatland to help the flow  
 9 of traffic out of traditions and that corridor over  
 10 into the east side of the community there. So  
 11 there's a lot of questions that haven't been touched  
 12 upon or brought up yet in this whole project which  
 13 has me concerned. My biggest concern is the  
 14 flooding issue. We already have a major problem  
 15 within Palm City Farms within the capacity flow of  
 16 the Danforth and Bessie Creek and this falls right  
 17 slap bang in the middle of all this. And that does  
 18 concern me as well, but we're not looking at the  
 19 potential flood issues that may be caused on either  
 20 side of this juncture and that's going to be  
 21 expanded. Thank you.  
 22 MR. HOWELL: Thank you, Frank. Is there anyone  
 23 else desiring to speak that hasn't had an  
 24 opportunity? Okay, Brian, that concludes the  
 25 speakers here at Stuart.

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1 MR. RIBARIC: We will now call upon our  
 2 participants at the Port Saint Lucie location who  
 3 have requested to speak. As Mark calls your name,  
 4 please step up to the microphone and state your name  
 5 and address. If you represent an organization,  
 6 municipality or other public body, please provide  
 7 that information as well. We ask that you limit  
 8 your comment time to three minutes. Mark.  
 9 MR. EASLEY: Yes, we have three speakers. The  
 10 first speaker will be Freddy O'Neil. After Freddy  
 11 O'Neil will be Mark Richardson. Once you come up  
 12 here, please state your name and your address and  
 13 then you can state your comments.  
 14 MR. O'NEIL: Good evening. My name is Freddy  
 15 O'Neil. I live at 702 Southwest Korean Terrace in  
 16 Port Saint Lucie. I moved from West Palm Beach in  
 17 2006 to Port Saint Lucie. I love the community that  
 18 I'm living in now. We have good neighbors, we all  
 19 get along. We have a crime watch in our community  
 20 and we all enjoy being together. Most of my  
 21 questions was answered on the screen about what  
 22 alternate plan that these Turnpikes would go on.  
 23 And I see where it's going to swing around, I see  
 24 alternate A because it's going to swing around to  
 25 the existing Turnpike that we're living, the

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1 Turnpike is right behind our homes. So we won't be  
 2 affected by moving as I can see it on this screen.  
 3 Now, my question is, I don't have a problem with the  
 4 noise or anything like that, but when this extension  
 5 is created, now it's going to move the Turnpike  
 6 closer to our homes and we would like to know what  
 7 type of barrier that would -- and the noise factor  
 8 during that time that it's going to create. And I  
 9 respect the ingress and egress of the Turnpike and  
 10 the extension of the Turnpike for reasons of the  
 11 economy and the environment, because in case of a  
 12 disaster, we need more lanes available to exit the  
 13 state or come into the state. So I have no problem  
 14 with that. But I thank you for understanding our  
 15 situation, the FDOT, for all our homeowners and  
 16 everyone involved. I am here to just be with  
 17 everyone and to enjoy life here in Port Saint Lucie.  
 18 And I thank you all for what you're doing for our  
 19 state and for our community, and I enjoy all our  
 20 residents for their participation. Thank you.  
 21 MR. EASLEY: Mark will be next and following  
 22 Mark will be Gustavo Gonzalez. Mark.  
 23 MR. RICHARDSON: Good evening, everybody. Mark  
 24 Richardson, 1697 Southwest Hampshire Lane, PSL  
 25 Florida -- sorry, Port Saint Lucie, Florida. The



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1 main thing we were worried about along our street  
 2 with our back yard is connected to -- or against the  
 3 Turnpike, were we being eminent domained out or were  
 4 we staying in our house and the road is going to get  
 5 closer? Either way, there's two things that we need  
 6 to know. One was answered that we're going to be  
 7 pretty much staying in our house, which is very  
 8 good. The other thing that I looked at online at  
 9 the noise document that was online. For some reason  
 10 it stopped at section 1500, and it didn't go all the  
 11 way to section 1512. That area there is our whole  
 12 Hampshire Lane area. And there's plenty of room  
 13 there for a wall, but they put a -- they call it a  
 14 shoulder, I call it a hill I guess instead of an  
 15 actual wall. My biggest thing is safety. I don't  
 16 know if you guys did any research as far as  
 17 accidents in that area. Our area right there is next  
 18 to a service plaza. If anybody in this room were to  
 19 go into that service plaza, you're going to come out  
 20 of the service plaza. The next person to you that's  
 21 coming down that road as you're entering the  
 22 Turnpike, automatically you're going to go to the  
 23 right to the outside lane. Well, guess where that  
 24 outside lane is when you're getting to go around  
 25 there? Right there at our property. So not only

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1 that, but there's been police records where I have  
 2 personally called where there's been accidents  
 3 behind us. In my property there has been tires from  
 4 accidents. So not only is it a noise that we're  
 5 looking for, we're looking for safety for not only  
 6 our people but our neighbors as well. There really  
 7 needs to be a person-to-person, property-to-property  
 8 where there is no wall or anything. You guys need to  
 9 come out and say hey, you do need something. And  
 10 number two, back in February and March you guys sent  
 11 out a letter. I know I'm going longer than three  
 12 minutes. We never received anything. So how can we  
 13 say our comments, how can we say our questions if  
 14 nobody's asking us anything? How are we knowing  
 15 that we need to be here? Our attorney down in Miami  
 16 that we chose out of, like, I think five or six  
 17 different attorneys, sent us mail saying, hey, you  
 18 guys are going to be eminent domained. What? So  
 19 we're getting eminent domain letters from attorneys  
 20 around the state. So where is the state telling the  
 21 people? We're right here. That's the main thing.  
 22 Two things, again, we didn't want to move, we're not  
 23 moving. Thank you. But we still want safety.  
 24 MR. EASLEY: Thank you for your comments. We  
 25 need to move on to the next speaker.

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1 MR. RICHARDSON: I understand that and I'm  
 2 sorry, but it's a safety thing that we're talking  
 3 about.  
 4 MR. EASLEY: Gustavo, would you like to speak?  
 5 MR. GONZALEZ: Hi, my name is Gustavo Gonzalez.  
 6 It seems to be I've got my neighbors right beside  
 7 me. I waited until 2006 to build my house in 710  
 8 Southwest Crean Terrace and I thank God because my  
 9 neighbor over here last week about two weeks ago I  
 10 was there with my wife showing the property and  
 11 showing her, look, this is going to be our new home.  
 12 You know, since I already live in Palm Beach and  
 13 then he told me, look, there's going to be a station  
 14 at the turnpike, so all these houses have to go  
 15 away, even your lot. I said it can't be that, no,  
 16 we haven't received nothing. So it seems to be two  
 17 days later I got the letter in the mail. I've been  
 18 living in Florida for almost 43 years, we're still  
 19 going to grow up no matter what, we're going to go  
 20 west, east, south, anywhere. People love Florida.  
 21 Our concern is like everybody here is, whatever  
 22 you're going to build, make sure you build it to be  
 23 comfortable because right now you're going to do  
 24 eight lanes, maybe in ten more years you want to do  
 25 12 lanes or whatever because people are moving to

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1 Florida. There seems to be people likes no more the  
 2 big cities because of whatever problem they've got  
 3 over there. Our concern is like the privacy wall.  
 4 You can do the road, you can do whatever you want  
 5 but I always lived down south in Palm Beach,  
 6 Broward, everywhere, they build those privacy walls  
 7 almost 20 feet high for safety. If I -- my kids are  
 8 playing in the back yard, like my neighbors say, and  
 9 so now you got a piece of rock, a piece of metal  
 10 flying in the air, what's going to happen to one of  
 11 those kids? Because you never know, anything can  
 12 happen today. Our concern is just the safety. If  
 13 you guys are going to spend so much money to build  
 14 something, don't forget about us. We are the ones  
 15 that need all the help, we're the ones that have to  
 16 be safe. There's always money over there to do what  
 17 you want, especially right now, but all I'm going to  
 18 say, build the privacy wall all the way even to the  
 19 ramp. I don't think it's that much money. It might  
 20 even cost you maybe a million dollars more, it may  
 21 cost you \$500,000 more, he told me it might go eight  
 22 feet. Well, don't go eight feet, go 12 feet, go 20  
 23 feet, whatever needs to be done. That's our  
 24 concern. Florida is going to be growing so fast  
 25 that you're never going to believe. With people





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1 living here for a lot of years, they know. I've  
 2 been living here for 40 years, I came all the way  
 3 from Miami all the way up here, I see this growing  
 4 up so fast, we're not going to escape the traffic,  
 5 we're not going to escape the people moving to  
 6 Florida. This is a retirement place, this is a  
 7 sunny place. We get sun here and we get hurricanes.  
 8 If we get hurricanes we can leave. People up north,  
 9 they get fire, they get tornadoes, they can't leave.  
 10 It takes a couple of seconds. Just don't forget  
 11 about us, you know, whoever is doing this project,  
 12 remember, we're the ones that always count on you  
 13 guys. We have to do what you guys say because we  
 14 don't have a choice, but let's do it right, please.  
 15 That's all I have to say. Thank you.  
 16 MR. EASLEY: Is there anyone else that would  
 17 like to speak? Brian, I think that's all.  
 18 MR. RIBARIC: We will now call upon our on-  
 19 line participants who requested to speak at  
 20 registration. When your name is called, you will be  
 21 unmuted. If the microphone on the webinar control  
 22 panel is green, you're ready to make your comment.  
 23 If the microphone icon is red, you will need to  
 24 click on the icon once. It will then turn green and  
 25 notify you that you're unmuted as shown on the go to

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1 webinar control panel to the right. You are then  
 2 ready to speak. Please state your name and address.  
 3 If you represent an organization or municipality or  
 4 other public body, please provide that information  
 5 as well. In an effort to accommodate all speakers,  
 6 we request that you take three minutes for your  
 7 comment. So the first speaker we are going to call  
 8 is Mayor Stephen Grant of Boynton Beach and then on  
 9 deck is Leroy Peterson. All right. Moving on to Mr.  
 10 Leroy Peterson, you are -- please feel free to  
 11 unmute yourself.  
 12 MR. PETERSON: Yes, hello?  
 13 MR. RIBARIC: Go ahead.  
 14 MR. PETERSON: Yeah, my question is brief. I'm  
 15 living 6938 Northwest Baroda Street and I'm on the  
 16 east side of the Turnpike. And I'm not quite sure  
 17 whether we're going to have sound walls up or not,  
 18 but based on everybody else I'm hearing, I think  
 19 that should be preference. Because I'd say about  
 20 two months ago there was a double trailer, 18-  
 21 wheeler had some sort of I guess explosive stuff in  
 22 the trailer and it caught on fire and it exploded.  
 23 And it was like we was up in Cape Cod when the  
 24 shuttle take off. My neighbors left and right of  
 25 me, you know, everybody, you know, came out of the

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1 house thinking it was some sort of major explosion.  
 2 I'm quite sure the expansion and the noise, even if  
 3 it's at the level now, you're talking about doubling  
 4 that level, so I really hope that you consider, you  
 5 know, some barriers in the area and have the  
 6 residents, you know, have the residents at heart in  
 7 mind. And that's it. I appreciate you, the time  
 8 and everything, and thank you.  
 9 MR. RIBARIC: Thank you. So right now we are  
 10 going to have Carolina Williams is next and then  
 11 Vincent Williams is on deck.  
 12 MS. WILLIAMS: Good evening, everybody. My  
 13 name is Carolina Williams and I reside on the 600  
 14 block of Southwest Montana Terrace, and I do share  
 15 similar sentiments as my neighbors in the Hampshire  
 16 Street. And pretty much I wanted to know if we are  
 17 going to be impacted by this project. I noticed  
 18 that some of the trees are already being cut down  
 19 and even the trees right next to my property since  
 20 we are adjacent to Turtle Run Park. And that was my  
 21 concern, yes, of course, the traffic right now, it's  
 22 almost unbearable and I can only imagine how much  
 23 more it's going to be impacted after these  
 24 additional lanes are added. But overall my concern  
 25 is if we are going to be impacted, how soon will I

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1 know in order for our family to take the necessary  
 2 measures if relocation will be needed? And that's  
 3 pretty much all. Thank you for your time.  
 4 MR. RIBARIC: Thank you. We have Vincent  
 5 Williams and then on deck is Robert Johnson. All  
 6 right. So Robert Johnson, you are now -- it's your  
 7 turn and then on deck is Gary Massing.  
 8 MR. JOHNSON: My name is Robert Johnson. I  
 9 live in the Rialto Development located in Jupiter,  
 10 Florida. The development is parallel to I-95 and  
 11 the Florida Turnpike. There are ten lanes of travel  
 12 currently in this area. Five lanes that go north  
 13 and five lanes that go south. The widening of the  
 14 Florida Turnpike will increase the traffic by  
 15 approximately 40 percent in this area. The Rialto  
 16 development is unique where it is right alongside  
 17 both of these major highways. My property value  
 18 will dramatically decrease, and my quality of life  
 19 will decrease. We need a sound barrier wall. The  
 20 noise study report that I read is flawed. The study  
 21 was performed during the pandemic. It was performed  
 22 on August 19, 2020, September 1, 2020, and October  
 23 26, 2020. We all know the traffic volume during  
 24 this timeframe was reduced over 50 percent, so this  
 25 study is flawed. It was also performed during the





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1 week. There was no days where they studied the sound during the weekend where we know there's more traffic. Also, the report doesn't indicate whether the wind was east or west, north or south. We all know living in south Florida, you have an easterly wind. I live on the east side; it's flowing the noise westerly. When I have a westerly wind, the noise increase is over 50 percent, so the report doesn't even indicate which direction the wind was flowing when they performed the testing. I ask that you put up a sound barrier wall, the -- for the entire length of the Rialto development and I appreciate your time. Thank you.

MR. RIBARIC: Thank you. So we have Gary Massing and then I think Vincent Williams has logged in, so he is on deck. All right. Looks like Gary might not be with us tonight, so let's see if Vincent Williams is available now.

MR. WILLIAMS: Good evening, my name is Vincent Williams. I live at 601 Southwest Montana Terrace in the beautiful city of Port Saint Lucie, Florida. My wife, Carolina, already spoke previously regarding some of our concerns that we had with this proposed turnpike expansion project. One of the main concerns I have regarding this project is how

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1 is it going to impact our proposed property taxes? And obviously, if we're going to be incorporating toll lanes on the Turnpike, that's going to be generating more revenue for the people that are utilizing the Turnpike to travel day in and day out. How would that reflect in our property taxes? Is that going to increase our property taxes? Are we going to see a break in our property taxes? Because some of us pay a pretty steep amount in our property taxes here in Port Saint Lucie and the standardized homestead exemption is only three percent. So I hope that our property taxes aren't going to go up anymore, because unfortunately some people will eventually price out of this beautiful community, not to mention the potential devastating effects of the noise and the construction is going to have on the everyday quality of life of people in this community. I hope they incorporate and take into consideration a sound barrier like many of the residents have already stated. I feel that is very necessary for safety and for quality of life. And I hope you guys strongly take that into consideration. And for the people that are in the potential relocation area, I hope that you mail out those letters to those people in a period of time that's

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1 appropriate for them to respond and to look for another home to move into if necessary so that they're not stuck at the last minute as some of these people indicated they had to receive information from their attorneys regarding a potential expansion project of the Turnpike. So I thank you very much for your time, and if necessary I look forward to hearing from you.

MR. RIBARIC: All right. Thank you for your comment. Now we have Ross Aronovitz and on deck will be Jonathan Uhler.

MR. ARONOVITZ: Good evening. Thank you for having me speak tonight. I wanted to thank Commissioner Marino for attending this hearing. I represent the Florida Directors for the Rialto Community in Jupiter, Florida. We're a development of 485 homes. Robert Johnson spoke earlier, and we are very opposed to the current project plan. We, as he had mentioned, reviewed the noise study and our attorney sent you a letter earlier today opposing the current plan in place. We feel that the noise study that was conducted has serious flaws in terms of when the noise study was surveyed during a pandemic when there was reduced traffic. Also, the report misidentified that there is sound

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1 mitigation that covers the entire community of Rialto. The sound mitigation which is a berm and a sound wall that was noted in the report is false. It doesn't cover the entire community, it ends south of Bearing Way. So the results of the report we feel are inaccurate. And we had also reached out to Brian. I've been communicating with Brian since 2019 and the prior meetings we were not given prior notice even though we had asked for notice to be able to attend. This community is very upset with the results that have been shared with us and we are strongly opposed to the project plan. We are seeking a sound barrier for the current project plan that is in place for the entire length of the community of Rialto. We are negatively impacted. As Bob had mentioned, there is two highways that are right next to each other. You're increasing the capacity, you're going to increase the noise, it's going to negatively impact our community, it's going to negatively impact our property values, and we're not in favor of that. And we would really appreciate -- and the other thing is that we have reached out to the Florida Turnpike to ask them if there are other vendors that they use to do these noise surveys. And so far to date in communicating





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1 with Brian and with Doug Zang we were given no -- we  
 2 were not provided with that information even though  
 3 we requested it and I've reached out to other  
 4 Florida Turnpike officials and haven't gotten a  
 5 response so that our community can assess the sound  
 6 study to determine whether or not the results are  
 7 accurate or not. Thank you for your time and we  
 8 strongly are requesting that a sound wall is  
 9 installed for the entire community for the length of  
 10 Rialto.  
 11 MR. RIBARIC: Would you please state your name  
 12 and address for the record? You didn't do that at  
 13 the beginning. Please?  
 14 MR. RANOWITZ: Yeah, my name is Ross Aronovitz,  
 15 and I am one of the board of directors for the  
 16 Rialto Community. My property address is 196  
 17 Bearing Way in Jupiter, Florida.  
 18 MR. RIBARIC: So now we have Jonathan Uhler,  
 19 and then on deck is Karen Monaco.  
 20 MR. UHLER: Hello, this is Jonathan Uhler, 220  
 21 Andrews Harbor Place in Rialto. I'm in the same  
 22 neighborhood as Ross who just spoke and I'll just  
 23 piggyback. We agree with everything he said in  
 24 regards to the noise, the survey, and the extension  
 25 of the wall. Thank you.

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1 MR. RIBARIC: Thank you. So now we have Karen  
 2 Monaco and then on deck is Donna Levensgood.  
 3 MS. MONACO: Hi, my name is Karen Monaco. I  
 4 live in Palm City Farms off of Layton Farms Road.  
 5 My concern is the additional traffic this will bring  
 6 to my neighborhood and my street in particular. So  
 7 my question is, has the issue of additional trucks  
 8 and cars using Southwest Honey Terrace as a cut  
 9 through to Citrus Boulevard been addressed and  
 10 researched? They paved this road two years ago,  
 11 traffic has increased tenfold since it's been paved,  
 12 especially during commute hours. And you know, I  
 13 used to live on a nice, quiet dirt road and now many  
 14 people drive 45 miles an hour down it to use it as a  
 15 shortcut. So my concern is that this is a farm  
 16 community and several of us ride our horses on the  
 17 road on a daily basis, not to mention the children  
 18 that ride their ATVs and bicycles. This cut through  
 19 has also led to several accidents at the  
 20 intersection of Honey Terrace and Citrus Boulevard.  
 21 And now we have several dump trucks that use it as a  
 22 cut through, and there's going to be plenty more  
 23 with all the construction going on on Citrus  
 24 Boulevard. So my concern is that we paid out of our  
 25 pockets, as this is a private road, to have it

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1 paved. And if we're going to have all of these  
 2 heavy trucks coming off this new exit ramp on Layton  
 3 Farms, if they're going to be using our road as a  
 4 cut through, it's going to put more wear and tear on  
 5 our road which will require more maintenance and  
 6 repairs. So one of my concerns is that we're going  
 7 to be expected to continue paying for our road more  
 8 frequently to be fixed due to this increased  
 9 traffic. And then second is are there plans to put  
 10 up signage that this is an equestrian area and to go  
 11 slow for horses and children? There are similar  
 12 communities in Jupiter and Wellington that have  
 13 these signs alerting motorists to slow for the  
 14 horses and children, and I'd like to see some in my  
 15 area to address this problem, as well as additional  
 16 speed limit signs. Anyway, that's my concern.  
 17 Thank you for having us tonight.  
 18 MR. RIBARIC: Thank you. So now we have Donna  
 19 Levensgood and then on deck is Greg Bernard.  
 20 Microphone is on. All right. We'll come back to  
 21 Donna here. So Greg Bernard and then on deck is  
 22 Barry Parker. All right. Greg does not appear to  
 23 be with us tonight. We'll go to Barry Parker. On  
 24 deck is Ryanne Powers- Cavo.  
 25 MR. PARKER: Hello, this is Barry Parker. I

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1 live in Port Saint Lucie at 891 Southwest Grand  
 2 Reserve Boulevard in the Vineyards. My question is,  
 3 with the Crosstown Parkway proposed entrance or  
 4 exit, it's proposing a roundabout on Cameo  
 5 Boulevard. And in that area are two schools, there  
 6 is a K through eight and then there is a high  
 7 school, so the added traffic and congestion in that  
 8 area compounded by two schools is a concern. And  
 9 then the next concern I have is Cashmere Boulevard  
 10 is the next parallel road to Cameo that goes between  
 11 Crosstown and Port Saint Lucie West Boulevard is the  
 12 major north/south. That's proposed to go from a  
 13 two-lane to a four-lane. I would hope that would be  
 14 accomplished before the proposed Turnpike, you know,  
 15 interchange on Crosstown because it's going to  
 16 impact that major north/south road which is Cashmere  
 17 Boulevard which is the back gate of two  
 18 developments. It's my development, the Vineyards of  
 19 192 homes, and Heatherwood which is about 400 homes.  
 20 And that exit onto Cashmere does not have a light.  
 21 And it also comes in part of, you know, it comes out  
 22 onto Cashmere where there's McChesney's Park and  
 23 then the two schools. And during school period,  
 24 it's very, very congested, and been involved with  
 25 accidents. And so my concern is thinking, you know,





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1 to Cashmere, and then that roundabout on Cameo with  
 2 two schools and then that proposed park which is a  
 3 very impactful -- another one for traffic, so those  
 4 are my concerns. Thank you.  
 5 MR. RIBARIC: Thank you. So next is Ryanne  
 6 Powers-Cavo and then on deck is Maria Wharton. All  
 7 right. We'll come back to Ryanne. So Maria Wharton  
 8 and then on deck is Cheryl Carneke. All right.  
 9 Seems like Maria is not with us tonight either.  
 10 Cheryl Kernecki and then Lana Shah is on deck. All  
 11 right. Lana Shah is now Thomas Kott and then seems  
 12 like he's not on-line with us tonight. Gregory  
 13 Gryczan? Is Clinton Harris? Gregory, please provide  
 14 your comment. You look, you're green on our side.  
 15 All right. We'll come back and try one more time.  
 16 Now we'll move onto Clinton Harris and then on deck  
 17 is John Vogt. All right. Seems Clinton is not with  
 18 us, so now we'll move on to John Vogt. They don't  
 19 seem to be with us tonight either. Jackson Hurst?  
 20 Moving on. Lilliann Alston doesn't seem to be with  
 21 us, and then Brian Akrami. All right. It seems  
 22 like we have gone through the list of those that  
 23 have preregistered. Let's jump back to see if I  
 24 think Donna Levengood was on-line, maybe she had a  
 25 moment. We're going to go ahead and unmute you,

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1 Donna, and see if you are able to provide your  
 2 comment now.  
 3 MS. LEVENGOOD: This is Donna Levengood. Are  
 4 you able to hear me?  
 5 MR. RIBARIC: Yes, we can. Thank you.  
 6 MS. LEVENGOOD: Hi. Thank you so much. I'm a  
 7 resident in Highlands Resort in Palm City off of  
 8 High Meadows which essentially serves as a service  
 9 road for I-95 and also connects a lot of people that  
 10 jump from I- 95 to the Turnpike. My concern is the  
 11 noise and whether there will be noise abatement for  
 12 the residents in Highlands Reserve as well as  
 13 Hammock Creek. I understand that the cost is  
 14 reasonable, and it was warranted based on the  
 15 studies but that there is a gas pipeline that might  
 16 prevent noise abatement. Can you speak to that, or  
 17 am I simply asking that it be included? I -- we  
 18 need that. And then the second thing is, the exit  
 19 at Kanner Highway and State Road 76 for the Turnpike  
 20 would greatly help diminish the amount of traffic  
 21 volume on High Meadows and traveling between the  
 22 exit at Martin Highway to go south again. And so I  
 23 would request that that access be strongly  
 24 considered at Kanner Highway. Thank you.  
 25 MR. RIBARIC: All right. I think we had a few

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1 other people that had registered previously that  
 2 have joined us. I will try this name. Idelfonso  
 3 Vaquero? You are unmuted on our side and you're  
 4 showing that you have been self-muted, so check your  
 5 audio settings to see if the microphone icon should  
 6 be green on the go to webinar. All right. And I  
 7 think we had Gregory Gryczan join us. Gregory, you  
 8 are unmuted, so check your setting to see if your  
 9 microphone is green and then provide your comment.  
 10 All right. There were also several other folks that  
 11 had registered online to be in person and provide  
 12 comment. I want to make sure that we give those  
 13 folks an opportunity to provide feedback at our in-  
 14 person locations. I will list a few folks and we'll  
 15 get with Mark and Bill to see if they are available.  
 16 Luis Valdez, Marylou Barrett and John Singleton.  
 17 Bill or Mark?  
 18 MR. HOWELL: Brian, there's no one here  
 19 anymore.  
 20 MR. RIBARIC: So some of the other in-person  
 21 folks were Marshall Winslow, Jennifer Whiting, and  
 22 Joy Puerta. Are those folks at the in-person venue?  
 23 MR. HOWELL: They're not in Stuart.  
 24 MR. EASLEY: There's no one here, everyone's  
 25 left.

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1 MR. RIBARIC: Thank you. I believe we had  
 2 Marylou Barrett join us online, so Marylou, we will  
 3 go ahead and unmute you. Please provide your  
 4 comment.  
 5 MS. BARRETT: Hi. I live off of Ann Arbor  
 6 which runs parallel to the Turnpike. And the noise  
 7 on a normal day is pretty deafening. So looking at  
 8 the plan, the noise barrier is not coming down this  
 9 far. There is nothing to abate the noise. Of  
 10 course, my concern is our property values, not being  
 11 able to enjoy our pool and our back yard any time of  
 12 the year because of the noise. Anything else? I  
 13 guess that's going to be it. Thank you.  
 14 MR. RIBARIC: Thank you. Thank you for your  
 15 comment. All right. I think we're ready to move on  
 16 and finish, close out the public hearing here. All  
 17 right. Bill, one last time, is there anybody at your  
 18 location that would like to -- that wishes to speak  
 19 tonight? If so, let's have them fill out a speaker  
 20 request card and provide it to one of our members  
 21 and let -- have them speak at this time. Bill.  
 22 MR. HOWELL: Brian, there are no further  
 23 speakers that wish to speak here in Stuart.  
 24 MR. RIBARIC: Thank you. Mark, is there anyone  
 25 at your location that wishes to speak? If so, have





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1 them fill out a speaker card and provide it to the  
 2 team and allow them to provide their comment.  
 3 MR. EASLEY: There are no other speakers,  
 4 Brian.  
 5 MR. RIBARIC: Thank you, Mark. Thank you for  
 6 your interest in this project and for taking the  
 7 time to attend this public hearing for the Turnpike  
 8 widening -- mainline widening of the PD&E study.  
 9 Again, Florida Turnpike Enterprise invites your  
 10 comments, so please use one of our four options on  
 11 the screen to submit your comments to me, the  
 12 project manager, Brian Ribaric. The comment period  
 13 will remain open for this public hearing through  
 14 August 11, 2021. If you provided an e-mail address  
 15 at registration, you will receive a follow up e-  
 16 mail within the next three business days with a link  
 17 to the recording of today's proceedings. The  
 18 recording link will also be posted on our project  
 19 website. When you exit the webinar, you will  
 20 receive a brief survey. Your participation will help  
 21 us for future public hearings. It is now 7:37. I  
 22 hereby officially close the public hearing for the  
 23 turnpike mainline widening PD&E study. On behalf of  
 24 Florida's Turnpike Enterprise, we thank you for  
 25 joining us tonight and take care. Good night.

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1 (PUBLIC MEETING CONCLUDED AT 7:37 P.M.)  
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1 CERTIFICATE  
 2  
 3 STATE OF FLORIDA)  
 4 COUNTY OF ORANGE)  
 5  
 6 I, ELIANETH BRITO, Court Reporter and Notary Public  
 7 for the State of Florida at Large, do hereby certify  
 8 that I was authorized to and did report the foregoing  
 9 proceeding, and that said transcript is a true record of  
 10 the said proceeding.  
 11  
 12 I FURTHER CERTIFY that I am not of counsel for,  
 13 related to, or employed by any of the parties or  
 14 attorneys involved herein, nor am I financially  
 15 interested in said action.  
 16  
 17 Submitted on: November 22, 2021.  
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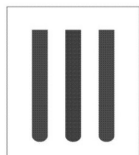


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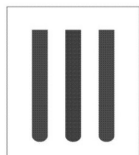


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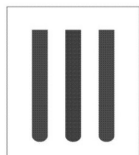
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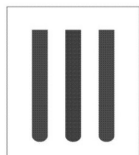


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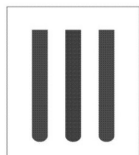


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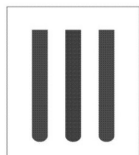


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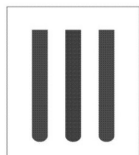


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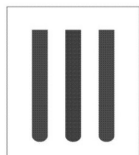


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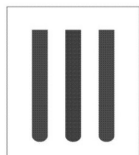


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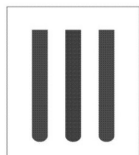


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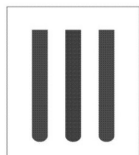


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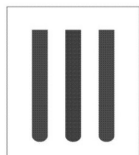


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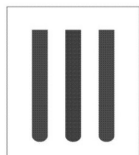


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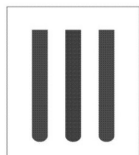


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Turnpike Mainline Widening PD&E Study

from Jupiter (Indiantown Road/SR706) to Fort Pierce  
(Okeechobee Road/SR70)  
Palm Beach, Martin, and St. Lucie Counties, Florida  
FPID 423374-1-22-01

PUBLIC HEARING  
(Live and Virtual Meeting)

Thursday, July 22, 2021  
at  
Clare & Gladys Wolf High - Technology Center  
(Stuart)  
Susan H. Johnson Auditorium  
Indian River State College  
2400 S.E. Salerno Road  
Stuart, Florida 34997

&

Schreiber Conference Center  
(St. Lucie West)  
Communities Auditorium  
Indian River State College  
500 NW California Boulevard  
Port St. Lucie, Florida 34986

TRANSCRIPT PREPARED BY:

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Fort Pierce, FL 34950  
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1 ---

2 MR. RIBARIC: Good evening everyone and

3 welcome to the public hearing for the Turnpike

4 Mainline Widening PD&E Study. Financial Project

5 Identification No. 423374-1.

6 We appreciate your attendance and

7 participation. I would like to formally open the

8 public hearing.

9 Today is Thursday, July 22, 2021 and the time

10 is approximately six o'clock.

11 My name is Brian Ribaric. I am the Turnpike

12 project manager with Atkins for this PD&E study and

13 we'll be moderating this public hearing from the

14 Turnpike Headquarters.

15 Bill Howell, the consultant project manager

16 from Lochner is overseeing the attendees at our

17 Stuart location. And Mark Easley from Lochner is

18 overseeing the attendees at the Port St. Lucie

19 location. We are all being supported by

20 representatives of the Turnpike and Lochner staff.

21 At this time, we would like to recognize all

22 federal, state, county or city officials who are

23 present tonight. Those that have pre-registered

24 are Mayor Steven Grant of Boynton Beach;

25 Commissioner Maria Marino with Palm Beach County;

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1 Commissioner Harold Jenkins and Commissioner Edward

2 Ciampi from Martin County; and Councilman David

3 Pickett from the City of Port St. Lucie.

4 Bill are there any officials present at your

5 location that have not already been identified?

6 MR. HOWELL: There are no other officials that

7 have not been recognized.

8 MR. RIBARIC: Thanks, Bill. Mark, are there

9 any officials present at your location that have

10 not already been identified?

11 MR. EASLEY: Yes. Frannie Hutchinson,

12 St. Lucie County Commissioner; Linda Bartz,

13 St. Lucie County Commissioner; Selena Griffett,

14 St. Lucie County Public Works Engineering; and

15 Edward Bass, St. Lucie County Public Works

16 Engineering.

17 MR. RIBARIC: Thank you, Mark. So now we will

18 watch our narrated public hearing presentation.

19 (The following is the narrated presentation.)

20 Welcome to Florida's Turnpike Enterprises

21 public hearing for the Turnpike Mainline Widening

22 from Jupiter to Fort Pierce Project Development and

23 Environment, or PD&E, study. We appreciate your

24 attendance and participation.

25 If you happen to experience technical issues

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1 during the meeting, please send an email to

2 Tpkmeetingsupport@dot.state.fl.us to report it.

3 Turnpike staff will do their best to assist you.

4 This hearing is being conducted in a hybrid

5 format to provide multiple opportunities for the

6 public to receive information and provide input.

7 This approach uses both a virtual and a

8 face-to-face component. There is no cost to the

9 public to participate in the hearing.

10 We will follow the agenda shown on this slide,

11 starting first with the purpose of the public

12 hearing and how you can comment and ask questions.

13 We will then review the preferred project build

14 alternative and potential project environmental

15 effects and open the public comment period.

16 The purpose of tonight's public hearing is to

17 share information with the public about the

18 preferred build alternative; it's conceptual

19 design; access changes; and the potential social,

20 economic, and environmental effects. The public

21 hearing also serves as an official forum for

22 members of the public to express their opinions

23 regarding the project. While comments and

24 questions are accepted at any time, they must be

25 received or postmarked by August 11, 2021 to

Page 5

1 become part of the study's public hearing record.

2 Public participation is encouraged and solicited

3 without regard to race, color, national origin,

4 age, sex, religion, disability or family status.

5 There have been various opportunities for the

6 public to provide input on this project. Public

7 information meetings, held on February 27, 2020 and

8 March 5, 2020, provided opportunities for public

9 input. A total of 155 people attended these

10 meetings, and 99 written comments were submitted

11 and addressed. In advance of this public hearing,

12 interested persons were encouraged to review

13 project information and to contact the project

14 manager with comments and questions.

15 We continue to encourage public input to help

16 us make this important decision. You can submit

17 written comments at the project website,

18 www.treasurecoastturnpike.com. If you are at one

19 of the in-person locations, you can complete a

20 printed comment form. You can also email the

21 project manager directly. And lastly, you can mail

22 in your written comments. The project manager's

23 contact information will be spelled out for the

24 benefit of our listen-only participants towards the

25 end of the presentation. It is also available on



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1 the hearing notification that you may have received  
2 by mail.

3 You can also make verbal comments. Virtual  
4 participants that requested to speak when  
5 registering will be called upon during the public  
6 comment period. If attending in person, you can  
7 fill out a Speaker Request Card to comment at the  
8 microphone during the public comment period. There  
9 is also a court reporter at the in-person locations  
10 to whom you can provide your comments directly.  
11 Lastly, you can call the project manager at  
12 (407)264-3095 to provide verbal comments during  
13 normal business hours after the public hearing.

14 Every comment method carries equal weight.  
15 While comments and questions will be accepted at  
16 any time, those submitted or mailed by  
17 August 11, 2021 will become part of the study's  
18 public hearing record. We will respond to all  
19 comments and questions in writing at a later date.  
20 This public hearing is being recorded and a  
21 verbatim transcript will be made of all oral  
22 proceedings. The public hearing video will be  
23 posted to the project website,  
24 [www.treasurecoastturnpike.com](http://www.treasurecoastturnpike.com) in the video section  
25 by Monday July 26, 2021. A link to the video will

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1 also be provided by email to all persons that  
2 registered.

3 This hearing is being held in compliance with  
4 Title 6 of the Civil Rights Act of 1964 as amended.  
5 Public participation at this hearing is encouraged  
6 and solicited without regard to race, color,  
7 national origin, age, sex, religion, disability, or  
8 family status. Persons wishing to express their  
9 concerns about Title 6 may do so by contacting  
10 either the Florida's Turnpike Enterprise or the  
11 Tallahassee office of the Florida Department of  
12 Transportation. The contact information is shown  
13 here and provided on a sign display at the entrance  
14 to this hearing and online in the public hearing  
15 exhibit room.

16 This public hearing is being held in  
17 accordance with federal and state regulations that  
18 govern the project development process to ensure  
19 adequate opportunity for public input is provided,  
20 including Section 339.155 and Section 335.199 of  
21 the Florida Statutes. This public hearing was  
22 advertised consistent with Chapter 120 of Florida  
23 Statutes and is being conducted consistent with the  
24 Americans with Disabilities Act as amended.  
25 In addition to local government agencies, the

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1 study was coordinated with stakeholders in the  
2 region and regulatory and resource agencies.

3 This public hearing was advertised in the  
4 Florida Administrative Register, FDOT's Public  
5 Notices website, Palm Beach Post, Stuart's Treasure  
6 Coast Newspaper, and the Fort Pierce Tribune, the  
7 TC Palm. In addition, adjacent property owners,  
8 interested individuals, elected and appointed  
9 officials, Native American Tribes and government  
10 agencies were sent information about tonight's  
11 public hearing.

12 Project documents are available for viewing  
13 until August 11, 2021 at the Clare & Gladys Wolf  
14 High-Technology Center, Indian River State College,  
15 2400 Southeast Salerno Road, Stuart, Florida 34997,  
16 phone number (772)419-5600 and Havert L. Fenn  
17 Center, 2000 Virginia Avenue, Fort Pierce, Florida  
18 34982, phone number 772(462)-1521, with available  
19 hours of Monday through Friday, from 9:00 a.m. to  
20 5:00 p.m. Project documents are also available on  
21 the project website a  
22 [www.treasurecoastturnpike.com](http://www.treasurecoastturnpike.com).

23 The Turnpike Mainline Widening project is  
24 located in Palm Beach, Martin and St. Lucie  
25 Counties, and extends from north of the Indiantown

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1 Road or State Road 706 interchange to north of  
2 Okeechobee Road or State Road 70 interchange. The  
3 total project length is approximately 37 miles.

4 Currently, the project segment of the  
5 Florida's Turnpike consists of a four-lane divided  
6 roadway, two lanes in each direction. There are  
7 four existing interchanges within the project  
8 limits, Southwest Martin Highway in Martin County,  
9 and Southeast Becker Road, Southwest Port St. Lucie  
10 Boulevard and Okeechobee Road in St. Lucie County.  
11 Numerous bridge structures are located along the  
12 project length, including crossings of the  
13 Loxahatchee River and St. Lucie Canal.

14 The project team identified the following  
15 deficiencies based on the existing mainline and  
16 interchange configurations, as well as the volume  
17 and characteristics of the traffic.

18 High truck volume leads to congestion at  
19 intersections.

20 Existing low speed ramps degrade operations.  
21 Emergency evacuation is hampered by the  
22 existing Turnpike lane configuration.

23 Turnpike access for existing and proposed  
24 Freight Logistic Zones is restricted by the low  
25 number of interchange points.



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1 Long-term traffic projections show a need for  
 2 eight lanes on the Turnpike.  
 3 This PD&E Study was initiated by the Florida's  
 4 Turnpike Enterprise to develop alternatives to meet  
 5 future mobility needs along Florida's Turnpike.  
 6 The purpose of the project is to accommodate future  
 7 travel demands expected along Florida's Turnpike  
 8 due to increased population, freight demands, and  
 9 employment opportunities. We have developed study  
 10 build alternatives to meet the project goals,  
 11 address traffic needs, provide long-term mobility,  
 12 enhance evacuation routes, enhance safety, and  
 13 avoid and minimize environmental impacts.  
 14 Interagency collaboration continues as we consider  
 15 opportunities to incorporate emerging  
 16 transportation technologies to further advance  
 17 safety.  
 18 Now, let's review the study's preferred build  
 19 alternative:  
 20 The study team developed and evaluated  
 21 multiple project build alternatives. Engineering  
 22 and environmental evaluations were performed, and  
 23 local agency and public input were solicited. The  
 24 preferred project build alternative is the one that  
 25 best meets the project purpose and need.

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1 The project segment of Florida's Turnpike was  
 2 originally constructed in the 1950s and has been  
 3 modified over the years to meet the changing needs  
 4 of the area and users. Florida's Turnpike is an  
 5 important route for commerce, commuters and  
 6 tourists traveling through Southeast Florida. It  
 7 is also a major evacuation route during emergency  
 8 events.  
 9 The preferred build alternative includes:  
 10 Widening the existing mainline roadway from  
 11 four lanes (two lanes in each direction) to eight  
 12 lanes (four lanes in each direction) from north of  
 13 Indiantown Road or State Road 706 to north of  
 14 Okeechobee Road or State Road 70, a distance of  
 15 approximately 37 miles. The proposed mainline  
 16 widening will occur to the west of the existing  
 17 road.  
 18 Additional improvements include:  
 19 The replacement of 13 of the 14 mainline  
 20 bridges within the project area, and the widening  
 21 of one mainline bridge, the northbound  
 22 Thomas B. Manuel Bridge over the St. Lucie Canal.  
 23 The replacement of 15 bridge structures that  
 24 cross over the Turnpike. These structures do not  
 25 accommodate the widened Turnpike proposed typical

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1 section. The existing overpass at West Midway Road  
 2 is not included in this study, as it is presently  
 3 being designed by FDOT District 4 and  
 4 St. Lucie County. This new bridge will accommodate  
 5 the widened Turnpike proposed typical section.  
 6 The placement of all electronic toll gantries  
 7 at the new interchanges with Crosstown Parkway and  
 8 Midway Road.  
 9 The existing Florida's Turnpike mainline  
 10 typical section includes:  
 11 Four 12-foot travel lanes (two in each  
 12 direction), ten-foot outside paved shoulders, a  
 13 ten-foot inside paved shoulder northbound, an  
 14 eight-foot inside paved shoulder southbound, and a  
 15 two-foot-wide median barrier.  
 16 Located to the east of the existing roadway is  
 17 a Florida Gas Transmission pipeline.  
 18 Proposed improvements to the Turnpike mainline  
 19 include the widening of the roadway from four lanes  
 20 (two in each direction) to eight lanes (four in  
 21 each direction). The proposed typical section of  
 22 the widened roadway includes:  
 23 Four 12-foot travel lanes in each direction.  
 24 twelve-foot inside and outside paved shoulders, and  
 25 a two-foot-wide concrete median barrier.

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1 Widening of the mainline will be to the west  
 2 of the existing roadway.  
 3 Also shown is the proposed typical section of  
 4 the Thomas B. Manuel Bridge over the  
 5 St. Lucie Canal. The proposed typical section of  
 6 the replaced southbound bridge and northbound  
 7 widened bridge includes:  
 8 Four 12-foot travel lanes in each direction,  
 9 twelve-foot inside and outside paved shoulders, and  
 10 one and one-half-foot wide concrete barriers on the  
 11 outside and inside of each bridge.  
 12 This project also includes operational  
 13 improvements at four existing interchanges and the  
 14 addition of two new interchanges along the Turnpike  
 15 mainline.  
 16 The existing interchanges are located at  
 17 Southwest Martin Highway or State Road 714,  
 18 Southeast Becker Road, Southwest Port St. Lucie  
 19 Boulevard, and Okeechobee Road or State Road 70.  
 20 The new interchanges are located at:  
 21 Crosstown Parkway, and West Midway Road.  
 22 Operational improvements at each of these  
 23 interchanges are discussed in detail during the  
 24 following slides.  
 25 Operational improvements proposed at the



1 Florida's Turnpike interchange at Southwest Martin  
 2 Highway include modification to both the southbound  
 3 and northbound travel movements. The southbound  
 4 off-ramp will diverge from the Turnpike and split  
 5 into a west and east movement with the western ramp  
 6 tying to a proposed roundabout at  
 7 Southwest Leighton Farms Avenue, which will convey  
 8 traffic northward to Southwest Martin Highway. The  
 9 off-ramp's eastern movement will carry traffic over  
 10 the turnpike, tying to Southwest Martin Highway  
 11 east of the Turnpike. The southbound on-ramp  
 12 starts east of the Turnpike and carries traffic  
 13 over the Turnpike, through a loop ramp, connecting  
 14 to the Turnpike south of Southwest Martin Highway.  
 15 The northbound off-ramp will diverge from the  
 16 Turnpike and split into a west and east movement  
 17 with the western ramp tying to Southwest Martin  
 18 Highway. This ramp will allow for a west and  
 19 through movement at Southwest Martin Highway. The  
 20 eastern ramp also ties to Southwest Martin Highway  
 21 and allows for an east and through movement at  
 22 Southwest Martin Highway. The northbound on-ramp  
 23 starts east of the turnpike and loops north,  
 24 connecting to the Turnpike north of Southwest  
 25 Martin Highway.

1 allowed at this intersection for southbound  
 2 traffic.  
 3 While modifications to the northbound off-ramp  
 4 are proposed, this ramp's connection to the  
 5 Southwest Martin Downs Boulevard intersection will  
 6 not change.  
 7 Operational improvements to the Turnpike's  
 8 interchange at Southeast Becker Road will be  
 9 limited primarily to the Turnpike's southbound on-  
 10 and off-ramps. The southbound off-ramp will be  
 11 shifted slightly to the west to connect to  
 12 Southeast Becker Road at an existing signalized  
 13 intersection. The southbound on-ramp will begin at  
 14 its present location, shift slightly to the west,  
 15 and connect to the Turnpike south of  
 16 Southeast Becker Road. The location of the  
 17 northbound on- and off-ramps will not change.  
 18 Additional improvements will include the  
 19 replacement of the Becker Road Bridge over the  
 20 Turnpike to accommodate the proposed widening of  
 21 the Turnpike.  
 22 Interchange improvements at Beck Road will not  
 23 result in changes to roadway access. While the  
 24 southbound Turnpike off-ramp and on-ramp will be  
 25 shifted slightly, they will still connect at an

1 Proposed improvements to the Southwest Martin  
 2 Highway interchange will result in roadway access  
 3 changes. At present, southbound traffic exiting  
 4 the Turnpike connects to Southwest Martin Highway  
 5 at an existing traffic light controlled  
 6 intersection with Southwest Martin Downs Boulevard.  
 7 This intersection allows for east, west, and  
 8 through movements.  
 9 As discussed previously, the proposed  
 10 southbound off-ramp from the Turnpike will split,  
 11 with the right lane connecting to a new roundabout  
 12 at the Southwest Leighton Farms Avenue intersection  
 13 with Southwest 39th Street. Traffic can then  
 14 travel north on Southwest Leighton Farms Avenue.  
 15 At its northern end, Southeast Leighton Farms  
 16 Avenue will be realigned to form a new intersection  
 17 with Deggeller Court. This intersection will be  
 18 traffic light controlled and allows for east, west  
 19 and through movements. The left lane of the  
 20 southbound Turnpike off-ramp crosses over the  
 21 Turnpike and connects to Southwest Martin Highway  
 22 at the Southwest Martin Downs Boulevard  
 23 intersection. This intersection is traffic light  
 24 controlled and will allow for an east and through  
 25 traffic movement. No west turn movement will be

1 existing traffic light controlled intersection. No  
 2 modifications to the northbound Turnpike off-ramp  
 3 or on-ramp are proposed at this interchange.  
 4 Operational improvements at the Florida's  
 5 Turnpike interchange at Southwest Port St. Lucie  
 6 Boulevard will include modifications to both the  
 7 southbound and northbound travel movements. The  
 8 southbound off-ramp will diverge from the Turnpike  
 9 north of Southwest Port St. Lucie Boulevard, loop  
 10 to the east and carry traffic over the Turnpike to  
 11 a new intersection with Southwest Bayshore  
 12 Boulevard. Traffic may then travel south along  
 13 Southwest Bayshore Boulevard to Southwest  
 14 Port St. Lucie Boulevard or north along  
 15 Southwest Bayshore Boulevard to Southwest Thornhill  
 16 Drive. Two new southbound on-ramps will also be  
 17 constructed. For traffic traveling west on  
 18 Southwest Port St. Lucie Boulevard, the Turnpike  
 19 on-ramp will diverge to the north, through a loop  
 20 ramp, and connect to the Turnpike south of  
 21 Southwest Port St. Lucie Boulevard. For traffic  
 22 traveling east on Southwest Port St. Lucie  
 23 Boulevard, the Turnpike on-ramp will diverge to the  
 24 south and connect to the Turnpike south of  
 25 Southwest Port St. Lucie Boulevard.



1 The northbound off-ramp will diverge from the  
 2 Turnpike south of Southwest Port St. Lucie  
 3 Boulevard, then turn east and north, and connect to  
 4 Southwest Bayshore Boulevard, south of Southwest  
 5 Port St. Lucie Boulevard. The northbound on-ramp  
 6 will extend from the new intersection with  
 7 Southwest Bayshore Boulevard, turn northward and  
 8 connect to the Turnpike north of Southwest  
 9 Port St. Lucie Boulevard. Traffic may also travel  
 10 south along Southwest Bayshore Boulevard from  
 11 Southwest Thornhill Drive to connect to this  
 12 on-ramp. Additional improvements will include the  
 13 replacement of the Southwest Port St. Lucie  
 14 Boulevard Bridge over the Turnpike to accommodate  
 15 the proposed Turnpike widening.

16 Proposed improvements at the Southwest  
 17 Port St. Lucie Boulevard interchange will result in  
 18 roadway access changes to the Turnpike northbound  
 19 on-ramp, and southbound on- and off-ramps. At  
 20 present, all on- and off-ramp movements from the  
 21 Turnpike connect to Southwest Port St. Lucie  
 22 Boulevard at an existing intersection with  
 23 Southwest Bayshore Boulevard.

24 The proposed southbound Turnpike off-ramp will  
 25 cross over the Turnpike and connect to Southwest

1 north of Crosstown Parkway.

2 The new proposed partial interchange at  
 3 Crosstown Parkway will result in new Turnpike  
 4 access. At this interchange, a new northbound  
 5 off-ramp and southbound on-ramp will connect to  
 6 Southwest Cameo Boulevard at a new proposed  
 7 roundabout at the Southwest Cameo Boulevard  
 8 intersection with an entrance to the St. Lucie West  
 9 Centennial High School. Northbound traffic exiting  
 10 the Turnpike can then travel south along Southwest  
 11 Cameo Boulevard to Crosstown Parkway. Southbound  
 12 traffic wishing to enter the Turnpike can travel  
 13 north along southwest Cameo Boulevard and access  
 14 the Turnpike using the southbound on-ramp.

15 A new diamond interchange at West Midway Road  
 16 is also proposed as part of this project. At this  
 17 interchange, the southbound off-ramp will diverge  
 18 from the Turnpike north of West Midway Road and  
 19 connect to West Midway Road at a new proposed  
 20 signalized intersection west of the Turnpike. The  
 21 southbound on-ramp will begin at this same proposed  
 22 intersection, continue south and connect to the  
 23 Turnpike south of West Midway Road.

24 The northbound off-ramp will diverge from the  
 25 Turnpike south of West Midway Road and connect to a

1 Bayshore Boulevard at a new traffic light  
 2 controlled intersection located north of Southwest  
 3 Port St. Lucie Boulevard. Traffic can then travel  
 4 south on Southwest Bayshore Boulevard to Southwest  
 5 Port St. Lucie Boulevard. Two new Turnpike  
 6 southbound on-ramps are also proposed, one for  
 7 westbound traffic and one for eastbound traffic.  
 8 These on-ramps are located west of the Turnpike and  
 9 connect to the Turnpike south of the Southwest  
 10 Port St. Lucie Boulevard. The proposed Turnpike  
 11 northbound on-ramp will begin at the proposed new  
 12 intersection at Southwest Bayshore Boulevard and  
 13 connect to the Turnpike north of Southwest  
 14 Port St. Lucie Boulevard.

15 A new partial interchange connecting Florida's  
 16 Turnpike to the Crosstown Parkway is proposed as  
 17 part of this project. This interchange will  
 18 include a southbound on-ramp and a northbound  
 19 off-ramp. The southbound on-ramp will connect to  
 20 the Turnpike to Southwest Cameo Boulevard north of  
 21 Crosstown Parkway. This ramp will loop to the east  
 22 and south connecting to the Turnpike south of the  
 23 Crosstown Parkway. The northbound off-ramp will  
 24 begin south of Crosstown Parkway, loop to the west  
 25 and south, connecting to Southwest Cameo Boulevard

1 new proposed signalized intersection east of the  
 2 Turnpike. The northbound on-ramp will begin at  
 3 this same proposed intersection, continue north,  
 4 and connect to the Turnpike north of West Midway  
 5 Road. Additional improvements at this intersection  
 6 include the widening of West Midway Road from  
 7 Northwest East Torino Parkway to South Jenkins Road  
 8 from two to four lanes. The West Midway Road  
 9 bridge over the Turnpike will also be replaced to  
 10 accommodate the widening of the Turnpike.

11 The new proposed interchange at West Midway  
 12 Road will result in new Turnpike access. For  
 13 southbound traffic entering or exiting the  
 14 Turnpike, a new intersection with West Midway Road  
 15 will be constructed west of the Turnpike. This  
 16 intersection will be traffic light controlled. For  
 17 northbound traffic entering or exiting the  
 18 Turnpike, a new traffic light controlled  
 19 intersection with West Midway Road will be  
 20 constructed east of the Turnpike. No other traffic  
 21 access changes are proposed at this interchange.

22 Operational improvements at the Okeechobee  
 23 Road interchange will be limited to the southbound  
 24 movements. The southbound off-ramp will diverge  
 25 from the Turnpike north of Okeechobee Road and tie



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1 to the Okeechobee Road at a new proposed signalized  
 2 intersection west of the Turnpike.  
 3 The southbound on-ramp will begin at this same  
 4 proposed intersection, travel south, and connect to  
 5 the Turnpike south of Okeechobee Road.  
 6 The location of the northbound on- and  
 7 off-ramps will not change. Additional improvements  
 8 at this interchange include the widening of  
 9 Okeechobee Road from four to six lanes between  
 10 Gordy Road and South Kings Highway, and the  
 11 widening of eastbound Okeechobee Road from two  
 12 lanes to three lanes from east of Coolidge Road to  
 13 Gordy Road.  
 14 Improvements to the Okeechobee Road  
 15 interchange will result in access changes to the  
 16 southbound Turnpike on- and off-ramps. These ramps  
 17 presently connect to Okeechobee Road east of the  
 18 Turnpike at an existing intersection at  
 19 South Kings Highway. The new proposed Turnpike  
 20 southbound on and off-ramps will connect to  
 21 Okeechobee Road west of the Turnpike at a new  
 22 traffic light controlled intersection with  
 23 Gordy Road. No other roadway access changes are  
 24 proposed at this interchange.  
 25 The No-Build Alternative is just as it sounds.

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1 What happens if the Turnpike Widening project is  
 2 not built. For this study, the No-Build  
 3 Alternative assumes projects previously approved in  
 4 the study area would be constructed but no other  
 5 improvements along the Turnpike mainline or at the  
 6 existing or proposed interchanges will be built.  
 7 The Preferred Build Alternative was evaluated  
 8 in detail to analyze potential effects to the  
 9 social, cultural, natural and physical environment  
 10 in accordance with state and federal regulations.  
 11 These evaluations are documented in the State  
 12 Environmental Impact Report, which is available for  
 13 public review.  
 14 The evaluation of the Preferred Build  
 15 Alternative shows there are potential impacts to  
 16 the social environment involving residential and  
 17 commercial parcel impacts and relocations; no  
 18 impacts to air quality, minimal impacts to cultural  
 19 resources and water quality; moderate impacts to  
 20 wetlands; moderate involvement of existing  
 21 contamination sites; and enhancements to areas with  
 22 special designations. It is possible for protected  
 23 species to occur within the study area; however, no  
 24 adverse impacts are expected.  
 25 The evaluation matrix, shown here and on

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1 display during the open house and in the online  
 2 Public Hearing Exhibit Room, summarizes the  
 3 evaluation of the Preferred Build Alternative.  
 4 One of the unavoidable consequences on a  
 5 project such as this is the necessary relocation of  
 6 residential and commercial sites. On this project,  
 7 we anticipate three residential relocations and one  
 8 commercial relocation. All right-of-way  
 9 acquisition will be conducted in accordance with  
 10 Florida Statute 339.09 and the Federal Uniform  
 11 Relocation Assistance and Real Property Acquisition  
 12 Policies Act of 1970, commonly known as the  
 13 Uniform Act.  
 14 If you are required to make any type of move  
 15 as a result of a Department of Transportation  
 16 project, you can expect to be treated in a fair and  
 17 helpful manner and in compliance with the  
 18 Uniform Relocation Assistance Act. If a move is  
 19 required, you will be contacted by an appraiser who  
 20 will inspect your property. We encourage you to be  
 21 present during the inspection and provide  
 22 information about the value of your property.  
 23 You may also be eligible for relocation  
 24 advisory services and payment benefits. If you are  
 25 being removed and you are unsatisfied with the

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1 Department's determination of your eligibility for  
 2 payment or the amount of that payment, you may  
 3 appeal that determination. You will be promptly  
 4 furnished necessary forms and notified of the  
 5 procedures to be followed in making that appeal.  
 6 A special word of caution. If you move before  
 7 you receive notification of the relocation benefits  
 8 that you might be entitled to, your benefits may be  
 9 jeopardized. If you'd like more information  
 10 regarding relocation, please contact the project  
 11 manager, Brian Ribaric, after this Public Hearing.  
 12 Based on background research and field  
 13 investigations, no archaeological sites were found  
 14 within the project's archaeological area of  
 15 potential effect. Two historic linear resources  
 16 eligible for listing in the National Register of  
 17 Historic Places were found within the project's  
 18 historic area of potential effect. Impacts to  
 19 these two resources, the St. Lucie Canal, and the  
 20 Florida East Coast Railroad - Lake Harbor Branch,  
 21 are not anticipated. Coordination with the State  
 22 Historic Preservation Office will continue during  
 23 the project's design phase.  
 24 Parks, trails, and other recreational  
 25 resources are found throughout the project area.



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1 Key trails include the Loxahatchee River Paddling  
 2 Trail, the Loxahatchee River Management Area  
 3 Multi-use Trail, and the Cypress Creek Trail, all  
 4 of which abut or cross the Turnpike in the area of  
 5 the Loxahatchee River. The proposed replacement of  
 6 the Loxahatchee River bridge will enhance the use  
 7 of these trails by removing bridge piles from the  
 8 river and aligning new piles with the adjacent  
 9 Interstate 95 bridge.

10 The Phipps Park and Boat Ramp is located  
 11 adjacent to the St. Lucie Canal, with the park  
 12 located west of the Turnpike and the boat ramp east  
 13 of the Turnpike. The park and boat ramp are  
 14 connected by an access road under the Turnpike's  
 15 Thomas B. Manuel Bridge. The proposed replacement  
 16 of the southbound and widening of the northbound  
 17 Manuel bridges will be done to avoid any impacts to  
 18 this park and boat ramp.

19 The City of Port St. Lucie is proposing to  
 20 construct an Adventure Park within a city owned  
 21 parcel located west of the Turnpike and north of  
 22 Crosstown Parkway. Ramps associated with the  
 23 proposed Turnpike interchange at Crosstown Parkway  
 24 will bisect this parcel north of their connection  
 25 to Southwest Cameo Boulevard. The Turnpike has and

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1 will continue to coordinate with the City to ensure  
 2 that the proposed interchange ramps do not  
 3 significantly impact the design and construction of  
 4 this proposed park.

5 Construction of the southbound on-ramp at the  
 6 proposed Turnpike interchange at West Midway Road  
 7 will result in the need for right-of-way from the  
 8 Winterlakes Neighborhood Park. This required  
 9 right-of-way will not result in impacts to proposed  
 10 park facilities. The Turnpike will continue to  
 11 coordinate with the City of Port St. Lucie to  
 12 minimize these impacts to the greatest extent  
 13 possible.

14 The proposed improvements will potentially  
 15 affect an estimated 63 acres of wetlands and 367  
 16 acres of other surface waters. The impacted  
 17 wetlands are located within, or adjacent to, the  
 18 existing roadway right-of-way and were previously  
 19 disturbed by commercial and residential  
 20 development, roadway construction, maintenance  
 21 activities, and the invasion of nuisance and exotic  
 22 species. The FDOT will mitigate wetland impacts  
 23 resulting from this project's construction to meet  
 24 state and federal requirements.

25 Federal listed species may be present or

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1 utilize areas within the proposed project, but no  
 2 impacts are anticipated. Florida's Turnpike  
 3 Enterprise will continue coordinating with the  
 4 United States Fish and Wildlife Service regarding  
 5 federal listed species and will conduct additional  
 6 species assessments during future projects phases.

7 State listed species may also be represent or  
 8 utilize areas within the proposed project, but no  
 9 impacts are anticipated. Florida's Turnpike  
 10 Enterprise will continue coordinating with the  
 11 Florida Fish and Wildlife Conservation Commission  
 12 regarding state listed species and will conduct  
 13 additional species assessments during future  
 14 project phases.

15 Approximately 78 acres of impacts are  
 16 anticipated to designated floodplains. However,  
 17 these impacts are negligible when compared to the  
 18 overall volume of floodplains in the area.  
 19 Compensation methods, such as floodplain storage  
 20 ponds, will be provided to minimize potential  
 21 impacts. There are three Federal Emergency  
 22 Management Agency Regulatory Floodways within the  
 23 project area. Loxahatchee River, Roebuck Creek,  
 24 and Ten Mile Creek. These floodways will be  
 25 bridged to avoid and minimize impacts. There is no

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1 significant change in flood risk as a result of the  
 2 proposed roadway improvement and minimal impacts  
 3 are expected to the 100-year floodplain.

4 The segment of the Loxahatchee River crossed  
 5 by the Florida's Turnpike is one of only two river  
 6 segments within the State of Florida designated  
 7 "Wild and Scenic" by the National Park Service.  
 8 The segment of the Loxahatchee River crossed by the  
 9 Florida's Turnpike is designated scenic. To  
 10 enhance this river segment's unique values, the  
 11 replacement bridge over the river will be designed  
 12 to eliminate bridge piles in the river channel,  
 13 thereby improving river flow and recreational use.

14 In addition, stormwater runoff will be  
 15 conveyed off the bridge and adjacent roadway and  
 16 treated in stormwater management facilities prior  
 17 to discharge into the river. The Turnpike will  
 18 continue to coordinate with the National Park  
 19 Service and state agencies during the project's  
 20 design phase.

21 Results of the contamination screening showed  
 22 that 22 sites ranked "Medium" risk and 5 sites  
 23 ranked "High" risk could be potentially impacted by  
 24 the Preferred Build Alternative. These sites will  
 25 be further evaluated during the design phase to



1 identify options to avoid, minimize, or mitigate  
2 contamination involvement.

3 A noise study was conducted in accordance with  
4 the state and federal regulations to evaluate  
5 traffic noise levels for the proposed improvements.  
6 If you'd like more information regarding traffic  
7 noise, a copy of the draft Noise Study Report and  
8 the Florida's Turnpike Enterprise Traffic Noise  
9 Video are available in the documents section of the  
10 project website. The Preferred Build Alternative  
11 concept plans, also available on the project  
12 website, show the locations of potentially feasible  
13 and reasonable noise walls along the project  
14 corridor. If you have questions, you may contact  
15 the project manager, Brian Ribaric, after this  
16 public hearing during normal business hours.

17 The current cost for the Preferred Build  
18 Alternative for 37 miles of widening and various  
19 interchange improvements is approximately  
20 1.6 billion dollars. Included in this estimated  
21 cost is design, right-of-way acquisition,  
22 construction, and other services.

23 Based on future traffic projections, the  
24 design of three project segments have been advanced  
25 into the State Transportation Improvement Program,

1 received or postmarked no later than  
2 August 11, 2021 to become part of the public  
3 hearing record.

4 The next step is to incorporate your input on  
5 this public hearing into our decision-making  
6 process. The comment period will close on  
7 August 11, 2021 and your input will be considered.  
8 The PD&E report will be sent to the  
9 Florida's Turnpike Enterprise and will be signed by  
10 Executive Director and Chief Executive Officer of  
11 Florida's Turnpike Enterprise. The PD&E Study was  
12 initiated in the Winter of 2017 and is expected to  
13 be complete in the Winter of 2022. The next phase  
14 in the process is design. Preliminary Design is  
15 currently funded for three project segments.  
16 These three project segments include the widening  
17 of the Turnpike mainline from four to eight lanes  
18 from Southwest Martin Highway or State Road 714 in  
19 Martin County to Beck Road in St. Lucie County, a  
20 total length of approximately four miles, and  
21 improvements to the Turnpike interchange at  
22 Southwest Martin Highway. Right-of-way and  
23 construction are currently not funded for any  
24 project segment. This project has and will  
25 continue to comply with all applicable state and

1 or STIP. These three project segments include the  
2 widening of the Turnpike mainline from four to  
3 eight lanes from Southwest Martin Highway or  
4 State Road 714 in Martin County to Becker Road in  
5 St. Lucie County, a total length of approximately  
6 four miles, and improvements to the Turnpike  
7 interchange at Southwest Martin Highway. These  
8 project segments are only funded for design.

9 Now that we've described the preferred build  
10 alternative with its potential impacts and benefits  
11 let's review the next steps.

12 We continue to encourage public input to help  
13 us make this important decision. You can submit  
14 written comments at the project website,  
15 www.treasurecoastturnpike.com. If you are at one  
16 of the in-person locations, you can complete a  
17 printed comment form. You can email the project  
18 manager directly. For the benefit of our  
19 listen-only participants, the project manager's  
20 contact information is Brian Ribaric, at  
21 Florida's Turnpike Enterprise, Post Office Box  
22 613069, Ocoee Florida 34761. His email address is  
23 brian.ribaric@dot.state.fl.us. His telephone  
24 number is 407-264-3095. While written comments and  
25 questions are accepted at any time, they must be

1 federal rules and regulations.

2 This concludes the presentation. We  
3 appreciate your interest in this PD&E Study.  
4 Anyone desiring to make a verbal statement  
5 regarding the location, conceptual design or  
6 social, economic, and environmental effects of the  
7 improvements will now have an opportunity to do so.

8 (Conclusion of the narrated presentation.)

9 MR. RIBARIC: That concludes the formal  
10 presentation. We will now begin the public comment  
11 period. Please note that we will not respond to  
12 your comments and questions today, but will respond  
13 in writing at a later date. Anyone desiring to  
14 make comments regarding the project will now have  
15 an opportunity to do so. There are multiple ways  
16 you may provide your comments tonight. Everyone  
17 online or who dialed in can submit written comments  
18 online by mail or email. If you are in the  
19 audience tonight at the Stuart or THE Port St.  
20 Lucie location, you may complete a comment form and  
21 drop it in the comment box or mail it in after the  
22 public comment period by August 11, 2021. You may  
23 also provide written verbal -- excuse me. You may  
24 also provide verbal comments in one of four ways:  
25 If you are attending either of our in-person



1 locations, please complete a Speaker Request Card  
 2 and submit it to a member of the project team.  
 3 Second, if you are online, you may provide a verbal  
 4 comment if you requested to speak at registration.  
 5 Third, you may speak directly to a court reporter  
 6 at either of our in-person locations. And fourth,  
 7 you can call me at 407-264-3095 during normal  
 8 business hours after the public hearing to provide  
 9 your verbal comments. Anyone that has dialed in  
 10 using the telephone only option, we are unable to  
 11 unmute you so if you wish to make a comment, please  
 12 use one of the other options presented tonight.

13 So we will now call on our participants at the  
 14 Stuart location who have requested to speak as. As  
 15 Bill calls your name, please step -- stand up to  
 16 the microphone and state your name and address for  
 17 the record. If you're representing an  
 18 organization, municipality or other public body,  
 19 please provide that information as well. We ask  
 20 that you limit your time to three minutes.

21 Bill?

22 MR. HOWELL: All right. Thank you, Brian. We  
 23 have seven speakers that have registered to speak.  
 24 The first speaker would be Beth Beltran and  
 25 following her will be Terry Rosenblum and followed

1 speaker will be Terry Rosenblum and then followed  
 2 by Antonia Barnes.

3 MS. ROSENBLUM: My name is Terry Rosenblum.  
 4 I'm HOA secretary for Copper Leaf. We have a  
 5 petition, which every resident has signed. I've  
 6 been in contact with Hammock Creek and working on  
 7 with Martin Downs Community for a sound wall to  
 8 complete all the way down from Becker Road to 714.

9 I noticed that there is a part of the Turnpike  
 10 that will have a sound wall, but then there's a  
 11 section that's not, and then there's some homes  
 12 that are impacted but they felt that wasn't a need  
 13 for it, which we all know it comes down to money,  
 14 but we all have to live there.

15 So, I mean, I'm requesting that they -- and  
 16 I've been in contact with Toby Overdorf, our  
 17 House Representative, as well to foresee into this  
 18 and, hopefully, we can get the wall to go all the  
 19 way down. We call it the 2.5, which is from  
 20 Copper Leaf all the way down to Martin Highway.

21 So that's what I would like to say. Thank  
 22 you.

23 MR. HOWELL: Thank you, Terry. And now  
 24 Antonia Barnes, to call you up, and then you'll be  
 25 followed by Lisa Tompson.

1 by Antonia Barnes.

2 So Beth, if you would stand right over here.

3 MS. BELTRAN: This is kind of strange.

4 Hi, I'm Beth Beltran. I'm the administrator  
 5 of the Martin Metropolitan Planning Organization,  
 6 and our MPO it's also known as, and I just wanted  
 7 to point out that this project, the widening of the  
 8 Turnpike to eight lanes and the changes to  
 9 State Road 714 interchange are not identified in  
 10 the Martin MPO 2045 long range transportation plan.  
 11 What is presented as the preferred alternative for  
 12 the State Road 714 interchange is not the preferred  
 13 alternative of the MPO Board. In fact, twice the  
 14 MPO Board has requested of the Turnpike to be  
 15 presented with all the alternatives considered for  
 16 the State Road 714 interchange and to date that has  
 17 not happened.

18 I'm hoping, however, that the Turnpike staff's  
 19 presentations at the August 10th Martin County  
 20 Commission meeting, as well as the September 20th  
 21 MPO Board meeting, will include all the  
 22 alternatives considered for the State Road 714  
 23 interchange as requested.

24 Thank you.

25 MR. HOWELL: Thank you, Beth. The next

1 MS. BARNES: Good evening. My name is  
 2 Tony Barnes. I live just west of the Turnpike and  
 3 off of 714, and in looking at that interchange that  
 4 they are proposing or redesigning the -- off --  
 5 onto 714, it appears to me that that's not going to  
 6 accomplish any of the goals that they say they're  
 7 going to accomplish. It's certainly not going to  
 8 improve mobility for those of us that live west of  
 9 the Turnpike off of 714, and it's certainly not  
 10 going to accommodate the future demands of traffic.  
 11 As we all know, there's a lot of building going on  
 12 west in Palm City Farms. One of the newest  
 13 developments is Newfield and its proposed traffic  
 14 increase is going to be 45,000 trips a day,  
 15 according to their own traffic report.

16 It seems to me that all you're doing is  
 17 splitting the which -- which way the traffic is  
 18 going to go, but it is not actually going to  
 19 increase our mobility because it's just going to be  
 20 more and more traffic on that little strip of road  
 21 between Citrus Boulevard and the Turnpike.

22 What I think needs to be done, and other  
 23 people may or may not agree with me, is that we  
 24 need to put a direct interchange between the  
 25 Turnpike and I-95. Not all traffic comes off of



1 there to just hit those two highways; however, a  
 2 large portion of it does. And if that traffic was  
 3 taken off of our -- our local road, we would  
 4 certainly then be able to use the off-ramp as it  
 5 currently exists, and they don't need to do this  
 6 very fancy split, which frankly I didn't really  
 7 understand. It just moved a little too fast for  
 8 me. Also, Leighton Farms Road is not a major  
 9 thoroughfare. It is a very small rural road, and  
 10 it would clearly change the whole dynamic of that  
 11 area to start putting huge volumes of traffic on  
 12 there.

13 So my one thought is, if you really want to  
 14 try and alleviate our traffic, you need to do a  
 15 direct interchange between the Turnpike and I-95.

16 My other thought was that you really -- I  
 17 realize this isn't part of this particular study,  
 18 but you need to put an access ramp onto I-95  
 19 directly onto Citrus Boulevard. So much of the  
 20 traffic that comes from the north part of  
 21 Citrus Boulevard turns onto 714 to get to I-95. If  
 22 you had a direct access straight down Citrus, you  
 23 would again alleviate a lot of traffic that goes  
 24 between Citrus Boulevard and the Turnpike. And  
 25 with all the development that's coming up,

1 stations anymore anyway so why are we thinking  
 2 about that now as we seem to be going in that  
 3 direction?

4 A sound barrier is absolutely required. I  
 5 would say from the -- all four miles, and I would  
 6 like the sound barrier put in before any  
 7 construction begins. They were doing some kind of  
 8 construction on the Turnpike about six months ago  
 9 and all you could hear at night, when you were  
 10 trying to get to sleep, was beep, beep, beep all  
 11 night long from machines going backwards and  
 12 forwards and all around the place, I guess.

13 So if we're going to be doing construction at  
 14 night, then we definitely need a sound barrier  
 15 prior to construction and it should be of the best  
 16 quality and best absorption, if there's any such  
 17 thing, that can exist.

18 The other thing I would like them to consider  
 19 is the type of tarmac, or asphalt, or whatever it  
 20 is you put under the road on the road itself to  
 21 reduce the sound, if it's possible. I mean, I'm  
 22 sure that if they can -- two billionaires can go up  
 23 into space within the space of about two weeks, we  
 24 can figure out some sort of road surfacing material  
 25 that is much more quiet and sound of -- sound

1 especially that little piece of land between the  
 2 Turnpike and High Meadows, there's going to be a  
 3 Wawa's, there's going to be a Tractor Supply,  
 4 there's going to be an Aldi's, there's going to be  
 5 a shopping center. That's all local traffic, but  
 6 you're going to have all of this other traffic  
 7 using our local road when they don't really need  
 8 to. You've put those kind of interchanges down in  
 9 Broward County. I personally use them. They're  
 10 wonderful. You go directly from the Turnpike to  
 11 I-95. Why can't you do the same for us?

12 Thank you for listening.

13 MR. HOWELL: Thank you, Antonia, very much for  
 14 your comments. We have Lisa Tompson will be next  
 15 followed by Nancy Urcheck.

16 MS. TOMPSON: My name is Lisa Tompson, and I  
 17 live in the Sanctuary at Hammock Creek, and I don't  
 18 have many comments, but I think the most important  
 19 thing I have to say is the question of the quality  
 20 of life that this will affect on many of us, as  
 21 well as many other species and plants and insects,  
 22 et cetera. So I would please like to ask that  
 23 people really think about this, not only for the  
 24 present but for the future way beyond 2045. And  
 25 who knows, hopefully, by 2045 we won't need gas

1 reduction.

2 And I know there was some comment in that  
 3 paper about the golf course, and I played golf the  
 4 other day at Hammock Creek along the bit that  
 5 parallels the Turnpike and it was extremely loud,  
 6 whether it's one golfer out there or five thousand  
 7 golfers. I think they were saying in one day or  
 8 some crazy number. The point is, that the houses  
 9 that are there are vastly affected, and we need to  
 10 really, really think about the quality of life  
 11 before we start these enormously expensive  
 12 projects, when \$1.6 billion could probably be used  
 13 in our school systems a lot more than it could be  
 14 used on our highways.

15 Thank you.

16 MR. HOWELL: Thank you very much, Nancy?  
 17 Robert Boyer is next. I'm sorry. That was --  
 18 you're Nancy, and then Robert Boyer's next.

19 MS. URCHECK: My name is Nancy Urcheck. I  
 20 live in Palm City in the Hammock Creek development.

21 Certainly over the last several years the  
 22 traffic along the Turnpike has increased. The  
 23 noise levels have increased. Recently there was a  
 24 resurfacing of the Turnpike that has dramatically  
 25 increased the noise levels even now so that we find



1 ourselves impacted in -- in the use of our  
 2 backyard. The noise levels, it's not quite so  
 3 relaxing, and even at night now, for the first  
 4 time, I'm hearing the Turnpike noise lying in bed.  
 5 The noise level studies show an increase of, I  
 6 think, eight decibels, which almost doubles the  
 7 current noise levels. I shudder to think how our  
 8 ability to use our properties or even to sleep at  
 9 night are going to be impacted by this project once  
 10 it's done. And the construction, as the prior  
 11 speaker said, when they were just resurfacing all  
 12 you could hear was the beeping and the noise at  
 13 night making it very difficult to sleep. The  
 14 construction, I can't imagine that impact on our  
 15 community with the noise from the construction.

16 So regardless, those noise barrier walls must  
 17 stay on the plans, and, yes, if possible, there  
 18 needs to be some way to mitigate the noise during  
 19 the construction period, because our community is  
 20 severely impacted.

21 Thank you.

22 MR. HOWELL: Next speaker Robert Boyer  
 23 followed by Frank McGann.

24 MR. BOYER: Thank you. I'm Bob Boyer. I live  
 25 in the Sanctuary also. As some of the previous

1 grinding. I mean, I think everything's been said  
 2 so far and I agree with the majority of it, and I  
 3 just hope they listen to the residents of Florida,  
 4 not just the consultants.

5 Thank you.

6 MR. HOWELL: Thank you. And Frank.

7 MR. MCGANN: Good evening. My name is  
 8 Frank McGann. I'm a resident within the Palm City  
 9 Farms area just off of 714. I'm very concerned  
 10 about our junction at 714 and what we've got to do  
 11 there. I think we've got something wrong in terms  
 12 of the conduit of traffic that flows across the  
 13 area. It appears that all traffic emerges right on  
 14 that junction in order to get into Palm City, in  
 15 order to get to Stuart, in order to get to the  
 16 Roosevelt Bridge, especially if you're coming south  
 17 to north traffic flow. We also then have the  
 18 potential of people getting off at 714 to head to  
 19 Okeechobee, if you're coming from down south.

20 So, again, that is just a major network that's  
 21 built up in one area and that does concern me.  
 22 Anybody that lives in -- in Palm City, if you want  
 23 to get to Jensen Beach, wherever else, you all  
 24 hitting that junction area. If you get off at 101,  
 25 get off at 102, and they bring you up on Kanner or

1 speakers have met -- you know, the noise at night  
 2 has just gotten terrible. Whoever -- the last  
 3 repave, I know it's not part of this, whatever  
 4 material change you made, it drove the Turnpike  
 5 noise up to unbearable levels at night. So, you  
 6 know, let's not go cheap because we ain't saving  
 7 any money.

8 The sound barrier is a must. It was  
 9 mentioned. I love the direct interchange from I-95  
 10 to the Turnpike. It serves more than just our  
 11 needs, it serves anybody commuting when there's a  
 12 traffic jam that can get from one road to another.  
 13 If they've got to evacuate the state -- I mean,  
 14 that's a great thing. You know, we're spending a  
 15 lot of money, one million six, it seems -- in this  
 16 day and age people may not think that's much money,  
 17 but if inflation takes off, watch these projects  
 18 get killed. Hours of construction, as they said, I  
 19 could hear the grind -- I live a thousand feet  
 20 away, it wasn't included in the mailing, but I  
 21 could hear the grinding of the Turnpike when they  
 22 was repaving at night. I mean, it would wake me up  
 23 two o'clock, three o'clock in the morning. If I  
 24 just woke up a little bit, it was there. It was  
 25 steady beep, beep, beep, and the noise and the

1 bring you up on High Meadows. And again, you're  
 2 hitting 714 and that network on that junction. And  
 3 the question is, is that actual junction a major  
 4 problem for the future of our community? And I  
 5 believe it is. And therefore, maybe part of this  
 6 project should be looking at an extra junction  
 7 within our community to help create a new conduit,  
 8 whether that's the extension of Becker, I don't  
 9 know, I'm not into any of this whatsoever else, but  
 10 it also creates a crossing over the canal, the  
 11 river, at South Fork to get onto US One. So I  
 12 think there's a major problem.

13 The Second part of my question would be, you  
 14 know, I'm involved with things in Port St. Lucie as  
 15 well -- I'm hearing talks of maybe an extra exit  
 16 that may go in at Kings Highway to access the  
 17 airport, that area. And there's maybe talk of  
 18 another junction between Becker and Gatlin to help  
 19 the flow of traffic out of Traditions and that  
 20 corridor over into -- into the east side of the  
 21 community there.

22 So there's a lot of questions that haven't  
 23 been touched upon or brought up yet in this whole  
 24 project, which has been me concerned. My biggest  
 25 concern is the flooding issue.



1 We already have a major problem with the --  
2 within Palm City Farms within the capacity flow of  
3 the Danforth and Bessey Creek, and this falls right  
4 slap bang in the middle of all this, and that does  
5 concern me as well that we're not looking at the  
6 potential flood issues that may be caused if either  
7 side of this juncture's going to be expanded.

8 Thank you.

9 MR. HOWELL: Thank you Frank. Is there anyone  
10 else desiring to speak that hasn't had an  
11 opportunity?

12 Okay. Brian, that concludes the speakers here  
13 at Stuart.

14 MR. RIBARIC: We'll now call upon our  
15 participants at the Port St. Lucie location who  
16 have requested to speak. As Mark calls your name,  
17 please step up to the microphone and state your  
18 name and address. If you represent an  
19 organization, municipality, or other public body,  
20 please provide that information as well. We ask  
21 that you limit your comment time to three minutes.  
22 Mark?

23 MR. EASLEY: Yes, we have three speakers. The  
24 first speaker will be Freddie O'Neal.

25 MR. O'NEAL: Yes.

1 that the -- the ingress and egress of the Turnpike  
2 and the extension of the Turnpike for reasons of  
3 the economy and the environment, because in case of  
4 a diaster then we need more lanes available for --  
5 to exit the state or come into the state. So I --  
6 I have no problem with that, but I thank you for  
7 understanding our situation. The FDOT to all our  
8 homeowners and everyone involved. I'm here to just  
9 be with everyone and to enjoy life here in  
10 Port St. Lucie. And I thank you all for what  
11 you're doing for our state and for our community,  
12 and I enjoy all our residents for their  
13 participation.

14 Thank you.

15 MR. EASLEY: Mark will be next and following  
16 Mark will be Gustavo Gonzalez. Mark?

17 MR. RICHARDSON: Good evening, everybody.  
18 Mark Richardson, 1697 Southwest Hampshire Lane,  
19 PSL, Florida -- sorry, Port St. Lucie, Florida.  
20 The main thing we were worried about along our  
21 street -- with our backyard is connected to -- or  
22 against the Turnpike were we being eminent domain  
23 out or were we staying in our house and the road is  
24 going to get closer. Either way there's two things  
25 that we need to know. One was answered, that we're

1 MR. EASLEY: After Freddie O'Neal will be  
2 Mark Richardson. Once you come up here, please  
3 state your name and your address and then you  
4 can -- and state your comments.

5 MR. O'NEAL: Good evening. My name is  
6 Freddie O'Neal. I live at 702 Southwest Crean  
7 Terrace in Port St. Lucie. I moved from  
8 West Palm Beach in 2006 to Port St. Lucie. I love  
9 the community that I'm living in now. We have good  
10 neighbors. We all get along. We have a crime  
11 watch in our community, and we all enjoy being  
12 together. Most of my questions was answered on the  
13 screen about what alternate plan that the Turnpike  
14 would go on, and I see where it's going to swing  
15 around. I see alternate A because it's going to  
16 swing around to the existing Turnpike that we're  
17 living -- the Turnpike is right behind our homes so  
18 we won't be affected by moving, as I can see it on  
19 this screen. Now, my -- my question is, I don't  
20 have a problem with the noise or anything like  
21 that, but when this extension is created, now it's  
22 going to move the Turnpike closer to our homes, and  
23 we -- we would like to know what type of a barrier  
24 that would -- and the noise factor during that time  
25 that it's going to create. And I -- I respect

1 going to be pretty much staying in our house, which  
2 is very good. The other thing that I looked at  
3 online, at the noise document that was online, for  
4 some reason it stopped at Section 1500 and it  
5 didn't go all the way to Section 1512. That area  
6 there is our whole Hampshire Lane area, and there's  
7 plenty of room there for a wall, but they put a --  
8 they call it a shoulder. I call it a hill, I  
9 guess, instead of an actual wall. My biggest thing  
10 is safety.

11 I don't know if you guys did any research as  
12 far as accidents in that area. Our area right  
13 there is next to a service plaza. If anybody in  
14 this room were to go into that service plaza,  
15 you're going to come out of the service plaza, the  
16 next person to you that's coming down that road as  
17 you're entering the Turnpike -- automatically  
18 you're going to go to the right, to the outside  
19 lane. Well, guess where that outside lane is when  
20 you're getting to around there, right there at our  
21 property so -- not only that but there's been  
22 police records that I have personally called where  
23 there's been accidents behind us. In my property,  
24 there has been tires from accidents. So not only  
25 is it a noise that we're looking for, we're looking



1 safety for not only our people but our neighbors as  
2 well. There really needs to be a plate -- a  
3 person-to-person, property-to-property, where  
4 there's no wall or anything, you guys need to come  
5 out and say, Hey, you do need something.

6 And No. 2, back in February and March you guys  
7 sent out a letter -- I know I'm going longer than  
8 three minutes. We never received anything so how  
9 can we say our comments? How can we say our  
10 questions, if nobody's asking us anything? How are  
11 we knowing that we need to be here? Our attorney  
12 down in Miami, that we chose out of, like, I think,  
13 five or six different attorneys, e -- or sent us  
14 mail saying, Hey, you guys are going to be eminent  
15 domain. What? So we're getting eminent domain  
16 letters from attorneys around the state, but  
17 where's the state telling the people? We're right  
18 here. That's the main thing.

19 Two things, again, we didn't want to move.  
20 We're not moving. Thank you. But we still want to  
21 be protected and safe.

22 MR. EASLEY: Thank you for your comments. We  
23 need to move on to the next speaker.

24 MR. RICHARDSON: I understand that and I'm  
25 sorry --

1 matter what. We're going to go west, east, south,  
2 anywhere. People love Florida. Our concern is,  
3 like everybody here, is whatever you're gonna build  
4 make sure you build it to be all comfortable,  
5 because right now you can do eight lanes, maybe in  
6 10 more years you're going to do 12 lanes, or  
7 whatever, because people are moving to Florida. It  
8 seems to be nobody likes no more the big cities,  
9 because whatever problem they got over there.

10 Our concern is, like, the privacy wall. You  
11 can do the road, you can do whatever you want, but  
12 I always live down south, Palm Beach, Broward,  
13 everywhere, they'll build those privacy walls  
14 almost 20 feet high for safety. You might even --  
15 kids that play in the backyard, like my neighbors  
16 say, and so how you going to -- you got a piece of  
17 rock, a piece of metal fly in the air, what's going  
18 to happen to one of those kids? Because you never  
19 know, anything can happen today.

20 Our concern is just the safety. If you guys  
21 gonna spend so much money to build something, don't  
22 forget about us. We are the one that need all the  
23 help. We the one that you have to be safe.  
24 There's always money over there to do whatever you  
25 want, especially right now, but all -- like I'm

1 MR. GONZALEZ: You can take my --

2 MR. RICHARDSON: -- but it's a safety thing  
3 that we're talking about.

4 MR. GONZALEZ: You can take my time. It's  
5 my --

6 MR. RIBARIC: -- thing that we're talking  
7 about.

8 MR. EASLEY: Mr. Gonzalez, would you like to  
9 speak?

10 MR. GONZALEZ: My name is Gustavo Gonzalez.  
11 It seems to be I got my neighbors right beside me.  
12 I waited since 2006 to build my house in  
13 710 Southwest Crean Terrace. And I thank God  
14 because my neighbor over here last week, or about  
15 two weeks ago, I was there with my wife showing the  
16 property and said, Look, this is going to be our  
17 new home, you know, since I already live in  
18 Palm Beach. And then he told me, Look, they're  
19 going to be -- do a station on the Turnpike so all  
20 these houses got to go away, even your lot. I  
21 said, Well, it can't be that, you know, we haven't  
22 received nothing. So it seems to be two days later  
23 I got the let -- the letter in the mail.

24 I've been living in Flor -- in Florida for  
25 almost 43 years. We're still going to grow up no

1 saying, build the privacy wall all the way to  
2 the -- even to the ramp. I don't think it's that's  
3 much money. It might cost you maybe a million  
4 dollar more, may cost you \$500,000 more. They --  
5 he told me it might go eight feet. Well, don't go  
6 a eight feet, go 12 feet, go 20 feet, whatever  
7 needs to be done. That's our concern.

8 Florida is going to be growing so fast that  
9 you're never going to believe. People living here  
10 for a lot of years, they know. I've been living  
11 here for 40 years. I came all the way from Miami  
12 all the way up here. I see this growing out so  
13 fast. We're not going to escape it. The traffic,  
14 we're not going to escape. The people moving to  
15 Florida, this is a retirement place. This is a  
16 sunny place. We get sun here and we get  
17 hurricanes. We get hurricanes, we can leave.  
18 People up north they get fire, they get tornadoes,  
19 they can't leave. It takes a couple of seconds.

20 Just don't forget about us, you know,  
21 whoever's doing this project. Remember we're the  
22 ones that always -- count on guys. We gotta do  
23 what you guys say because we don't -- sometimes we  
24 don't have no more choice, but let's do it right,  
25 please. That's all I say.



1 MR. EASLEY: Thank you. Is there anyone else  
2 who would like to speak?

3 Brian, I think that's all.

4 MR. RIBARIC: We will now -- we will now call  
5 upon our online participants who requested to  
6 speaker at registration. When your name is called,  
7 you will be unmuted. If the microphone icon on the  
8 go to webinar control panel is green, you are ready  
9 to make your comment. If the microphone icon is  
10 red, you will need to click on the icon once, it  
11 will then turn green and notify you that you are  
12 unmuted, as shown on the go to webinar control  
13 panel to the right. You are then ready to speak.

14 Please state your name and address. If you  
15 represent an organization, municipality, or other  
16 public body, please provide that information as  
17 well.

18 In an effort to accommodate all speakers, we  
19 request that you take three minutes for your  
20 comment. So the first speaker we are going to call  
21 is Mayor Steven Grant of Boynton Beach and then on  
22 deck is Leroy Petersen.

23 All right. Moving on. We're going to  
24 Mr. Leroy Petersen.

25 You are -- please feel free to unmute

1 Thank you.

2 MR. RIBARIC: Thank you. So right now we are  
3 going to have Carolina Williams, is next, and then  
4 Vincent Williams is on deck.

5 MS. WILLIAMS: Good evening, everybody. My  
6 name is Carolina Williams, and I reside on the  
7 600 block of Southwest Montana Terrace, and I do  
8 share similar sentiments as my neighbors in the  
9 Hampshire Street. And pretty much I wanted to know  
10 if we are going to be impacted by this project. I  
11 noticed that some of the trees are already being  
12 cut down, and even the trees right next to my  
13 property, since we are adjacent to Turtle Run Park,  
14 and that was my concern. Yes, of course, the  
15 traffic right now is almost unbearable, and I can  
16 only imagine how much more it's going to be  
17 impacted after these additional links are added,  
18 but overall my concern is if we are going to be  
19 impacted how soon will I know in order for our  
20 family to take the necessary measures if a  
21 relocation will be needed, and that's pretty much  
22 all. I do thank you for your time.

23 MR. RIBARIC: Thank you. We have  
24 Vincent Williams, and then on deck is  
25 Robert Johnson.

1 yourself.

2 MR. PETERSEN: Okay. Yes. Hello.

3 MR. RIBARIC: Go ahead.

4 MR. PETERSEN: Yeah, my question is brief.  
5 I'm living 6938 Northwest Baroda Street, and I'm on  
6 the east side of the Turnpike, and I'm not quite  
7 sure whether we're going to have sound walls up or  
8 not, but based on everybody else I'm hearing, I  
9 think that's the -- that should be preference  
10 because I say about two, two months ago there was a  
11 double trailer, 18 wheeler had some sort of, I  
12 guess, explosive stuff in the trailer and caught on  
13 fire and it exploded. It was like -- we was up in  
14 Cape Cod [verbatim] there when the -- the shuttle  
15 take off, you know, the house shook, my neighbors  
16 left and right on me, you know, everybody you know  
17 came out the house thinking that there was some  
18 sort of major explosion. So I'm not quite sure the  
19 expansion and the noise. Even if it's at the level  
20 now, you're talking about doubling that level.

21 So I really hope that you consider, you know,  
22 sound barriers in the area and have the  
23 residents -- you know, have the residents at -- at  
24 heart, in mind, and that's it.

25 I appreciate you -- the time and everything.

1 All right. We'll come back. So  
2 Robert Johnson you are now -- it's your turn, and  
3 then on deck is Gary Massing.

4 MR. JOHNSON: Good evening. My name is  
5 Robert Johnson. I live in the Rialto development  
6 located in Jupiter, Florida. The development is  
7 parallel to I-95 and the Florida Turnpike. There  
8 are 10 lanes of travel currently in this area, five  
9 lanes that go north and five lanes that go south.  
10 The widening of the Florida Turnpike will increase  
11 the traffic by approximately 40 percent in this  
12 area.

13 The Rialto development is unique where it is  
14 right alongside both of these major highways. My  
15 property value will dramatically decrease, and my  
16 quality of life will decrease. We need a sound  
17 barrier wall.

18 The noise study report that I read is flawed.  
19 The study was performed during the pandemic. It  
20 was performed on August 19th of 2020, September 1st  
21 of 2020, and October 26th of 2020. We all know the  
22 traffic volume during this time frame was reduced  
23 over 50 percent so the study is flawed. It was  
24 also performed during the week. There was no days  
25 where they studied the sound during the weekend



1 where we know there's more traffic.

2 Also, the report doesn't indicate whether the  
3 wind was east or west, north or south. We all know  
4 living in South Florida if you have a easterly  
5 wind -- I live on the east side, it's blowing the  
6 noise westerly. When I have a westerly wind, the  
7 noise increase is over 50 percent. So the report  
8 doesn't even indicate which direction the wind was  
9 blowing when they performed the testing.

10 I ask that you put up a sound barrier wall  
11 the -- for the entire length of the Rialto  
12 development.

13 And I appreciate your time. Thank you.

14 MR. RIBARIC: Thank you. So we have  
15 Gary Massing, and then I think Vincent Williams has  
16 logged in so he is on deck.

17 All right. It looks like Gary may not be with  
18 us tonight so let's see if Vincent Williams is  
19 available.

20 MR. WILLIAMS: Good evening. My name is  
21 Vincent Williams. I live at 601 Southwest Montana  
22 Terrace in the beautiful City of Port St. Lucie,  
23 Florida. My wife Carolina already spoke previously  
24 regarding some of our concerns that we had with  
25 this proposed Turnpike expansion project.

1 One of the main concerns I have regarding this  
2 project is, how is it going to impact our proposed  
3 property taxes? And obviously, if we're going to  
4 be incorporating toll -- toll lanes on the  
5 Turnpike, that's going to be generating more  
6 revenue for the people that are utilizing the  
7 Turnpike to travel day in and out. How would that  
8 reflect in our property taxes? Is that going to  
9 increase our property taxes? Are we going to see a  
10 break in our property taxes? Because some of us  
11 pay a pretty steep amount in our property taxes  
12 here in Port St. Lucie and the standardized  
13 homestead exemption is only 3 percent. So I hope  
14 that our property taxes aren't going to go up  
15 anymore, because, unfortunately, some people will  
16 eventually be priced out of this beautiful  
17 community, not to mention the potential devastating  
18 effects of the noise and the construction is going  
19 to have on the everyday quality of life of people  
20 in this community.

21 I hope that they incorporate and take into  
22 consideration a sound barrier like many of the  
23 residents have already stated. I feel that that's  
24 very necessary for safety and for quality of life,  
25 and I hope you guys strongly take that into

1 consideration. And for the people that are in the  
2 potential relocation area, I hope that you mail out  
3 those letters to those people in a period of time  
4 that's appropriate for them to respond and to look  
5 for another home to move into if necessary so that  
6 they're not stuck, at the last minute. As some of  
7 these people have indicated, they had to receive  
8 information from their attorneys regarding the  
9 potential expansion project of this Turnpike.

10 So I thank you very much for your time. And  
11 if necessary, I look forward to hearing from you.

12 MR. RIBARIC: All right. Thank you for your  
13 comment.

14 Now we have Ross Aronovitz and on deck will be  
15 Jonathan Uhler.

16 MR. ARONOVITZ: Good evening. Thank you for  
17 having me speak tonight. I wanted to thank  
18 Commissioner Marino for attending his hearing.

19 I represent the Board of Directors for the  
20 Rialto community in Jupiter, Florida, for a  
21 development of 485 homes.

22 Robert Johnson spoke earlier, and we are very  
23 opposed to the current project plan. We, as he had  
24 mentioned, reviewed the noise study and our  
25 attorney sent you a letter earlier today opposing

1 the current plan in place. We feel that the noise  
2 study that was conducted has serious flaws, in  
3 terms of when the noise study was surveyed during a  
4 pandemic when there was reduced traffic. Also, the  
5 reporter misidentified that there is sound  
6 mitigation that covers the entire community Rialto.  
7 The sound mitigation, which is a berm, and a sound  
8 wall that was noted in the report, is false. It  
9 doesn't cover the entire community. It ends south  
10 of the Behring Way. So the results of the report  
11 we feel are inaccurate.

12 And we had also reached out to Brian. I've  
13 been communicating with Brian since 2019. And the  
14 prior meetings we were not given notice, even  
15 though we had -- had asked for notice to be able to  
16 attend. This community is very upset with the  
17 results that have been shared with us, and we are  
18 strongly opposed to the project plan.

19 We are seeking a sound barrier in -- for the  
20 current project plan that's in place for the entire  
21 length of the community of Rialto. We are  
22 negatively impacted, as Bob had mentioned. There's  
23 two highways that are right next to each other.  
24 You're increasing the capacity. You're going to  
25 increase the noise. It's going to negatively



1 impact our community, it's going to negatively  
 2 impact our property value, and we're not in favor  
 3 of that. And we would really appreciate -- and the  
 4 other thing is, is that we've reached out to the  
 5 Florida Turnpike to ask them if there are other  
 6 vendors that they use to do these noise surveys and  
 7 so far to date, in communicating with Brian and  
 8 with Doug Zang, we were given no -- we were not  
 9 provided with that information, even though we  
 10 requested it. And I've reached out to other  
 11 Florida Turnpike officials and haven't gotten a  
 12 response so that our community can assess the sound  
 13 study to determine whether or not the results are  
 14 accurate or not.

15 Thank you for your time, and we strongly are  
 16 requesting that a sound wall is installed for the  
 17 entire community for the length of Rialto.

18 MR. RIBARIC: Would you please state your name  
 19 and address for the record. You didn't do that at  
 20 the beginning. Please.

21 MR. ARONOVITZ: Yeah. My name is  
 22 Ross Aronovitz, and I am one of the Board of  
 23 Directors for the Rialto community. My property  
 24 address is 196 Behring Way in Jupiter, Florida.

25 MR. RIBARIC: So now we have Jonathan Uhler,

1 the children that ride their ATVs and bicycles.  
 2 This cut through has also led to several accidents  
 3 at the intersection of Honey Terrace and  
 4 Citrus Boulevard. And now we have several dump  
 5 trucks that use it as a cut through, and there's  
 6 going to be plenty more with all the construction  
 7 going on on Citrus Boulevard.

8 So my concern is that we paid out of our  
 9 pockets, as this is a private road, to have it  
 10 paved, and if we're going to have all these heavy  
 11 trucks coming off this new exit ramp on  
 12 Leighton Farms, if they're going to be using our  
 13 road as a cut through, it's going to put more wear  
 14 and tear on our road, which will require more  
 15 maintenance and repairs. So one of my concerns is  
 16 that we're going to be expected to continue to pay  
 17 for our road more frequently to be fixed due to  
 18 this increased traffic. And then second is, are  
 19 there plans to put up signage, because this is an  
 20 equestrian area and to slow for horses and  
 21 children. There are similar communities in Jupiter  
 22 and Wellington that have these signs alerting  
 23 motorists to slow for the horses and children, and  
 24 I'd like to see some in my area to address this  
 25 problem, as well as additional speed limit signs.

1 and then on deck is Karen Monaco.

2 MR. UHLER: Hello. This is Jonathan Uhler at  
 3 220 Andros Harbour Place in Rialto. I'm in the  
 4 same neighborhood as Ross that just spoke, and I'll  
 5 just piggyback we agree with everything he said in  
 6 regards to the noise survey and the extension of  
 7 the wall.

8 Thank you.

9 MR. RIBARIC: Thank you. So now we have  
 10 Karen Monaco, and then on deck is Donna Levegood.

11 MS. MONACO: Hi. My name is Karen Monaco. I  
 12 live in Palm City Farms off of Leighton Farms Road.

13 My concern is the additional traffic this will  
 14 bring to my neighborhood and my street in  
 15 particular. So my question is, has the issue of  
 16 additional trucks and cars using Southwest Honey  
 17 Terrace as a cut through to Citrus Boulevard been  
 18 addressed and researched? They paved this road two  
 19 years ago, traffic has increased tenfold since it's  
 20 been paved, especially during commute hours and --  
 21 you know, I used to live on a nice quiet dirt road,  
 22 and now many people drive 45 miles an hour down it  
 23 to use it as a shortcut. So my concern is that  
 24 this is a farm community and several of us ride our  
 25 horses on the road on a daily basis, not to mention

1 Anyhow, that's my concern. Thank you for  
 2 having us tonight.

3 MR. RIBARIC: Thank you. So now we have  
 4 Donna Levegood, and then on deck is Greg Barnard.

5 Microphone is on. All right. We'll come back  
 6 to Donna here.

7 So Greg Barnard, and then on deck is  
 8 Barry Parker.

9 All right. Greg does not appear to be with us  
 10 tonight. We'll go to Barry Parker. On deck is  
 11 Ryanne Powers-Cavo.

12 MR. PARKER: Hello. This is Barry Parker. I  
 13 live in Port St. Lucie at 891 Southwest Grand  
 14 Reserves Boulevard in the Vineyards. Like is --  
 15 with the Crosstown Parkway proposed entrance or  
 16 exit, it's proposing a roundabout on  
 17 Cameo Boulevard and in that area are two schools.  
 18 There's a K through 8 and then there's a high  
 19 school. So the added traffic and congestion in  
 20 that area, compounded by two schools, is a concern.  
 21 And then the next concern I have is  
 22 Cashmere Boulevard is the next parallel road to  
 23 Cameo that goes between Crosstown and  
 24 Port St. Lucie West Boulevard -- is the major  
 25 north/south. That's proposed to go from a two-lane



1 to a four-lane. I would hope that would be  
 2 accomplished before the proposed Turnpike, you  
 3 know, interchange on Crosstown, because it's going  
 4 to impact that major north/south road, which is  
 5 Cashmere Boulevard, which is the back gate of two  
 6 developments. It's my development, the Vineyards,  
 7 192 homes and Heatherwood, which is about 400  
 8 homes, and that -- that exit onto Cashmere does not  
 9 have a light and it also comes in part of the --  
 10 you know, it comes out onto Cashmere where there's  
 11 McChesney Park and then the two schools. And  
 12 during school period it's very, very congested and  
 13 been involved with accidents, and so my concern is  
 14 thinking, you know, to Cashmere, and then that  
 15 roundabout on Cameo with two schools, and then that  
 16 proposed park, which is a -- a very impactful --  
 17 another one for traffic.

18 So those are my concerns. Thank you.  
 19 MR. RIBARIC: Thank you. So next is  
 20 Ryanne Powers-Cavo, and then on deck is  
 21 Maria Wharton.  
 22 All right. We'll come back to Ryanne.  
 23 So Maria Wharton, and then on deck is  
 24 Cheryl Carneke.  
 25 All right. Seems like Maria is not with us

1 MS. LEVENGOOD: Hi. Thank you so much. I'm a  
 2 resident in Highlands Reserve in Palm City off of  
 3 High Meadows, which essentially serves as a service  
 4 road for I-95, and also connects a lot of people  
 5 that jump from I-95 to the Turnpike.

6 My concern is the noise and whether there will  
 7 be noise abatement for the residents in  
 8 Highlands Reserve, as well as Hammock Creek. I  
 9 understand that the cost is reasonable and it was  
 10 warranted based on the studies but that there's a  
 11 gas pipeline that might prevent noise abatement.  
 12 Can you speak to that or am I simply asking that it  
 13 be included? I -- I just -- we need that.

14 And then the second thing is, the exit at  
 15 Kanner Highway and State Road 76 for the Turnpike  
 16 would greatly help diminish the amount of traffic  
 17 volume on High Meadows and traveling between the  
 18 exit at Martin Highway to go south again. And so I  
 19 would request that that access be strongly  
 20 considered at Kanner Highway.

21 Thank you.  
 22 MR. RIBARIC: All right. I think we have a  
 23 few other people that have registered previously  
 24 that have joined us. I will try this name,  
 25 Idelfonso Vaquero, Vearo (phonetic). You have been

1 tonight either. Cheryl Carneke and then Lana Shah  
 2 is on deck.

3 All right. Lana Shah is now -- Thomas Kott?  
 4 And then seems like he's not online with us  
 5 tonight.

6 Gregory Gryczan? Is Clinton Harris?  
 7 Gregory? Please provide your comment. You're  
 8 look green on our side. All right. We'll come  
 9 back and try one more time.

10 Now, we'll move on to Clinton Harris, and then  
 11 on deck John Vogt.

12 All right. It seems Clinton is not with us.  
 13 So now we'll move on to John Vogt. They don't seem  
 14 to be with us tonight either.

15 Jackson Hurst? Moving on to Lillian Alston,  
 16 doesn't seem to be with us. And then Brian Akrami.

17 All right. It seems like we have gone through  
 18 the list of those that have pre-registered. Let's  
 19 jump back to see if -- I think Donna Levengood was  
 20 online. Maybe she had a moment. We're gonna go  
 21 ahead and unmute you Donnamen -- Donna and see if  
 22 you're able to provide your comment now.

23 MS. LEVENGOOD: Donna Levengood, are you able  
 24 to hear me?

25 MR. RIBARIC: Yes, we can. Thank you.

1 unmuted on our side and you're showing that you've  
 2 been self-muted so check your audio settings to see  
 3 if -- the microphone icon should be green on the go  
 4 to webinar.

5 All right. And I think we had Gregory Gryczan  
 6 join us. Gregory? You are unmuted so check your  
 7 settings to make sure that your microphone is  
 8 green, and then please provide your comment.

9 All right. There were also several other  
 10 folks that had registered online to be in person  
 11 and provide comment. I want to make sure we give  
 12 those folks an opportunity to provide feedback at  
 13 our in-person locations. I'll list a few folks and  
 14 then we'll get with Mark and Bill to see if they're  
 15 available.

16 Luis Valdes, Marylou Barrett, and  
 17 John Singleton. Bill or Mark?

18 MR. HOWELL: Brian, there's no one here with  
 19 those names.

20 MR. RIBARIC: All right. So some of the  
 21 other in-person folks where Marshall Winslow,  
 22 Jennifer Whiting and Joy Puerta. Are those folks  
 23 at the in-person venue?

24 MR. HOWELL: They're not in Stuart.

25 MR. EASLEY: There's no one here. Everyone's



1 left.

2 MR. RIBARIC: Thank you. I believe we had

3 Marylou Barrett join us online. So we will go

4 ahead, Marylou, and unmute you, and please provide

5 your comment.

6 MS. BARRETT: Hi. I live off of Ann Arbor,

7 which runs parallel to the Turnpike, and the noise

8 on a normal day is pretty deafening. So looking at

9 the plan the noise barrier is not coming down this

10 far. There is nothing to abate the noise. Of

11 course my concern is, our property values, not

12 being able to enjoy our -- our pool and our

13 backyard anytime of the year because of the noise.

14 Anything else? Yes. That's gonna be it. Thank

15 you.

16 MR. RIBARIC: Thank you. Thank you for your

17 comment.

18 All right. I think we're ready to move on and

19 finish -- close out the public hearing here.

20 All right. Bill, one last time, is there

21 anybody at your location that would like to -- that

22 wish to speak tonight? If so, let's have them

23 speak out, fill out a Speaker Request Card, and

24 provide it to one of our members and let -- have

25 them speak at this time. Bill?

1 brief survey. Your participation will help us for

2 future public hearings. It is now 7:37. I hereby

3 officially close the public hearing for the

4 Turnpike Mainline Widening PD&E Study. On behalf

5 of Florida's Turnpike Enterprise, we thank you for

6 joining us tonight and take care.

7 Good night.

8 (Concluded at 7:37 P.M.)

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1 MR. HOWELL: Brian, there are no further

2 speakers that wish to speak here at Stuart.

3 MR. RIBARIC: Thank you. Mark, does anyone

4 location wish to speak? If so, have them fill out

5 a Speaker Card and provide it to the team and then

6 allow them to provide their comment.

7 MR. EASLEY: There are no other speakers,

8 Brian.

9 MR. RIBARIC: Thank you, Mark?

10 Thank you for your interest in this project

11 and for taking the time to attend this public

12 hearing for the Turnpike Widening -- Mainline

13 Widening of the PD&E Study. Again, Florida's

14 Turnpike Enterprise invites your comments so please

15 use one of our four options on the screen to submit

16 your comments to me, the project manager,

17 Brian Ribaric. The public comment period will

18 remain open for this public hearing through

19 August 11, 2021. If you provided an email address

20 at registration, you will receive a follow-up email

21 within the next three business days with a link to

22 the recording of today's proceedings. The

23 recording link will also be posted on our project

24 website.

25 When you exit the webinar, you will receive a



1 STATE OF FLORIDA        )  
                                  :SS  
2 COUNTY OF ST. LUCIE )

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## CERTIFICATE

5

I, KAREN M. BELLEMARE, FPR, a Shorthand

6

Reporter, certify that the foregoing meeting,

7

Pages 1 through 72, was stenographically reported by

8

me and is a true and accurate transcription of said

9

meeting.

10

I certify further I am neither attorney nor

11

counsel for, nor related to, nor employed by any of

12

the parties to the action in which the meeting is

13

taken and, further, that I am not a relative or an

14

employee of any attorney or counsel employed in this

15

case, nor am I financially interested in the outcome

16

of this action.

17

Dated this 8th day of November, 2021.

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Karen M. Bellemare, FPR

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| <b>understand</b> 38:7<br>50:24 68:9                                        | <b>virtual</b> 1:7 4:7 6:3                                                               | <b>week</b> 51:14 57:24                                                                                                                                                                                                        | <b>wind</b> 58:3,5,6,8                                                                               | <b>2017</b> 32:12                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                |
| <b>understanding</b><br>48:7                                                | <b>Vogt</b> 67:11,13                                                                     | <b>weekend</b> 57:25                                                                                                                                                                                                           | <b>Winslow</b> 69:21                                                                                 | <b>2019</b> 61:13                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                |
| <b>unfortunately</b><br>59:15                                               | <b>volume</b> 9:16,18<br>28:18 57:22 68:17                                               | <b>weeks</b> 40:23 51:15                                                                                                                                                                                                       | <b>Winter</b> 32:12,13                                                                               | <b>2020</b> 5:7,8 57:20,21<br>57:21                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              |
| <b>Uniform</b> 24:10,13<br>24:18                                            | <b>volumes</b> 38:11                                                                     | <b>weight</b> 6:14                                                                                                                                                                                                             | <b>Winterlakes</b> 27:8                                                                              | <b>2021</b> 1:9 2:9 4:25<br>6:17,25 8:13 32:2<br>32:7 33:22 71:19<br>73:17                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       |
| <b>unique</b> 29:10 57:13                                                   | <hr/> <b>W</b> <hr/>                                                                     | <b>welcome</b> 2:3 3:20                                                                                                                                                                                                        | <b>wish</b> 34:11 70:22<br>71:2,4                                                                    | <b>2022</b> 32:13                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                |
| <b>United</b> 28:4                                                          | <b>waited</b> 51:12                                                                      | <b>Wellington</b> 64:22                                                                                                                                                                                                        | <b>wishing</b> 7:8 20:12                                                                             | <b>2045</b> 35:10 39:24<br>39:25                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                 |
| <b>unmute</b> 34:11<br>54:25 67:21 70:4                                     | <b>wake</b> 43:22                                                                        | <b>west</b> 1:15 11:16<br>12:1 13:1,21 14:5<br>14:16,18 15:7,18<br>15:25 16:11,14<br>17:17 19:8,24<br>20:8,15,18,19,20<br>20:23,25 21:4,6,8<br>21:11,14,15,19<br>22:2,21 26:12,21<br>27:6 37:2,8,12<br>47:8 52:1 58:3<br>65:24 | <b>woke</b> 43:24                                                                                    | <b>20th</b> 35:20                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                |
| <b>unmuted</b> 54:7,12<br>69:1,6                                            | <b>wall</b> 36:7,10,18<br>49:7,9 50:4 52:10<br>53:1 57:17 58:10<br>61:8 62:16 63:7       | <b>westbound</b> 19:7                                                                                                                                                                                                          | <b>Wolf</b> 1:10 8:13                                                                                |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>unsatisfied</b> 24:25                                                    | <b>walls</b> 30:13 42:16<br>52:13 55:7                                                   | <b>westerly</b> 58:6,6                                                                                                                                                                                                         | <b>wonderful</b> 39:10                                                                               |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>upset</b> 61:16                                                          | <b>want</b> 38:13 44:22<br>50:19,20 52:11,25<br>69:11                                    | <b>western</b> 14:5,17                                                                                                                                                                                                         | <b>word</b> 25:6                                                                                     |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>Urcheck</b> 39:15<br>41:19,19                                            | <b>wanted</b> 35:6 56:9<br>60:17                                                         | <b>wetland</b> 27:22                                                                                                                                                                                                           | <b>working</b> 36:6                                                                                  |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>use</b> 26:6 29:13<br>34:12 38:4 39:9<br>42:1,8 62:6 63:23<br>64:5 71:15 | <b>warranted</b> 68:10                                                                   | <b>wetlands</b> 23:20<br>27:15,17                                                                                                                                                                                              | <b>Works</b> 3:14,15                                                                                 |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>users</b> 11:4                                                           | <b>wasn't</b> 36:12 43:20                                                                | <b>Wharton</b> 66:21,23                                                                                                                                                                                                        | <b>worried</b> 48:20                                                                                 |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>uses</b> 4:7                                                             | <b>watch</b> 3:18 43:17<br>47:11                                                         | <b>whatsoever</b> 45:9                                                                                                                                                                                                         | <b>writing</b> 6:19 33:13                                                                            |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>utilize</b> 28:1,8                                                       | <b>water</b> 23:19                                                                       | <b>wheeler</b> 55:11                                                                                                                                                                                                           | <b>written</b> 5:10,17,22<br>31:14,24 33:17,23                                                       |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>utilizing</b> 59:6                                                       | <b>waters</b> 27:16                                                                      | <b>Whiting</b> 69:22                                                                                                                                                                                                           | <b>wrong</b> 44:11                                                                                   |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <hr/> <b>V</b> <hr/>                                                        | <b>Wawa's</b> 39:3                                                                       | <b>whoever's</b> 53:21                                                                                                                                                                                                         | <b>www.treasureco...</b><br>5:18 6:24 8:22<br>31:15                                                  |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>Valdes</b> 69:16                                                         | <b>way</b> 36:8,19,20<br>37:17 39:24 42:18<br>48:24 49:5 53:1<br>53:11,12 61:10<br>62:24 | <b>wide</b> 13:10                                                                                                                                                                                                              | <b>X</b>                                                                                             |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>value</b> 24:22 57:15<br>62:2                                            | <b>ways</b> 33:15,24                                                                     | <b>widened</b> 11:25 12:5<br>12:22 13:7                                                                                                                                                                                        | <b>Y</b>                                                                                             |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>values</b> 29:10 70:11                                                   | <b>we'll</b> 2:13 46:14<br>57:1 65:5,10<br>66:22 67:8,10,13<br>69:14                     | <b>widening</b> 1:1 2:4<br>3:21 8:23 11:10<br>11:16,20 12:19                                                                                                                                                                   | <b>Yeah</b> 55:4 62:21                                                                               |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>Vaquero</b> 68:25                                                        | <b>we're</b> 40:13 43:14<br>46:5 47:16 48:25                                             |                                                                                                                                                                                                                                | <b>year</b> 70:13                                                                                    |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>various</b> 5:5 30:18                                                    |                                                                                          |                                                                                                                                                                                                                                | <b>years</b> 11:3 41:21<br>51:25 52:6 53:10<br>53:11 63:19                                           |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>vastly</b> 41:9                                                          |                                                                                          |                                                                                                                                                                                                                                | <b>Z</b>                                                                                             |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>Vearo</b> 68:25                                                          |                                                                                          |                                                                                                                                                                                                                                | <b>Zang</b> 62:8                                                                                     |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>vendors</b> 62:6                                                         |                                                                                          |                                                                                                                                                                                                                                | <b>Zones</b> 9:24                                                                                    |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
| <b>venue</b> 69:23                                                          |                                                                                          |                                                                                                                                                                                                                                | <b>0</b>                                                                                             |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |
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**22** 1:9 2:9 29:22**220** 63:3**2400** 1:12 8:15**26** 6:25**26th** 57:21**27** 5:7

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**3** 59:13**335.199** 7:20**339.09** 24:10**339.155** 7:20**34761** 31:22**34950** 1:24**34982** 8:18**34986** 1:17**34997** 1:12 8:15**367** 27:15**37** 9:3 11:15 30:18**39th** 15:13

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**4** 12:3**40** 53:11 57:11**400** 66:7**407-264-3095**

31:24 34:7

**407)264-3095** 6:12**423374-1** 2:5**423374-1-22-01** 1:4**43** 51:25**45** 63:22**45,000** 37:14**485** 60:21

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**5**

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**5** 5:8 29:22**5:00** 8:20**50** 57:23 58:7**500** 1:16**500,000** 53:4

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**8**

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**8** 65:18**891** 65:13**8th** 73:17

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**9**

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**9:00** 8:19**95** 26:9**99** 5:10

# APPENDIX H

## *Delegation Letter*



**From:** [Colon, Christina](#)  
**To:** [Stults, Jennifer](#)  
**Cc:** [Pinzon, Henry](#)  
**Subject:** FW: SEIR Approval Delegation  
**Date:** Thursday, September 10, 2020 11:01:21 AM

---

I discussed with Nicola and we agree to delegate State Environmental Impact Report (SEIR) approvals to the Planning and Environmental Management Office (PLEMO) Administrator, Jennifer Stults.

Thanks,

**Christina N. Colón, P.E.**

Director of Transportation Development

**Florida's Turnpike Enterprise**

Florida Department of Transportation

Mile Post 263, Building 5315, Ocoee, FL 34761

**Tel** (407) 264-3603 / **Mobile** (407) 457-1024

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**From:** Colon, Christina  
**Sent:** Friday, September 4, 2020 3:46 PM  
**To:** Liquori, Nicola <Nicola.Liquori@dot.state.fl.us>  
**Cc:** Carrier, Denise <Denise.Carrier@dot.state.fl.us>  
**Subject:** FW: SEIR Approval Delegation

Nicola,

Jennifer Stults and I discussed this topic today and she has provided details below. BLUF – For our PD&E projects, you are now able to delegate State Environmental Impact Report (SEIR) approvals to either me or Jennifer's role. We can explore other options if you prefer but CO would like it to be a managerial position at or above PLEMO Administrator. Currently, all District Secretaries except for District 1 have delegated these SEIR approvals to the PLEMO Administrators. Whoever you decide, CO has advised that the approver will need SWEPT (StateWide Environmental Project Tracker) credentials and some training to navigate the program. I'm told it can be done with very minimal training and not the 5-7 hr training they've been holding. Jennifer has already completed it so it would only apply to me or you.

Please let me know your preference and we'll proceed accordingly with CO on next steps for the SWEPT access and training. My understanding is they would like to get an assessment of who needs the setup/training within the next week or two so they can coordinate calendars and put together an efficient implementation plan.

Thanks,

**Christina N. Colón, P.E.**

Director of Transportation Development

**Florida's Turnpike Enterprise**

Florida Department of Transportation

Mile Post 263, Building 5315, Ocoee, FL 34761

**Tel** (407) 264-3603 / **Mobile** (407) 457-1024

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**From:** Stults, Jennifer <[Jennifer.Stults@dot.state.fl.us](mailto:Jennifer.Stults@dot.state.fl.us)>

**Sent:** Friday, September 4, 2020 12:48 PM

**To:** Colon, Christina <[Christina.Colon@dot.state.fl.us](mailto:Christina.Colon@dot.state.fl.us)>

**Subject:** SEIR Approval Delegation

Christina,

To follow up on our recent discussion, Central Office has advised that we are able to delegate State Environmental Impact Report (SEIR) approvals. Typically, District Secretaries have approved these documents, which are the state versions of a federal Project Development & Environment (PD&E) document. Currently, all District Secretaries except for District 1 have delegated these SEIR approvals to the PLEMO Administrators.

Part of this is also the move to electronic documents to improve efficiency and consistency statewide. The SEIR approvals will now take place within the SWEPT system, making it available to EDMS and other FDOT electronic document database systems. This should be helpful to Design and other subsequent phases who may need to access this information. Central Office has advised that the approver will need SWEPT credentials but can set this up with minimal training.

We have several options here: Turnpike Secretary/CEO Nicola Liquori retains approval authority, Secretary Liquori delegates authority to Transportation Development Director Christina Colon, or Secretary Liquori delegates authority to Planning and Environmental Management Office (PLEMO) Administrator Jennifer Stults. We can also explore other options if you prefer. I am happy to accept this delegation if that is the decision. I completed the training in November, 2016, with subsequent required courses in 2017 as part of NEPA Assignment. Can you let me know of any preference, and I will proceed accordingly with necessary approvals and working with Central Office to get this set up in SWEPT?

Thanks,

Jennifer A. Stults, AICP CTP, CPM, FCCM  
Planning & Environmental Mgmt. Administrator



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[jennifer.stults@dot.state.fl.us](mailto:jennifer.stults@dot.state.fl.us)